

LEGEND

- △ BENCHMARK
- SECTION CORNER
- IRON PIPE FOUND
- IRON PIPE SET
- BOLLARD
- + SOIL BORING/MONITORING WELL/CORE LOCATION
- † FLAGPOLE
- ✕ MAILBOX
- SIGN
- BILLBOARD
- CONTROL BOX
- ⚡ TRAFFIC SIGNAL
- † RAILROAD CROSSING SIGNAL
- CABLE PEDESTAL
- ⊕ POWER POLE
- ⊕ GUY POLE
- ⊕ GUY WIRE
- ⊕ LIGHT POLE
- ✕ GROUND OR OTHER SPOT SHOT
- ♿ HANDICAPPED PARKING
- ⊕ ELECTRIC MANHOLE
- ⊕ ELECTRIC PEDESTAL
- ⊕ ELECTRIC METER
- ⊕ TELEPHONE MANHOLE
- ⊕ TELEPHONE PEDESTAL
- FO MARKED FIBER OPTIC
- ⊕ GAS VALVE
- ⊕ GAS METER
- ⊕ STORM MANHOLE
- ⊕ ROUND INLET
- ⊕ CATCH BASIN
- ⊕ STORM SEWER END SECTION
- ⊕ SANITARY MANHOLE
- ⊕ SANITARY CLEANOUT OR SEPTIC VENT
- ⊕ SANITARY INTERCEPTOR MANHOLE
- ⊕ MISCELLANEOUS MANHOLE
- ⊕ WATER VALVE
- ⊕ HYDRANT
- ⊕ WATER SERVICE CURB STOP
- ⊕ WATER MANHOLE
- ⊕ WELL
- ⊕ WATER SURFACE
- ⊕ WETLANDS FLAG
- ▲ MARSH
- ★ CONIFEROUS TREE
- DECIDUOUS TREE
- SHRUB
- EDGE OF TREES
- S — SANITARY SEWER
- STO — STORM SEWER
- W — WATERMAIN
- G — MARKED GAS MAIN
- E — MARKED ELECTRIC
- OHW — OVERHEAD WIRES
- B — BUREAU ELEC. SERV.
- T — MARKED TELEPHONE
- TV — MARKED CABLE TV LINE
- FO — MARKED FIBER OPTIC
- FM — FORCE MAIN

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HORIZONTAL COORDINATES ARE BASED ON WISCONSIN STATE PLANE COORDINATE SYSTEM SOUTH ZONE, NAD-83.

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).

VILLAGE OF STURTEVANT
 JACK FEINER
 PUBLIC WORKS MANAGER
 262-886-7202

OWNER:
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 STURTEVANT, WI 53177

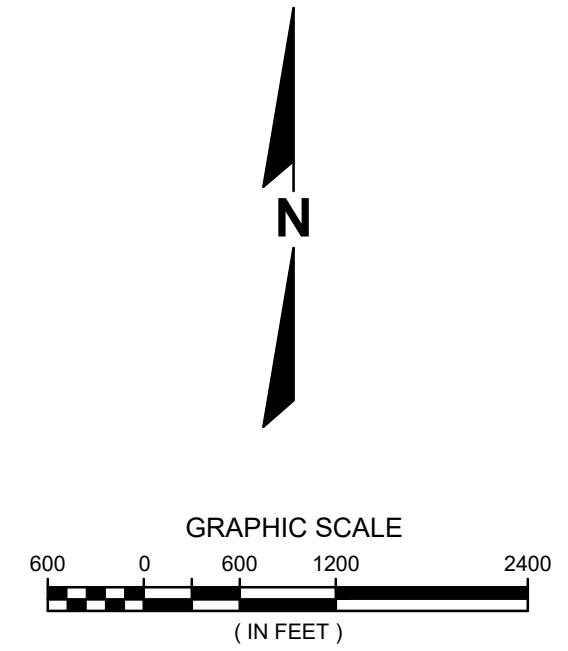
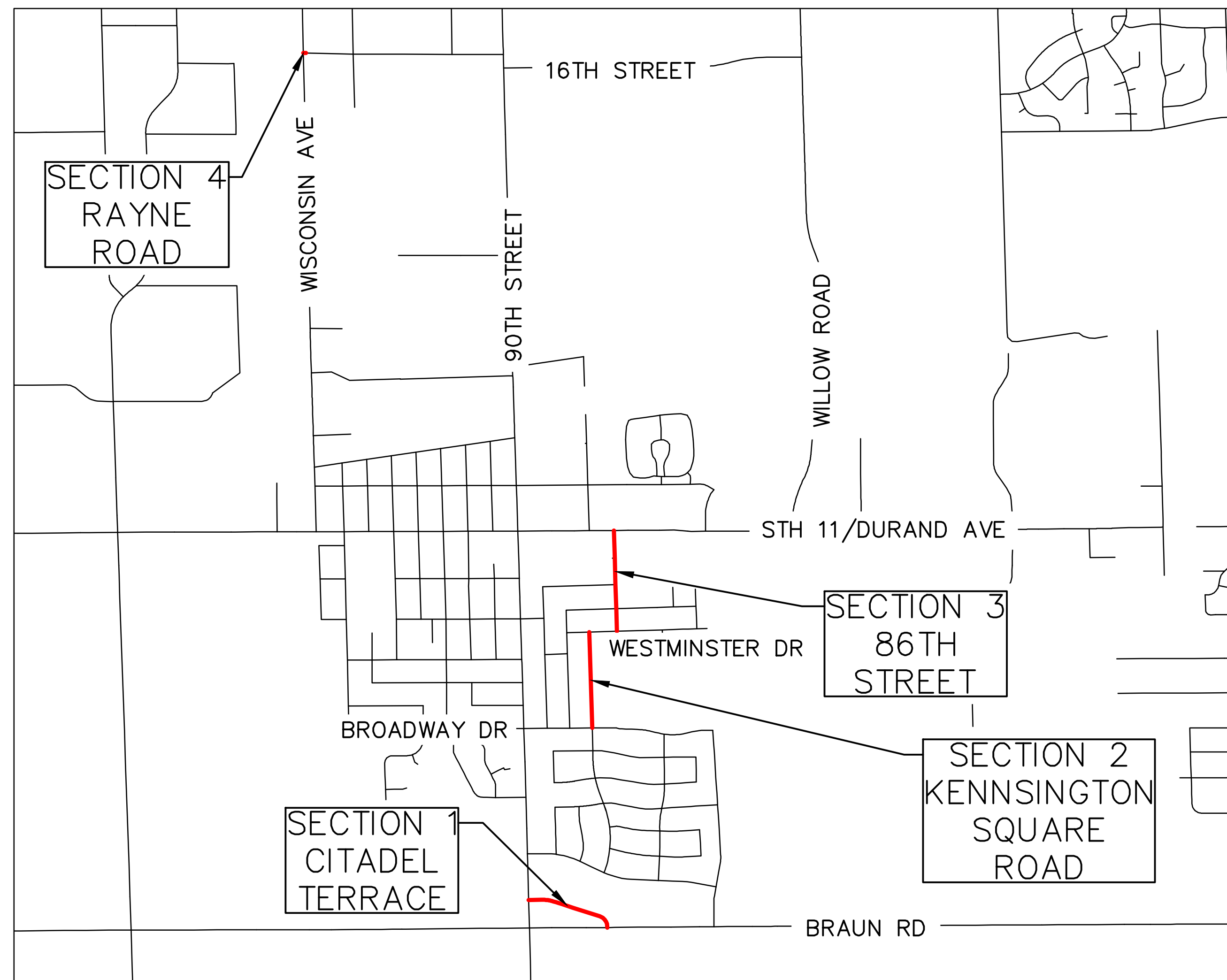
VILLAGE OF STURTEVANT

RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM

SECTIONS 1-4

LOCATION MAP



PLAN INDEX SHEET NO.

DESCRIPTION

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DESCRIPTION

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 Brookfield, WI 53005-5938
 (262) 781-1000
 rasmith.com



VILLAGE OF STURTEVANT
 RACINE COUNTY, WISCONSIN
2023 PAVING PROGRAM
COVER SHEET

DRAFT
 NOT FOR CONSTRUCTION

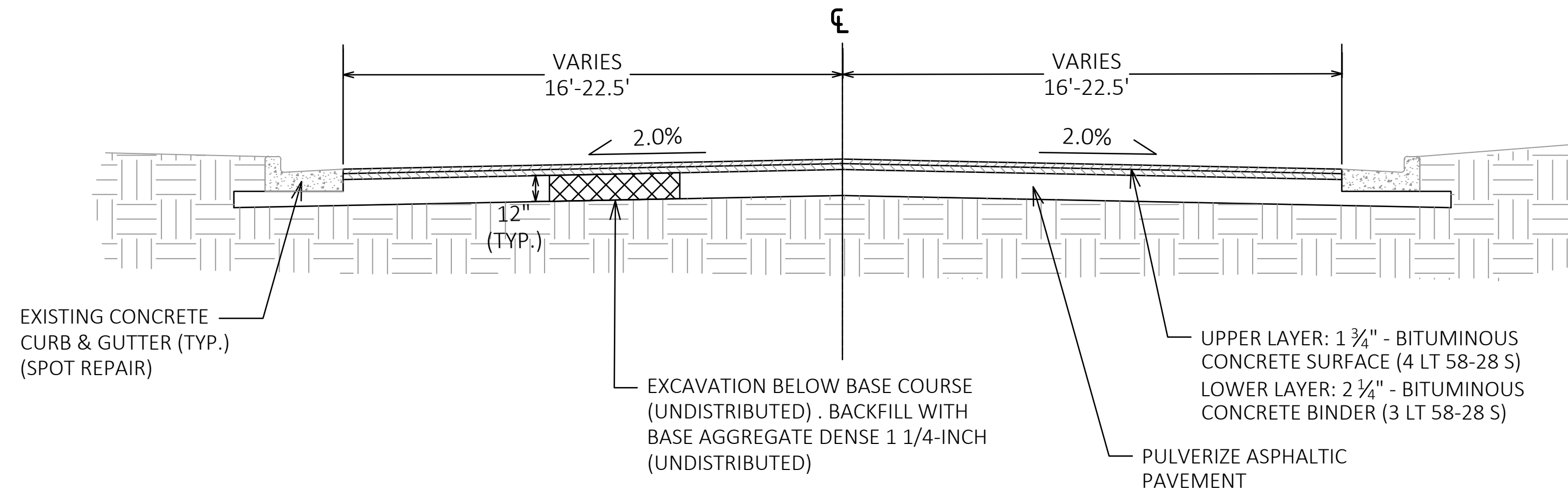
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DATE: 05/02/2023
SCALE: 1" = 1200'
JOB NO. 2232100
PROJECT MANAGER: JASON FEUCHT, P.E.
DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
01

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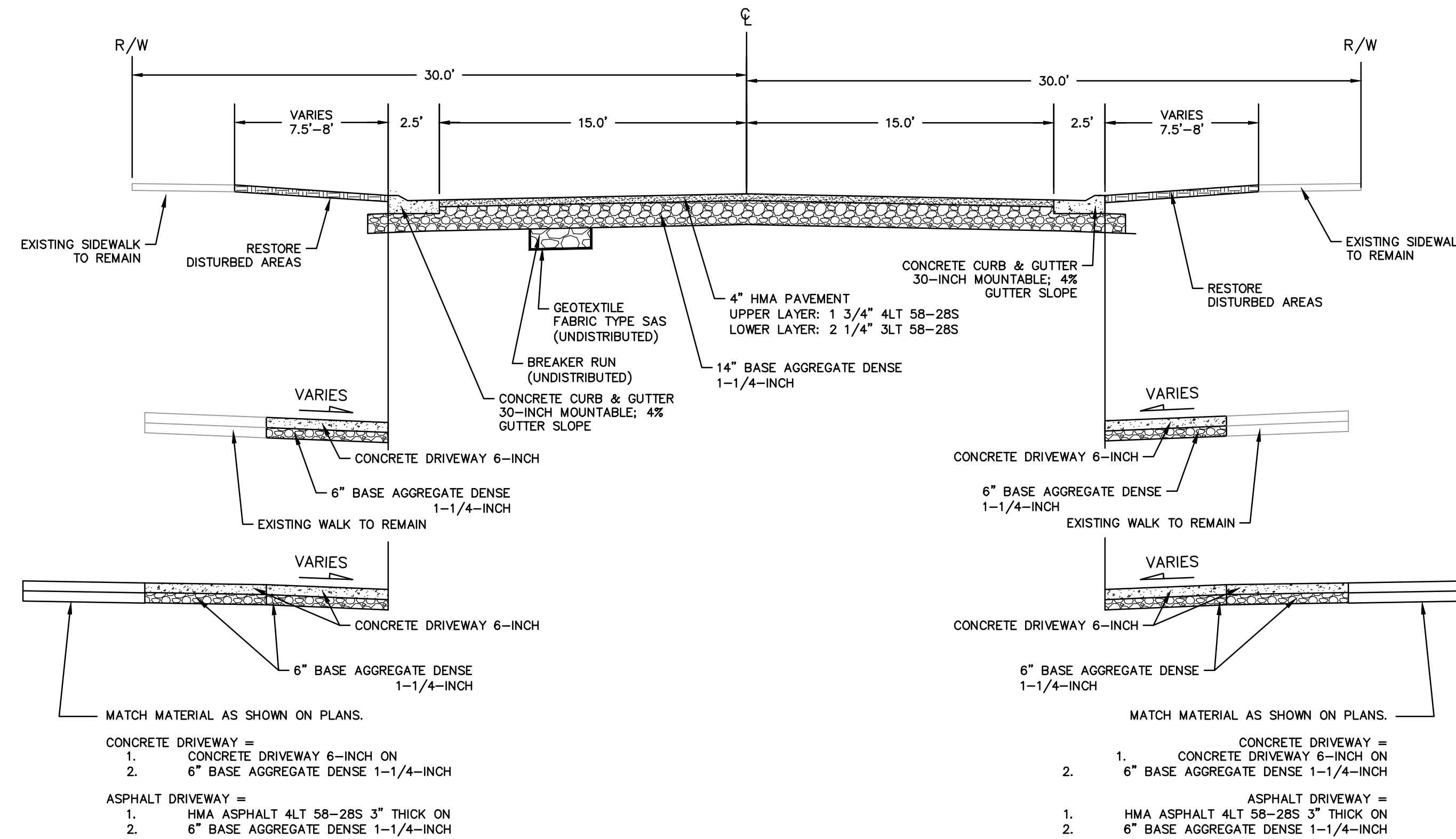


**Know what's below.
Call before you dig.**

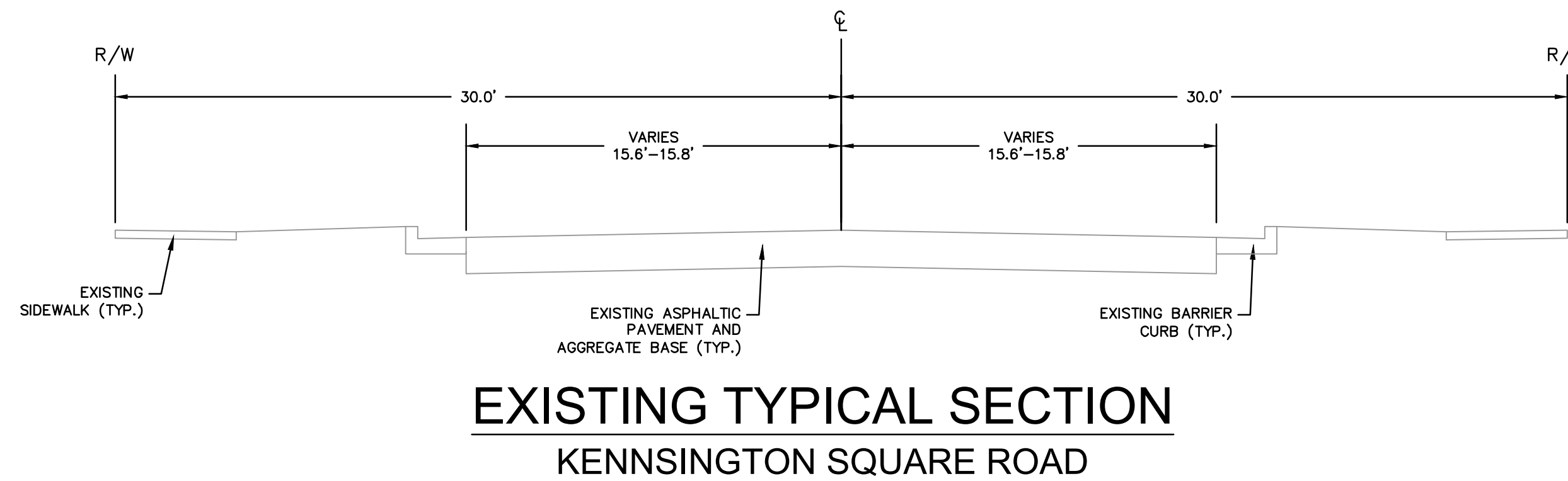
GENERAL NOTES:	CALENDAR DAYS. TEMPORARY SEED (SEEDING TEMPORARY, 630.0200) IF NEEDED SHALL BE INCIDENTAL TO EXCAVATION COMMON BID ITEM.	BE UPDATED AND RESUBMITTED BY THE GENERAL CONTRACTOR TO THE VILLAGE AND WDNR.	DESCRIPTION
	EROSION CONTROL OPERATION AND MAINTENANCE PLAN	EROSION CONTROL MEASURES	DATE
<p>1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE VILLAGE OF STURTEVANT STANDARDS, THE STANDARD SPECIFICATIONS FOR SEWER & WATER CONSTRUCTION IN WISCONSIN, SIXTH EDITION DATED: DECEMBER 22, 2003, w/ ADDENDA NOS. 1 & 2 & THE STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION; LATEST EDITION, ADDITIONS, AND REVISIONS.</p> <p>2. THE LOCATIONS OF EXISTING UTILITY INSTALLATIONS AS SHOWN ON THIS PLAN ARE APPROXIMATE. THERE MAY BE OTHER UNDERGROUND UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXISTENCE AND LOCATION OF ALL UNDERGROUND UTILITIES.</p> <p>3. R.A. SMITH, INC. ASSUMES NO RESPONSIBILITY FOR DAMAGES, LIABILITY OR COSTS RESULTING FROM CHANGES OR ALTERATIONS MADE TO THIS PLAN WITHOUT THE EXPRESSED WRITTEN CONSENT OF R.A. SMITH, INC.</p> <p>4. ALL COPYRIGHTS TO THESE DRAWINGS ARE RESERVED. THEY MAY NOT BE COPIED, CHANGED, OR ASSIGNED TO ANY THIRD PARTY WITHOUT OBTAINING THE EXPRESSED WRITTEN PERMISSION OF R.A. SMITH, INC.</p> <p>5. CONTRACTOR TO VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITY CONNECTIONS PRIOR TO CONSTRUCTION.</p> <p>6. CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE FOR UTILITY MARKING AND LOCATIONS PRIOR TO CONSTRUCTION.</p> <p>7. TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY ENGINEER.</p> <p>8. NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT ENGINEER APPROVAL.</p> <p>9. ALL SIGNS AND SIGN POSTS REMOVED SHALL BE DELIVERED TO THE VILLAGE OF STURTEVANT.</p> <p>10. WHEN THE QUANTITY OF BASE AGGREGATE DENSE IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY ENGINEER.</p> <p>11. EXCAVATION BELOW SUBGRADE (EBS), IF DETERMINED BY THE ENGINEER, SHALL BE PAID FOR AS COMMON EXCAVATION. THE LOCATION OF EBS WILL BE DETERMINED BY THE ENGINEER. SUCH EBS IS NOT USED TO BALANCE YARDAGE.</p>	<p>6. IF TEMPORARY STORAGE IS REQUIRED FOR ANY EXCESS SOIL OR SPOIL MATERIAL, PERIMETER CONTROL AND TEMPORARY STORAGE AND STABILIZATION WILL BE NECESSARY WITHIN 7 CALENDAR DAYS, PER WDNR TECHNICAL STANDARDS.</p> <p>1. ALL EROSION AND SEDIMENTATION CONTROL PRACTICES SHALL BE, AT A MINIMUM, BE INSPECTED WEEKLY AND WITHIN 24 HOURS AFTER EVERY PRECIPITATION EVENT THAT PRODUCES 0.5 INCHES OF RAIN OR MORE DURING A 24-HOUR PERIOD. NEEDED REPAIRS SHALL BE MADE IMMEDIATELY TO MAINTAIN ALL PRACTICES AS DESIGNED.</p> <p>2. A COPY OF THE STORM WATER MANAGEMENT REPORT, GRADING & EROSION CONTROL PLAN, PROJECT PERMITS, EROSION CONTROL INSPECTION REPORTS, AND LOG BOOK CONTAINING: INSPECTION DATES AND TIMES, REPAIRS NECESSARY, AND REPAIRS MADE, SHALL BE KEPT ON SITE AT ALL TIMES AT AN ACCESSIBLE LOCATION, SUCH AS A DEDICATED PROJECT MAILBOX.</p> <p>3. CONTRACTORS ARE TO MAINTAIN THE CONSTRUCTION SITE IN A NEAT AND TIDY MANNER FOR THE DURATION OF THE PROJECT.</p> <p>4. ALL MAINTENANCE IS TO BE IN ACCORDANCE WITH APPLICABLE WISCONSIN DEPARTMENT OF NATURAL RESOURCES TECHNICAL STANDARDS.</p> <p>5. SITE SHALL BE STABILIZED IN PHASES AS WORK IN VARIOUS AREAS IS COMPLETED (AREA BROUGHT UP TO GRADE AND TOPSOIL PLACED OR PAVED).</p> <p>6. ALL DISTURBED AREAS THAT HAVE BEEN BROUGHT TO FINAL GRADE WILL BE STABILIZED WITH TOPSOIL, SEED, FERTILIZER AND EROSION MAT, AS SHOWN ON THE PLANS, WITHIN 7 DAYS OF THE END OF ACTIVE DISTURBANCE OF SOIL SURFACES; OTHERWISE INTERIM SOIL STABILIZATION METHODS ARE REQUIRED.</p> <p>7. INTERIM SOIL STABILIZATION METHODS, SUCH AS TEMPORARY GRADING PRACTICES FOR EROSION CONTROL, TEMPORARY SEEDING AND POLYMER APPLICATION IN ACCORDANCE WITH DNR TECHNICAL STANDARDS 1067, 1059 AND 1050, RESPECTIVELY, ARE REQUIRED FOR AREAS OF THE SITE WHERE THERE IS NO ACTIVE SOIL DISTURBANCE WITHIN 7 DAYS. DUST CONTROL SHALL BE PROVIDED WHEN NECESSARY, IN ACCORDANCE WITH DNR TECHNICAL STANDARD 1068.</p> <p>8. ALL PERMANENT SEEDING SHALL BE COMPLETED BY SEPTEMBER 15. ALL TEMPORARY SEEDING SHALL BE COMPLETED BY OCTOBER 15. (REFER TO DNR TECHNICAL STANDARD 1059.)</p> <p>9. STABILIZATION AFTER OCTOBER 15 SHALL CONSIST OF ANIONIC POLYACRYLAMIDE (PAM) IN ADDITION TO TEMPORARY SEEDING. PLACE PAM IN ACCORDANCE WITH WDNR TECHNICAL STANDARD 1050. ANIONIC PAM SHALL NOT BE APPLIED WITHIN 30 FEET (MINIMUM) OF SURFACE WATERS OF THE STATE.</p> <p>10. ALL SEEDED AREAS SHALL BE FERTILIZED, RE-SEEDED AS NECESSARY, AND MATTED ACCORDING TO PROJECT SPECIFICATIONS TO MAINTAIN A VIGOROUS DENSE VEGETATIVE COVER.</p> <p>11. SILT FENCE SHALL BE INSTALLED AROUND THE BASE OF THE TOPSOIL STOCKPILES WITHIN 7 DAYS OF LAY-UP AND TEMPORARILY SEEDED WITHIN 30 DAYS OF LAY-UP. LONG-TERM STOCKPILES WILL BE PERMANENTLY SEEDED WITHIN 7 DAYS OF THE END OF ACTIVE DISTURBANCE.</p> <p>12. SEDIMENT SHALL BE REMOVED FROM BEHIND THE SILT FENCE AND EROSION BALES WHEN IT REACHES HALF THE HEIGHT OF THE FENCE OR BALE. THE SILT FENCE AND EROSION BALES WILL BE REPAIRED AS NECESSARY TO MAINTAIN A BARRIER.</p> <p>13. DEWATERING SHALL BE PROVIDED IN ACCORDANCE WITH WDNR TECHNICAL STANDARD 1061. THE CONTRACTOR SHALL CHOOSE AN APPROPRIATE DEWATERING METHOD/DEVICE, PER DNR TECHNICAL STANDARD 1061, TO REMOVE SEDIMENT FROM WATER GENERATED DURING DEWATERING ACTIVITIES PRIOR TO DISCHARGING OFF-SITE, TO WATERS OF THE STATE, OR WETLANDS.</p> <p>14. ANY SEDIMENT TRACKED ONTO A PUBLIC OR PRIVATE ROAD SHOULD BE REMOVED BY STREET CLEANING, NOT FLUSHED, BEFORE THE END OF EACH WORKING DAY.</p> <p>15. SITE AREAS LEFT INACTIVE FOR 7 CALENDAR DAYS SHALL BE TEMPORARILY SEEDED IN ACCORDANCE WITH WDNR TECHNICAL STANDARD 1059.</p> <p>16. CONTRACTOR'S SPECIFIC CONSTRUCTION METHODS MAY DICTATE ADDITIONAL SPECIFIC EROSION CONTROL MEASURES NOT INCLUDED HEREIN. DOCUMENTED ADDITIONS AND OR CHANGES SHALL BE APPROVED BY WDNR AND THE MUNICIPALITY WITH THE ENGINEER'S CONSENT PRIOR TO IMPLEMENTATION.</p> <p>17. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING EROSION & SEDIMENT CONTROL THROUGHOUT CONSTRUCTION UNTIL SITE STABILIZATION IS ATTAINED.</p>	<p>1. CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION CONTROL MEASURES AS INDICATED ON THESE PLANS AND PER THE LATEST WDNR TECHNICAL STANDARDS. TECHNICAL STANDARDS MAY BE VIEWED ONLINE AT: https://dnr.wi.gov/topic/Stormwater/standards/index.html</p> <p>2. ALL EROSION CONTROL MEASURES SHALL AT A MINIMUM BE INSPECTED WEEKLY AND WITHIN 24 HOURS AFTER EVERY PRECIPITATION EVENT THAT PRODUCES ½ INCH OF RAIN OR MORE DURING A 24 HOUR PERIOD.</p> <p>3. CATCH BASINS SHALL BE PROTECTED WITH INLET FILTERS THAT ARE PHASED IN WITH CONSTRUCTION TO REDUCE SEDIMENT FROM ENTERING THESE AREAS PER WDNR TECHNICAL STANDARD 1060 AS FOLLOWS:</p> <p>3.1. ALL FABRIC BARRIERS SELECTED FOR INLET/CATCH BASIN PROTECTION DEVICES SHALL BE SELECTED FROM THE LIST OF APPROVED FABRICS CERTIFIED FOR INLET PROTECTION, GEOTEXTILE FABRIC, TYPE FF IN THE CURRENT EDITION OF THE WISDOT PRODUCT ACCEPTABILITY LIST (PAL). THE PAL MAY BE ATTAINED ONLINE AT: http://wisconsin-dot.gov/Pages/doing-bus-eng-consultants/cnsit-rsrces/tools/pal/default.aspx</p> <p>3.2. PLACEMENT OF SPOIL MATERIAL, DEBRIS, OR SOILS ON TOP OF INLETS/CATCH BASINS, EVEN IF TEMPORARY IS STRICTLY DISCOURAGED AND PROHIBITED. SEDIMENT DEPOSITS SHALL BE REMOVED AND THE INLET PROTECTION DEVICE RESTORED TO ITS ORIGINAL DIMENSIONS WHEN THE SEDIMENT HAS ACCUMULATED BETWEEN ¼ AND ½ THE DESIGN DEPTH OF THE DEVICE, OR WHEN THE DEVICE IS NO LONGER FUNCTIONING PER MANUFACTURER'S SPECIFICATIONS. ALL SEDIMENT COLLECTED SHALL BE PROPERLY DISPOSED OF TO PREVENT DISCHARGE INTO LOCAL STORM SEWER PIPES, WETLANDS, OR WATERWAYS.</p> <p>3.4. DUE CARE SHALL BE TAKEN TO ENSURE SEDIMENT DOES NOT FALL INTO THE INLETS/CATCH BASINS AND IMPEDE THE INTENDED FUNCTION OF THE DEVICE. ANY MATERIAL FALLING INTO THE INLET/CATCH BASIN SHALL BE REMOVED AND PROPERLY DISPOSED OF PER NOTE 3.3 ABOVE.</p> <p>3.5. INLET FILTERS MAY BE REMOVED AND PROPERLY DISPOSED OF UPON COMPLETION OF CONSTRUCTION, HAULING OR MOVEMENT OF CONSTRUCTION EQUIPMENT THROUGHOUT THE SITE, AND ONCE THE SITE IS ADEQUATELY STABILIZED, UNLESS OTHERWISE NOTIFIED BY WDNR.</p> <p>4. CONSTRUCTION SITE PERIMETER SHALL BE PROTECTED AS SHOWN ON THE PLANS PRIOR TO THE START OF CONSTRUCTION TO INTERCEPT AND REDUCE THE FLOW OF SEDIMENT-LADEN SHEET FLOW RUNOFF FROM THE CONSTRUCTION SITE PURSUANT TO WDNR TECHNICAL STANDARD 1056. SEDIMENT LOGS MAY BE USED IN PLACE OF SILT FENCE:</p> <p>4.1. SILT FENCE ENDS SHALL BE EXTENDED UP-SLOPE TO PREVENT WATER FROM FLOWING AROUND THE ENDS OF THE FENCE.</p> <p>4.2. INSTALLED SILT FENCE SHALL BE IN ACCORDANCE WITH THE DETAIL IN THESE PLANS (MINIMUM 14" HEIGHT, MAXIMUM HEIGHT 28" MEASURED FROM THE INSTALLED GROUND ELEVATION).</p> <p>4.3. SILT FENCE SHALL BE SUPPORTED BY EITHER STEEL OR WOOD SUPPORT POSTS.</p> <p>4.4. THE MAXIMUM POST SPACING FOR NON-WOVEN SILT FENCE SHALL BE 3 FEET AND 8 FEET FOR WOVEN FABRIC.</p> <p>4.5. SILT FENCE SHALL HAVE A SUPPORT CORD AT THE TOP OF THE FENCE.</p> <p>4.6. WHERE JOINTS ARE NEEDED, EACH END OF THE FABRIC SHALL BE SECURELY FASTENED TO A POST. THE POSTS SHALL BE WRAPPED AROUND EACH OTHER TO PRODUCE A STABLE AND SECURE JOINT OR SHALL BE OVERLAPPED THE DISTANCE BETWEEN TWO POSTS.</p> <p>4.7. THE POSTS SHALL EXTEND INTO THE GROUND 20 INCHES AFTER INSTALLATION.</p> <p>4.8. SILT FENCE SHALL BE ANCHORED BY SPREADING AT LEAST 8 INCHES OF THE FABRIC IN A 4 INCH WIDE BY 6 INCH DEEP TRENCH, OR 6 INCH DEEP V-TRENCH ON THE UPSLOPE SIDE OF THE FENCE. THE TRENCH SHALL BE BACKFILLED AND COMPACTED. TRENCHES SHALL NOT BE EXCAVATED ANY WIDER OR DEEPER THAN NECESSARY FOR PROPER INSTALLATION.</p> <p>4.9. ON THE TERMINAL ENDS OF THE SILT FENCE THE FABRIC SHALL BE WRAPPED AROUND THE POST SUCH THAT THE STAPLES ARE NOT VISIBLE.</p> <p>4.10. GEOTEXTILE FABRIC SPECIFICATIONS SHALL MEET VALUES ESTABLISHED IN TECHNICAL STANDARD 1056.</p> <p>4.11. SILT FENCE SHALL BE REMOVED ONCE THE SITE IS ADEQUATELY STABILIZED.</p> <p>4.12. WHEN PLACING SILT FENCE NEAR TREES, CARE SHALL BE TAKEN TO MINIMIZE DAMAGE TO THE ROOT SYSTEM BY AVOIDING COMPACTION AND ROOT CUTTING WITHIN 1.5 FEET MULTIPLIED BY THE INCH DIAMETER OF THE TREE.</p> <p>4.13. THE CONTRACTOR MAY FURTHER STRENGTHEN THE SILT FENCE BY USING STRAW BALES ON THE DOWN SLOPE SIDE AS NEEDED.</p> <p>4.14. DAMAGED OR DECOMPOSED SILT FENCE, UNDERCUTTING, OR FLOW CHANNELS AROUND THE END OF THE FENCE SHALL BE REPAIRED OR CORRECTED.</p> <p>4.15. SEDIMENT SHALL BE PROPERLY DISPOSED OF ONCE THE DEPOSITS REACH ½ THE HEIGHT OF THE FENCE TO PREVENT DISCHARGE INTO AREA WATERWAYS OR WETLANDS.</p> <p>5. DITCH CHECKS ARE TO BE USED TO REDUCE THE VELOCITY OF WATER FLOWING IN CHANNELS AS SHOWN ON THE PLANS. DITCH CHECKS SHALL BE CONSTRUCTED FROM ENGINEERED PRODUCTS FOUND ON THE PAL. DITCH CHECKS ARE TO BE INSTALLED IN THE AREAS SHOWN ON THE PLANS PER WDNR TECHNICAL STANDARD 1062 AS FOLLOWS:</p> <p>5.1. DITCH CHECKS ARE TO BE INSTALLED TO A MINIMUM HEIGHT OF 10 INCHES AND SHALL NOT EXCEED A MAXIMUM HEIGHT OF 16 INCHES FOR MANUFACTURED OR BIODEGRADABLE MATERIALS.</p> <p>5.2. DITCH CHECKS MUST BE INSTALLED WITH THE CENTER LOWER THAN THE SIDES FORMING A WEIR. IF THIS IS NOT DONE, STORMWATER FLOWS ARE FORCED TO THE EDGE OF THE DITCH CHECK, THUS PROMOTING SCOUR, OUT OF THE CHANNEL FLOW CAUSING EXCESSIVE EROSION.</p> <p>5.3. AT A MINIMUM, INSTALL ONE DITCH CHECK FOR EVERY TWO FEET OF DROP IN THE CHANNEL.</p> <p>5.4. DITCH CHECKS SHALL BE PLACED SUCH THAT THE RESULTANT PONDING WILL NOT CAUSE INCONVENIENCE OR DAMAGE TO ADJACENT AREAS.</p> <p>5.5. DITCH CHECKS SHALL BE UTILIZED DURING ROUGH GRADING AND SHALL BE REMOVED ONCE THE FINAL GRADING AND CHANNEL STABILIZATION IS APPLIED.</p> <p>5.6. CHANNEL EROSION MAT SHALL BE PLACED AT THE BASE OF THE DITCH CHECK, AND EXTENDED A MINIMUM OF 6 FEET, TO PREVENT SCOUR AND WASHING OUT THE TOE OF THE DITCH CHECK.</p> <p>5.7. DITCH CHECKS INSTALLED IN GRASS LINED CHANNELS MAY KILL THE VEGETATION IF WATER IS PONDED FOR EXTENDED PERIODS. PROPER MAINTENANCE IS REQUIRED TO KEEP AREAS ABOVE AND BELOW THE DITCH CHECK STABILIZED.</p> <p>6. NON-CHANNEL EROSION MAT TO BE USED TO PROTECT THE SOIL SURFACE ON ERODIBLE SLOPES FROM THE EROSION EFFECT OF RAINFALL AND PREVENT SHEET EROSION DURING THE ESTABLISHMENT OF GRASS OR OTHER VEGETATION, AND TO REDUCE SOIL MOISTURE LOSS DUE TO EVAPORATION. EROSION MAT TO BE INSTALLED IN THE AREAS SHOWN IN THE PLANS PER WDNR TECHNICAL STANDARD 1052 AND AS FOLLOWS:</p> <p>6.1. CLASS I TYPE B EROSION MATTING SHALL BE USED FOR A SHORT TERM DURATION (MAXIMUM OF 6 MONTHS) AND APPLIED ON ERODIBLE SLOPES 2:1 OR FLATTER.</p> <p>6.2. CLASS I TYPE B URBAN EROSION MATTING SHALL BE USED FOR A SHORT TERM DURATION (MAXIMUM 6 MONTHS) WHERE MOWING MAY BE ACCOMPLISHED WITHIN</p>	<p>6.3. TWO WEEKS AFTER INSTALLATION AND APPLIED ON SLOPES 2.5:1 OR FLATTER. CLASS II TYPE C EROSION MATTING SHALL BE USED FOR A LONG TERM DURATION (~ 3 YEARS) ON SLOPES 2:1 OR FLATTER AND IN ENVIRONMENTALLY SENSITIVE AREAS WHERE PLASTIC NETTING IS INAPPROPRIATE.</p> <p>6.4. MATTING SHALL BE INSTALLED AFTER ALL TOPSOILING, FERTILIZING AND SEEDING IS COMPLETE.</p> <p>6.5. THE MATS SHALL BE IN FIRM AND INTIMATE CONTACT WITH THE SOIL. IT SHALL BE INSTALLED AND ANCHORED PER THE MANUFACTURER'S RECOMMENDATION.</p> <p>6.6. AT THE TIME OF INSTALLATION, DOCUMENT THE MANUFACTURER AND MAT TYPE BY RETENTION OF MATERIAL LABELS AND MANUFACTURER'S INSTALLATION INSTRUCTIONS. RETAIN THIS DOCUMENTATION UNTIL THE SITE HAS STABILIZED.</p> <p>7. SEEDING AND MULCHING TECHNIQUES SHALL BE USED ON AREAS OF EXPOSED SOIL WHERE THE ESTABLISHMENT OF VEGETATION IS DESIRED. TEMPORARY SEEDING APPLIES TO DISTURBED AREAS THAT WILL NOT BE BROUGHT TO FINAL GRADE OR ON WHICH LAND-DISTURBING ACTIVITIES WILL NOT BE PERFORMED FOR A PERIOD OF GREATER THAN 7 CALENDAR DAYS, REQUIRING VEGETATIVE COVER FOR LESS THAN ONE YEAR. SEED AND MULCH SHALL BE UTILIZED THROUGHOUT THE DURATION OF CONSTRUCTION TO ESTABLISH TEMPORARY VEGETATION TO HELP REDUCE EROSION PER WDNR TECHNICAL STANDARDS 1059 AND 1058 RESPECTIVELY AS FOLLOWS:</p> <p>7.1. TEMPORARY SEEDING REQUIRES A SEEDBED OF LOOSE SOIL TO A MINIMUM OF 2 INCHES.</p> <p>7.2. FERTILIZER APPLICATION IS NOT GENERALLY REQUIRED FOR TEMPORARY SEEDING; HOWEVER, ANY APPLICATION OF FERTILIZER OR LIME SHALL BE BASED ON SOIL TESTING.</p> <p>7.3. THE SOIL SHALL HAVE A pH RANGE OF 5.5 TO 8.0.</p> <p>7.4. ALL SEED SHALL CONFORM TO THE REQUIREMENTS OF THE WISCONSIN STATE STATUTES AND THE ADMINISTRATIVE CODE CHAPTER ATCP 20.01 REGARDING NOXIOUS WEED SEED CONTENT AND LABELING.</p> <p>7.5. SEED SHALL NOT BE USED LATER THAN ONE YEAR AFTER THE TEST DATE ON THE LABEL.</p> <p>7.6. IN THE SPRING OR SUMMER, CONTRACTOR SHALL USE OATS APPLIED AT 131 LBS/ACRE FOR TEMPORARY SEEDING PURPOSES. IN THE FALL THE CONTRACTOR SHALL USE ANNUAL RYE GRASS APPLIED AT 80 LBS/ACRE OR WINTER WHEAT APPLIED AT 131 LBS/ACRE. THE CONTRACTOR SHALL USE STRAW MULCH APPLIED AT 1.5 TONS/ACRE. DORMANT SEED SHALL BE USED WHEN SOIL TEMPERATURE IS CONSISTENTLY BELOW 53 DEGREES FAHRENHEIT (TYPICALLY AFTER NOVEMBER 1, UNTIL SNOW COVER ANNUALLY). NEVER PLACE SEED ON TOP OF SNOW. IF COVER IS NEEDED AFTER SNOW FALL, CONTRACTOR MAY CHOOSE TO USE A DRY, NON-TOXIC TYPE B SOIL STABILIZER PER MANUFACTURER'S SPECIFICATIONS, AS REQUIRED BY WDNR.</p> <p>7.7. SEEDING SHALL NOT TAKE PLACE WHEN SOIL IS TOO WET.</p> <p>7.8. CONTRACTOR MAY CONSIDER WATERING TO HELP ESTABLISH THE SEED. WATER APPLICATION RATES SHALL BE CONTROLLED TO HELP PREVENT EROSION.</p> <p>7.9. INSPECT WEEKLY DURING THE GROWING SEASON UNTIL THE VEGETATION IS DENSELY ESTABLISHED OR THE SOIL IS LAID. REPAIR AND RESEED AREAS THAT HAVE EROSION DAMAGE AS NECESSARY.</p> <p>7.10. CONTRACTOR IS TO LIMIT VEHICLE TRAFFIC AND OTHER FORMS OF COMPACTION IN AREAS THAT ARE SEEDED AS MUCH AS POSSIBLE. RE-SEED DRIVEN OVER AREAS AS NEEDED.</p> <p>DEWATERING PLAN</p> <p>TO FACILITATE CONSTRUCTION AT THE PROJECT SITE, DEWATERING MAY TAKE PLACE BY THE SELECTED CONTRACTOR. CONTRACTOR TO FOLLOW THESE INSTRUCTION WHILE PERFORMING DEWATERING ACTIVITIES ON-SITE.</p> <p>NOTE: THESE INSTRUCTIONS DO NOT APPLY TO WATER BEING DISCHARGED DIRECTLY TO GROUNDWATER OR KARST FEATURES OR WELL DEWATERING SYSTEMS. CONTRACTOR SHALL COORDINATE ACCORDINGLY FOR OTHER DEWATERING ACTIVITIES AS DEEMED NECESSARY WITH THE WDNR.</p> <p>1. CONTRACTOR SHALL ENSURE THE DEWATERING PRACTICES CARRIED OUT MEET OR EXCEED WDNR TECHNICAL STANDARD 1061.</p> <p>2. A PAN OR OTHER CONTAINMENT DEVICE SHALL BE PLACED UNDERNEATH THE PUMP TO CAPTURE ANY SPILLS. OILS, GASOLINE, ETC. SHALL NOT BE STORED WITHIN WETLANDS, NEAR THE STORMWATER POND, OR OTHER ON-SITE WATER AREAS.</p> <p>3. A TYPE 2 GEOTEXTILE BAG THAT IS NO SMALLER THAN 100 SQUARE FEET; HAS A MAXIMUM APPARENT OPENING SIZE OF 0.212 mm; HAS A GRAB TENSILE STRENGTH OF 300 LBS; MULLEN BURST OF 580 PSI; PERMEABILITY OF 0.2 CM/SEC; FABRIC WEIGHT OF 12 OZ SHALL BE USED. THE GEOTEXTILE BAG AREA AND DOWNGRADE FLOW AREA SHALL CONSIST OF VEGETATED AND UNDISTURBED SOILS.</p> <p>4. POLYMER APPROVED BY THE WDNR MEETING TECHNICAL STANDARD 1051 MAY BE USED IN COMBINATION WITH THE DEWATERING BAG, IF THE DEWATERING BAG IS NOT DOING AN ADEQUATE JOB ALONE OF FILTERING SEDIMENTS. THE CONTRACTOR SHALL SUPPLY TOXICITY TESTING DATA TO THE WDNR BEFORE USE ON-SITE FOR WDNR APPROVAL. POLYMER SHALL NOT BE DIRECTLY APPLIED TO SURFACE WATER. CONTRACTOR SHALL OBTAIN THE POLYMER MATERIAL SAFETY DATA SHEETS (MSDS) FOR THE SELECTED POLYMER, MANUFACTURER'S INFORMATION AND WDNR USE RESTRICTIONS (SEE TECHNICAL STANDARD 1051) AND KEEP ALL THIS INFORMATION ON-SITE. CONTRACTOR SHALL ADHERE TO MANUFACTURER AND WDNR'S APPLICATION RATES FOR THE POLYMER. THE APPLICATION RATE SHALL NOT EXCEED THE WDNR USE RESTRICTION, EVEN IF THIS IS THE RECOMMENDED RATE BY THE MANUFACTURER. THE CONTRACTOR SHALL TAKE STEPS TO ENSURE THAT THE POLYMER IS NOT SPILLED. THE MANUFACTURER'S RECOMMENDED CLEANUP PROCEDURES SHALL BE FOLLOWED IN THE EVENT OF A SPILL.</p> <p>5. A TARP MAY BE UTILIZED UNDERNEATH THE TYPE 2 GEOTEXTILE BAG AND JUST DOWN SLOPE OF THE BAG TO DISCOURAGE EROSION AND SCOUR.</p> <p>6. A FLOATING SUCTION HOSE OR OTHER FLOTATION METHOD SHALL BE UTILIZED WHEN PUMPING FROM AN AREA WITH STANDING WATER TO AVOID SUCKING SEDIMENT FROM GRADE.</p> <p>7. IF TURBID WATER IS LEAVING THE GEOTEXTILE BAG, THE CONTRACTOR SHALL SHUT OFF THE PUMP TO ALLOW SEDIMENTS TO SETTLE INTO THE BAG. CONTRACTOR SHALL FOLLOW THE MANUFACTURER'S SPECIFICATIONS FOR DETERMINING THE SEDIMENT CAPACITY OF THE GEOTEXTILE BAG USING GOOD COMMON SENSE. SEDIMENT LEVELS CONTAINED IN THE BAG SHALL BE MONITORED TO MEASURE THE LOSS OF STORAGE CAPACITY OVER TIME. THE CONTRACTOR SHALL PROPERLY DISPOSE OF THE GEOTEXTILE BAG IN A WASTE RECEPTACLE ONCE IT IS NO LONGER USED.</p> <p>8. DURING DEWATERING ACTIVITIES THE CONTRACTOR SHALL MONITOR DEWATERING PRACTICES AND KEEP A LOG OF THE FOLLOWING:</p> <p>8.1. DISCHARGE DURATION AND SPECIFIED PUMPING RATE.</p> <p>8.2. OBSERVED WATER TABLE AT TIME OF DEWATERING.</p> <p>8.3. MAINTENANCE ACTIVITIES.</p> <p>8.4. NAME AND QUANTITY OF POLYMER USED AND PRODUCT TYPE.</p> <p>8.5. APPLICATION RATE OF POLYMER IN POUNDS/ACRE FEET OF WATER.</p> <p>8.6. DATE AND TIME APPLIED.</p> <p>8.7. WEATHER CONDITIONS DURING APPLICATION.</p> <p>8.8. METHOD OF APPLICATION.</p> <p>THIS LOG SHALL BE KEPT ON SITE FOR WDNR REGULATORY REVIEW. COPIES OF THIS DOCUMENTATION SHOULD BE KEPT IN THE THE CONTRACTOR'S MONITORING LOG AND MADE AVAILABLE UPON REQUEST.</p>
PAVING NOTES:			
<p>1. ALL DIMENSIONS ARE TO FLANGE OF CURB UNLESS OTHERWISE NOTED.</p> <p>2. ACCESSIBLE ROUTES TO HAVE CROSS SLOPES NO GREATER THAN 2% AND RUNNING SLOPES NO GREATER THAN 5%.</p> <p>3. THE CONTRACTOR SHALL APPLY PAVEMENT MARKING AND INSTALL SIGNS IN ACCORDANCE WITH WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, STATE OF WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION LATEST EDITION, AND STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STANDARD DETAIL DRAWINGS.</p> <p>4. FURNISH WOOD POSTS IN ACCORDANCE WITH SECTION 634 OF WISDOT STANDARD SPECIFICATIONS TO SUPPORT NEW SIGNS.</p>	EROSION CONTROL SEQUENCE OF CONSTRUCTION		
CONCRETE CONSTRUCTION:			
<p>1. EXPANSION JOINTS SHALL BE CONSTRUCTED AT ALL RADIUS POINTS IN CURB & GUTTER ADJACENT TO ASPHALTIC PAVEMENT.</p> <p>2. CONCRETE CURB & GUTTER JOINT SPACING SHALL BE 10 FEET UNLESS OTHERWISE APPROVED BY ENGINEER.</p>	<p>1. AFTER BIDS ARE RECEIVED AND A GENERAL CONTRACTOR IS SELECTED, A PRE-CONSTRUCTION MEETING SHALL TAKE PLACE WITH ALL RELEVANT PARTIES IN ATTENDANCE.</p> <p>2. OBTAIN PLAN APPROVAL FROM THE MUNICIPALITY AND ALL APPLICABLE PERMITS, INCLUDING EROSION CONTROL PERMIT.</p> <p>3. SILT FENCE, INLET PROTECTION, AND/OR TRACKING PAD(S) SHALL BE INSTALLED AS SHOWN ON THE PLANS AND INSPECTED PRIOR TO COMMENCING ANY LAND DISTURBING ACTIVITIES.</p> <p>4. CLEAR AND GRUB AREAS AS SHOWN ON THE THE PLANS OR AS DIRECTED.</p> <p>5. REMOVE TOPSOIL IN THE GRADING AREA AS SHOWN ON THE PLANS. STOCKPILE EXCESS TOPSOIL AS DIRECTED, SEPARATE FROM THE REST OF THE EXCAVATED MATERIAL.</p> <p>6. PERFORM REMOVALS AS DIRECTED ON THE PLANS AND IN CONJUNCTION WITH ENGINEER'S APPROVAL. EARTHWORK AND UTILITY INSTALLATION WILL PROCEED IN CONJUNCTION WITH ONE ANOTHER. BASE AGGREGATE MATERIALS WILL BE BROUGHT IN AS NEEDED AND STORED WITHIN THE PROPOSED ROADWAY, PLACED AND COMPACTED. PROVIDE POSITIVE DRAINAGE THROUGHOUT THE PROJECT SITE TO PROPOSED STRUCTURES OR TEMPORARY PRACTICES AS FIELD CONDITIONS DICTATE.</p> <p>7. PROPOSED CATCH BASIN INLET PROTECTION SHALL BE INSTALLED AND MAINTAINED AS THE STRUCTURES ARE CONSTRUCTED AND BROUGHT TO GRADE.</p> <p>8. CURB AND GUTTER AND PAVEMENT OPERATIONS WILL FOLLOW ALONG WITH DISTURBED AREA RESTORATION AS INDICATED ON THESE PLANS BY PLACING TOPSOIL, SEED, FERTILIZER, MULCH AND/OR EROSION MATTING AS PER THESE APPROVED PLANS AND SPECIFICATIONS. SLOPES SHALL BE STABILIZED WITH EROSION MAT AS DIRECTED.</p> <p>9. FINAL SITE STABILIZATION IS ANTICIPATED FOLLOWING THE COMPLETION OF ALL GRADING AND PAVING ACTIVITIES. TEMPORARY STABILIZATION IS NOT ANTICIPATED GIVEN THE PROJECT SCHEDULE AND POTENTIAL GROWING SEASON BEING IN CONCERT. IN THE EVENT AREAS OF DISTURBANCE ARE LEFT INACTIVE FOR A PERIOD OF 7 CALENDAR DAYS OR THE PROJECT SCHEDULE AND GROWING SEASON ARE NOT IN CONCERT, THEN TEMPORARY SEEDING SHALL BE REQUIRED ON SAID INACTIVE AREAS.</p> <p>10. AFTER ENTIRE PROJECT SITE IS STABILIZED AND APPROVAL FROM THE VILLAGE OR THEIR REPRESENTATIVE, IS ATTAINED, REMOVE TEMPORARY EROSION CONTROL MEASURES, INCLUDING SILT FENCE, INLET PROTECTION OR OTHER MEASURES INCLUDING ANY ACCUMULATED SEDIMENT, AND DISPOSE OF MATERIALS PROPERLY.</p> <p>11. CONTRACTOR SHALL FILE NOTICE OF TERMINATION WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES.</p> <p>12. FINAL "AS-BUILT" SURVEYS ARE TO BE CONDUCTED AND FINAL DOCUMENTS FORWARDED TO THE VILLAGE.</p> <p>13. WATERING THE SEEDED/RESTORED AREAS IS NOT ANTICIPATED WITH THIS CONSTRUCTION SCHEDULE; HOWEVER, IF ADEQUATE RAIN IS NOT EXPERIENCED WITHIN ONE WEEK AFTER INITIAL SEED GERMINATION AT ANY POINT DURING THE CONSTRUCTION PROCESS, WATER SHALL BE TRUCKED IN AND APPLIED TO THE SEEDED/RESTORED AREAS ONCE PER WEEK, OR IN ACCORDANCE WITH THE SEED PRODUCER'S SPECIFICATIONS. IF CONSTRUCTION SCHEDULE SHOULD CHANGE SIGNIFICANTLY, THIS PLAN NARRATIVE WILL</p>		
ASPHALT CONSTRUCTION:			
<p>1. A SAWED JOINT IS REQUIRED WHERE NEW ASPHALTIC CONCRETE SURFACES MEET EXISTING ASPHALTIC CONCRETE SURFACES.</p> <p>2. TACK COAT HAS BEEN ESTIMATED AT AN APPLICATION RATE OF 0.050 – 0.070 GALLONS PER SQUARE YARD AND SHALL BE PLACED BETWEEN LAYERS OF ASPHALTIC PAVEMENT, INCIDENTAL TO PAVING ITEM.</p>			
UTILITY NOTES:			
<p>1. UTILITY STRUCTURE STATIONS AND OFFSETS ARE CENTER OF STRUCTURE. FLARED END SECTION STATIONS AND OFFSETS ARE TO END OF PIPE (EOP).</p> <p>2. CATCH BASIN/INLET RIM ELEVATIONS ARE TO FLANGE OF CURB.</p> <p>3. ALL UTILITY EXCAVATIONS SHALL BE BACKFILLED WITH ¾" CRUSHED LIMESTONE AND MECHANICALLY COMPACTED UNLESS OTHERWISE NOTED ON THE PLANS.</p> <p>4. ALL EXISTING GAS MAINS AND OTHER UTILITY SERVICES SHALL BE EXPOSED BY THE CONTRACTOR IN ADVANCE OF PROPOSED UTILITY INSTALLATION TO VERIFY HORIZONTAL AND VERTICAL GAS FACILITIES LOCATIONS.</p>			
EROSION CONTROL NOTES:			
<p>1. CONTRACTOR MUST CONFORM WITH ANY STATE, FEDERAL, AND LOCAL PERMITS, ORDINANCES AND/OR REGULATIONS AND WITH THE CONDITIONS INCLUDED IN THIS PLAN SET. EROSION CONTROL MEASURES SHALL BE INSTALLED, MAINTAINED AND REMOVED IN CONFORMANCE WITH THE MUNICIPALITY'S ORDINANCES AND THE WISCONSIN DNR STORM WATER MANAGEMENT TECHNICAL STANDARDS, WITH THE DETAILS AND NOTES LISTED IN THIS PLAN SET, AND ADJUSTED TO FIT FIELD CONDITIONS.</p> <p>2. ANY DEVIATION FROM THESE NOTES BY THE CONTRACTOR SHALL BE APPROVED BY WDNR, MUNICIPALITY, AND ENGINEER.</p> <p>3. REMOVAL OF EROSION CONTROL DEVICES AND MOBILIZATION FOR SUCH REMOVAL SHALL BE INCIDENTAL TO THE RESPECTIVE DEVICES INSTALLATION BID ITEMS.</p> <p>4. EROSION CONTROL MEASURES AS SPECIFIED IN THESE PLANS SHALL BE INSTALLED PRIOR TO ANY GRADING OR LAND DISTURBANCE. EROSION CONTROL MEASURES SHALL NOT BE REMOVED UNTIL THE AREAS THEY SERVE HAVE ESTABLISHED VEGETATIVE COVER (DEFINED AS 80% VEGETATIVE GROWTH OR AS OTHERWISE AUTHORIZED BY REGULATORY AGENCIES). APPLY APPROPRIATE SOIL CONSERVATION TECHNIQUES TO PROTECT PROJECT AREA AND ADJACENT LANDS. ALL EROSION CONTROL MEASURES SHALL BE ADJUSTED TO MEET FIELD CONDITIONS AT THE TIME OF CONSTRUCTION. EROSION CONTROL MEASURES INCLUDE BUT MAY NOT BE LIMITED TO SILT FENCE, WATTLE, INLET PROTECTION, MULCHING, EROSION MATTING, AND RAPID GROWTH VEGETATION.</p> <p>5. TEMPORARY SEED SHALL BE USED WHEN LAND DISTURBING CONSTRUCTION ACTIVITIES HAVE TEMPORARILY CEASED AND WILL NOT RESUME FOR A PERIOD EXCEEDING 7</p>			<p>16745 W. Bluemound Road Brookfield, WI 53005-9398 (262) 781-1000 rasmith.com creativity BEYOND ENGINEERING raSmith 2023 PAVING PROGRAM GENERAL NOTES DRAFT NOT FOR CONSTRUCTION</p> <p>© COPYRIGHT 2023 R.A. Smith, Inc.</p> <p>DATE: 05/02/2023</p> <p>SCALE: N/A</p> <p>JOB NO. 2232100</p> <p>PROJECT MANAGER: JASON FEUCHT, P.E.</p> <p>DESIGNED BY: RJW</p> <p>CHECKED BY: JMF</p> <p>SHEET NUMBER</p> <p>02</p>



TYPICAL SECTION
CITADEL TERRACE, 86TH STREET



PROPOSED TYPICAL SECTION
KENNSINGTON SQUARE ROAD



EXISTING TYPICAL SECTION
KENNSINGTON SQUARE ROAD

EARTHWORK VOLUMES (CY)		
(ALL VOLUMES ASSUME A 1:1 RATIO. NO SHRINK OR SWELL FACTORS ASSOCIATED WITH VOLUMES)		
	CUT	FILL
ROAD CONSTRUCTION	3,185	0

DESCRIPTION

DATE

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VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
TYPICAL SECTIONS

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DATE: 05/02/2023

SCALE: 1" = 5'

JOB NO. 2232100

PROJECT MANAGER:
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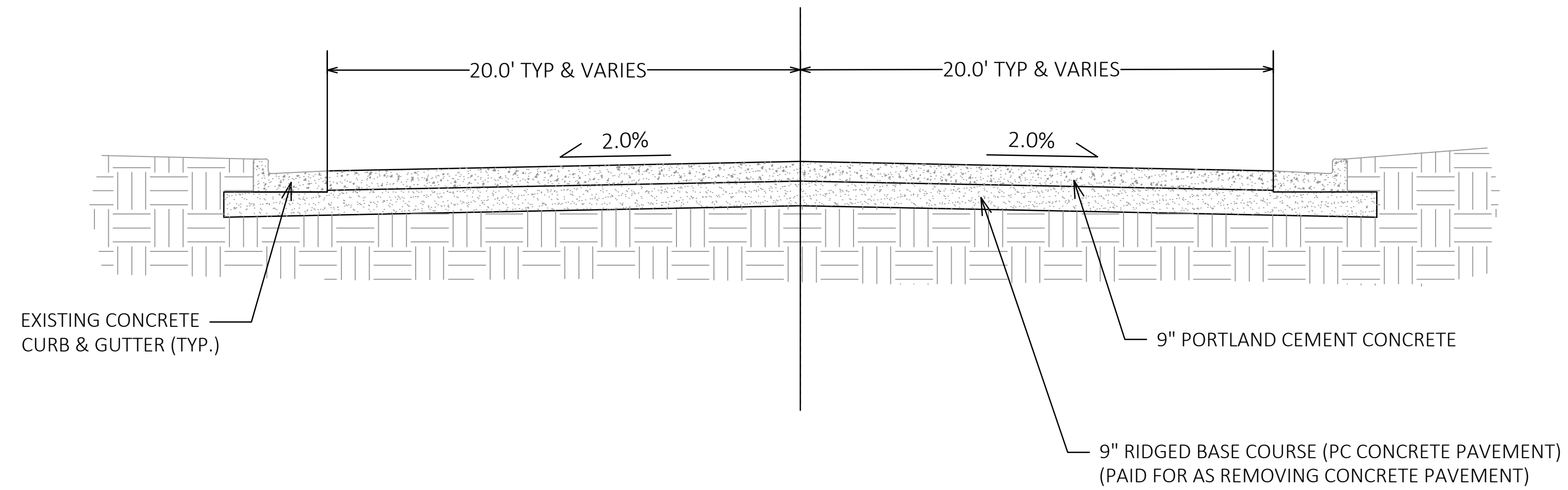
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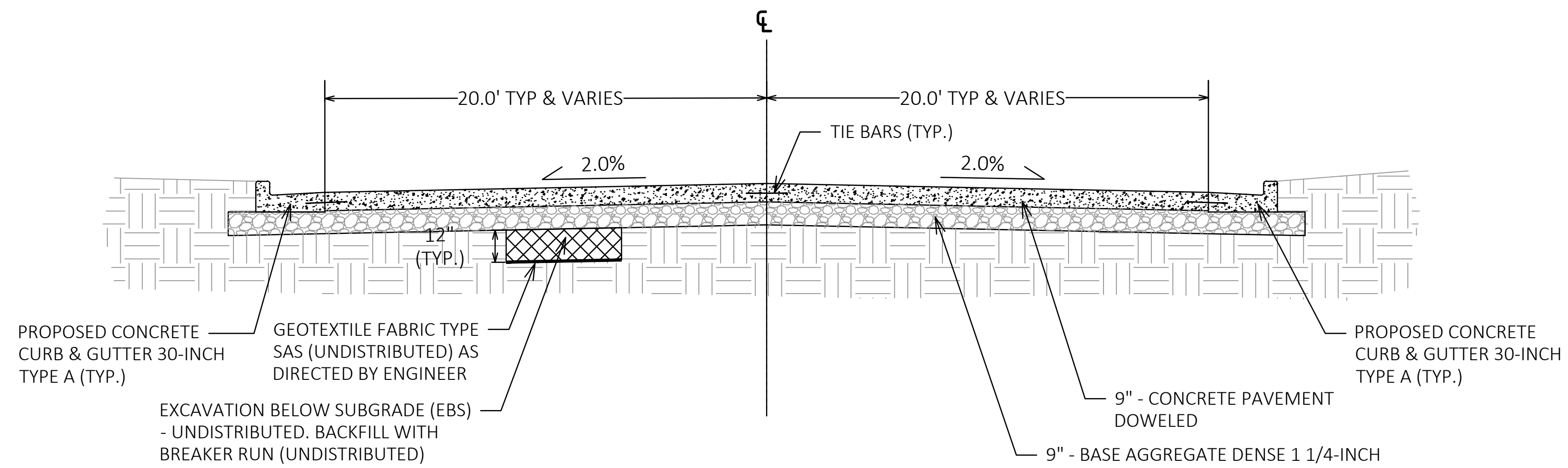
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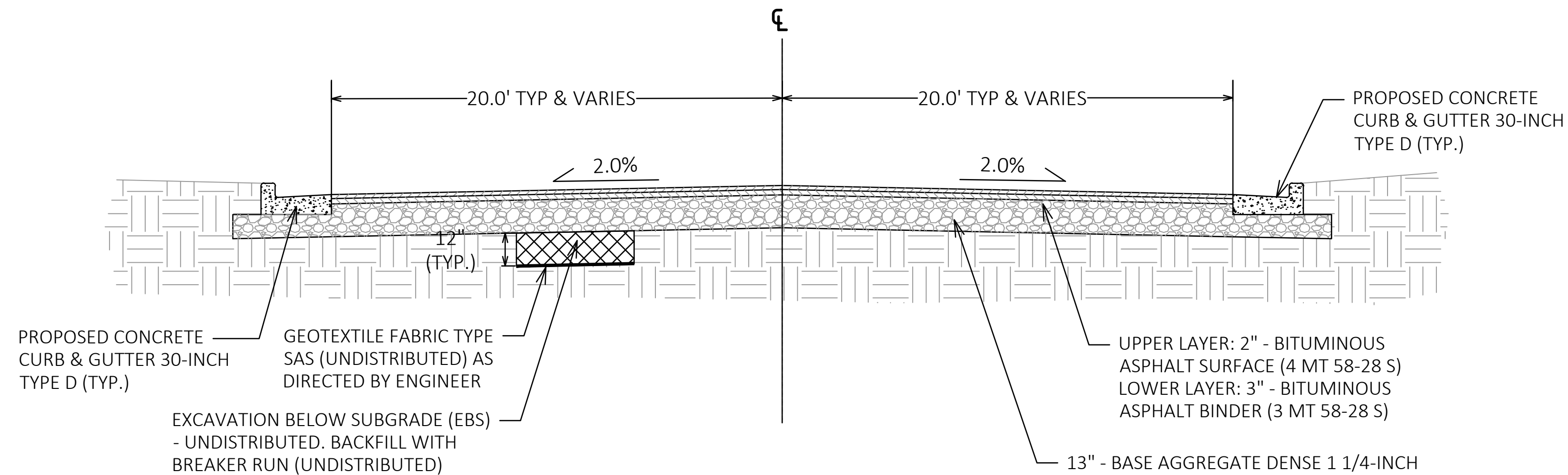
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TYPICAL EXISTING SECTION
RAYNE ROAD



TYPICAL CONCRETE SECTION
RAYNE ROAD



TYPICAL ALTERNATE ASPHALT SECTION
RAYNE ROAD

DATE	DESCRIPTION

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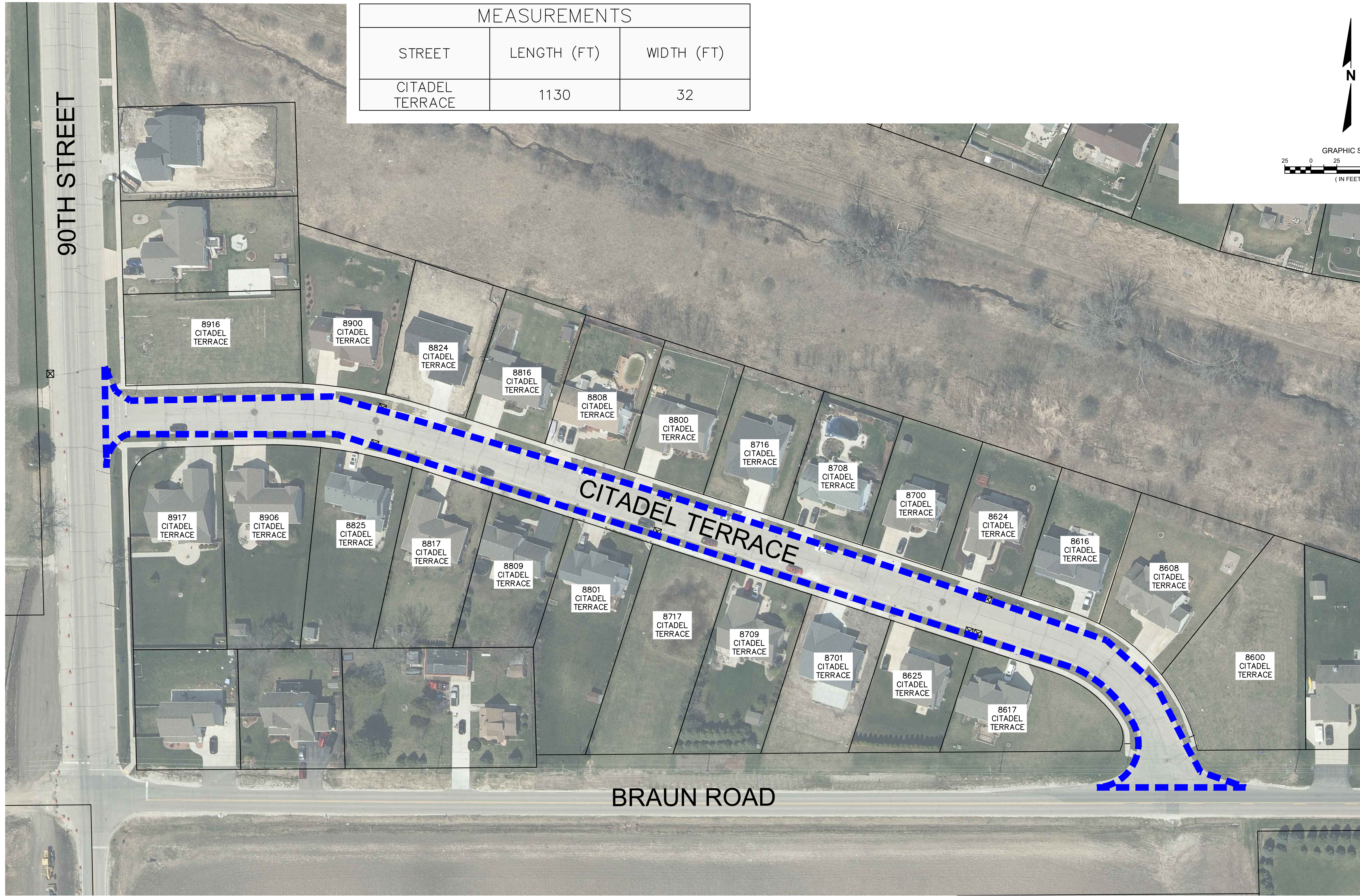
VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
TYPICAL SECTIONS

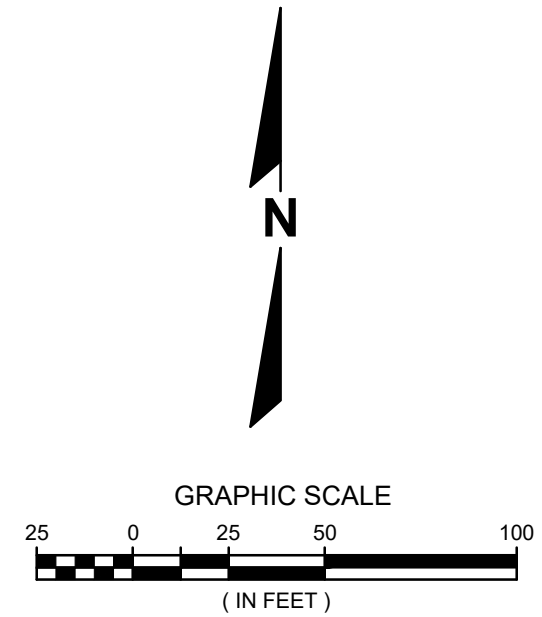
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DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
04

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MEASUREMENTS		
STREET	LENGTH (FT)	WIDTH (FT)
CITADEL TERRACE	1130	32



DATE	DESCRIPTION

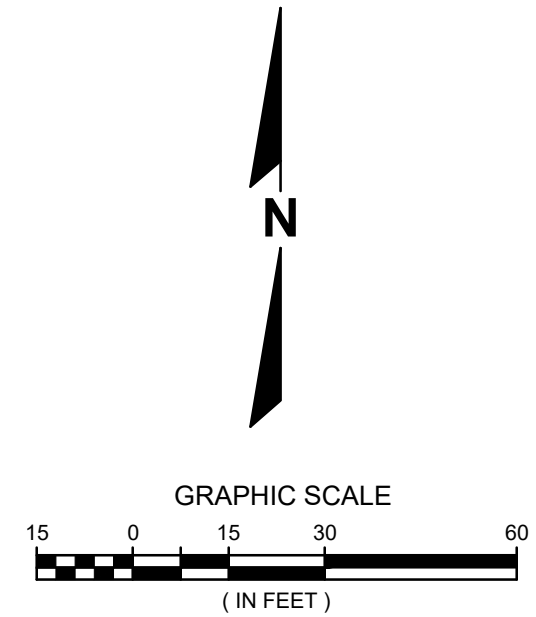
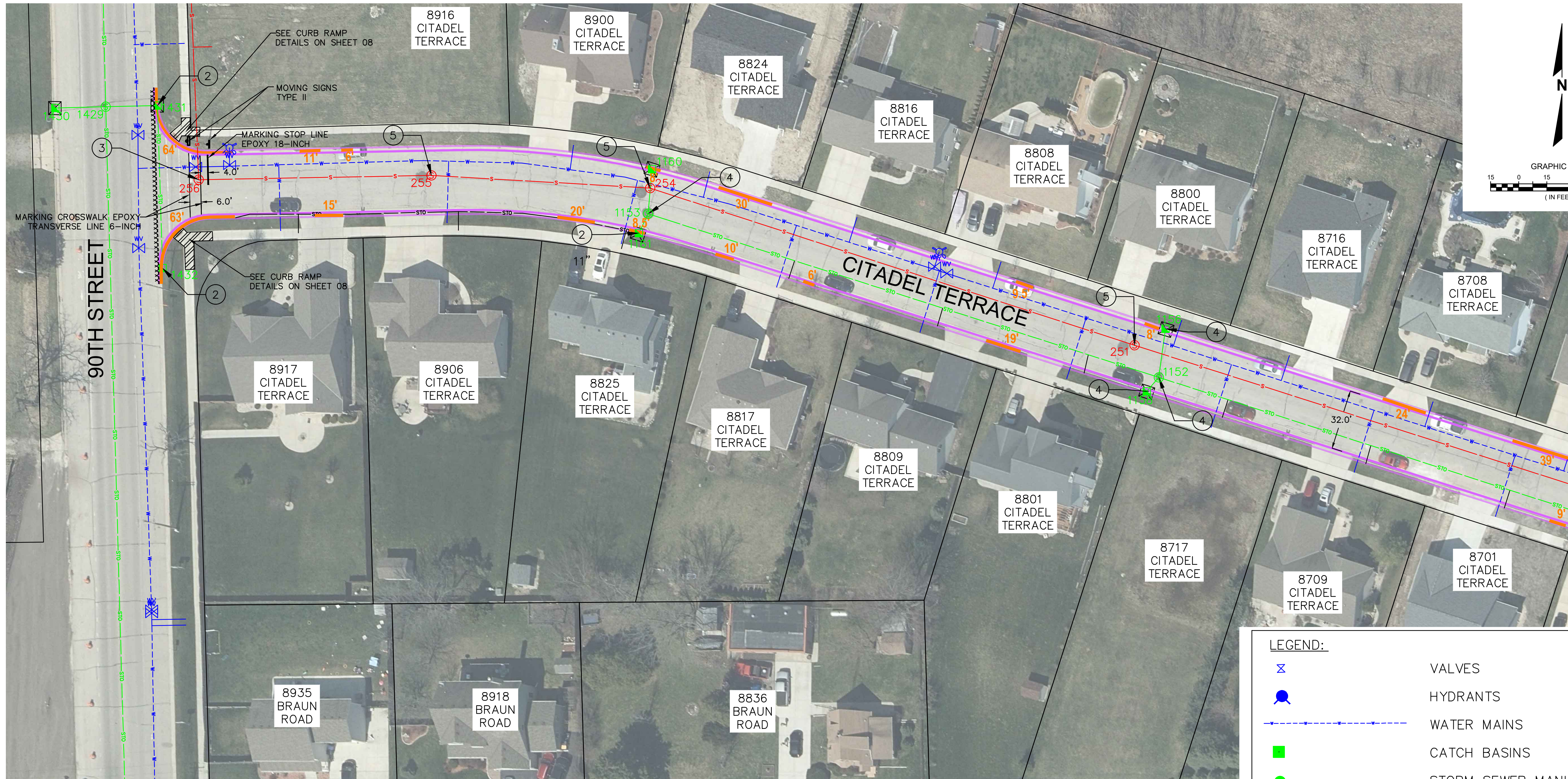

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VILLAGE OF STURTEVANT
 RACINE COUNTY, WISCONSIN
 2023 PAVING PROGRAM
 SECTION 1
 CITADEL TERRACE-OVERVIEW

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DESIGNED BY: RJW
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SHEET NUMBER
05

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(SEE SHEET 07)

NOTE:

INLET PROTECTION, TYPE B REQUIRED AT ALL STORM MANHOLES WITH OPEN GRATES. INLET PROTECTION, TYPE C REQUIRED AT ALL CURBSIDE INLETS.

THE ROADWAY MATCH POINTS WILL BE MARKED BY ENGINEER PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL COORDINATE WITH RACINE WATER UTILITY FOR WATER VALVE BOX ADJUSTMENTS.

REMOVED FRAMES AND GRATES/COVERS SHALL BE DELIVERED TO THE OWNER.

PROTECT DRIVEWAY AND LANDSCAPE WALLS/ITEMS DURING CONSTRUCTION.

CONTRACTOR'S RESPONSIBILITY TO STAKE CENTERLINE OF EXISTING PAVEMENT.

ALL CONCRETE CURB AND GUTTER SPOT REPLACEMENT SHALL MATCH THE EXISTING ADJACENT CURB AND GUTTER TYPE. CURB AND GUTTER TO BE MARKED IN THE FIELD BY THE ENGINEER.

CONTRACTOR SHALL MAINTAIN ACCESS TO DRIVEWAYS AT ALL TIMES.

STRUCTURE LEGEND

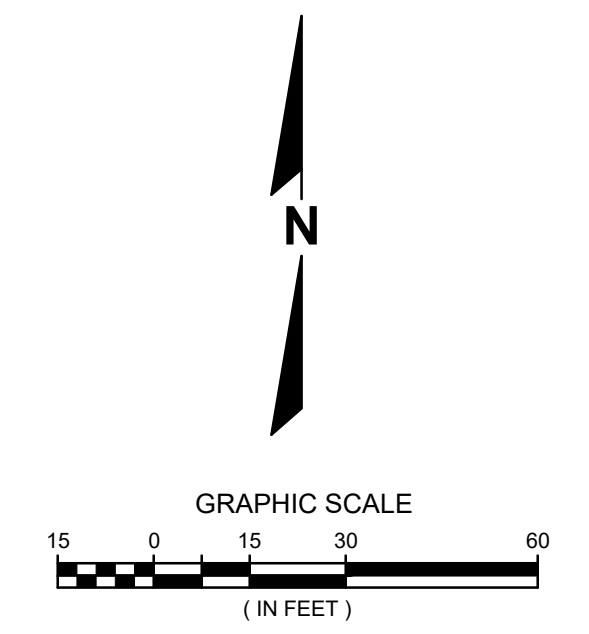
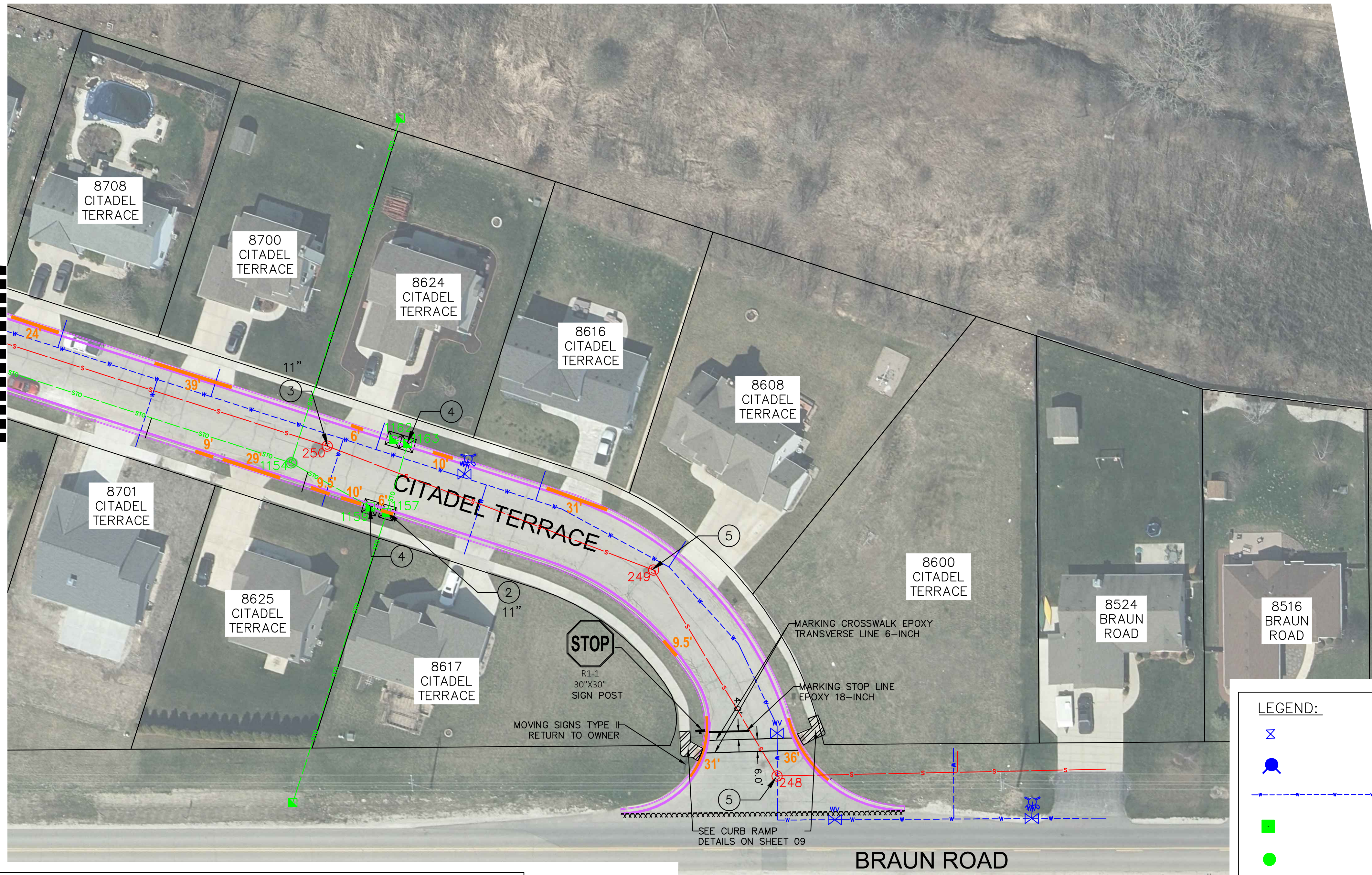
- | | | | |
|---------|---|---|--------------------------------|
| ①
X" | ADJUSTING STORM MANHOLES (CHIMNEY DEPTH) | ④ | TUCKPOINTING STORM STRUCTURES |
| ②
X" | ADJUSTING INLETS (CHIMNEY DEPTH) | ⑤ | TUCKPOINTING SANITARY MANHOLES |
| ③
X" | ADJUSTING SANITARY MANHOLES (CHIMNEY DEPTH) | | |

LEGEND:

- | | |
|--|---|
| | VALVES |
| | HYDRANTS |
| | WATER MAINS |
| | CATCH BASINS |
| | STORM SEWER MANHOLES |
| | STORM SEWER |
| | SANITARY MANHOLE |
| | SANITARY SEWER |
| | EXISTING CURB & GUTTER |
| | SAW CUT PAVEMENT |
| | CONCRETE CURB AND GUTTER SPOT REPLACEMENT (LF) |
| | SIDEWALK REMOVAL (PAID AS REMOVING CONCRETE PAVEMENT) |
| | INLET PROTECTION TYPE B (SEE DETAIL) |
| | INLET PROTECTION TYPE C (SEE DETAIL) |

DESCRIPTION	
DATE	
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VILLAGE OF STURTEVANT RACINE COUNTY, WISCONSIN	2023 PAVING PROGRAM SECTION 1 CITADEL TERRACE
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DATE: 05/02/2023	
SCALE: 1" = 30'	
JOB NO. 2232100	
PROJECT MANAGER: JASON FEUCHT, P.E.	
DESIGNED BY: RJW	
CHECKED BY: JMF	
SHEET NUMBER	
06	

(SEE SHEET 06)



NOTE:

INLET PROTECTION, TYPE B REQUIRED AT ALL STORM MANHOLES WITH OPEN GRATES. INLET PROTECTION, TYPE C REQUIRED AT ALL CURBSIDE INLETS.

THE ROADWAY MATCH POINTS WILL BE MARKED BY ENGINEER PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL COORDINATE WITH RACINE WATER UTILITY FOR WATER VALVE BOX ADJUSTMENTS.

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PROTECT DRIVEWAY AND LANDSCAPE WALLS/ITEMS DURING CONSTRUCTION.

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CONTRACTOR SHALL MAINTAIN ACCESS TO DRIVEWAYS AT ALL TIMES.

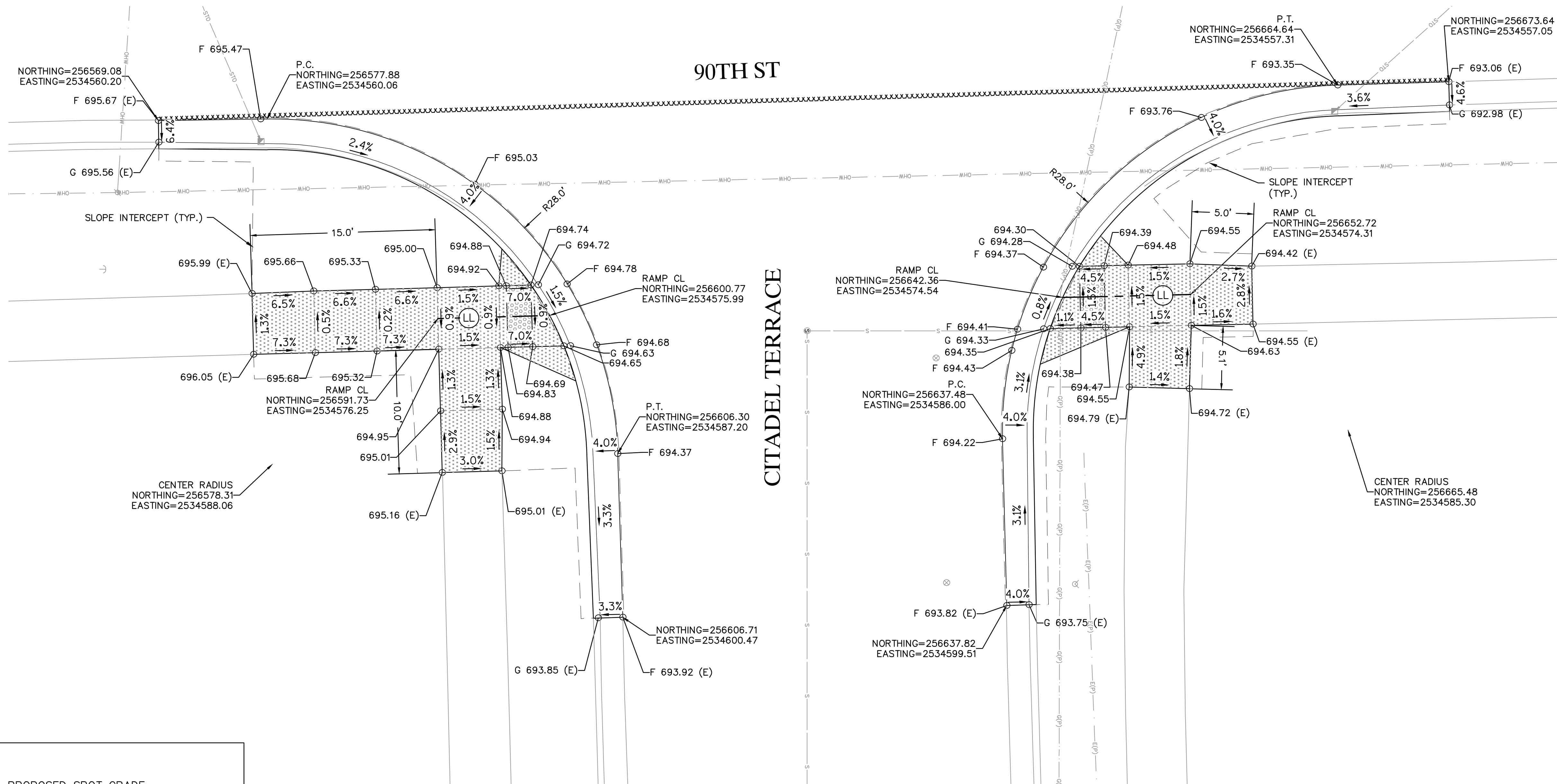
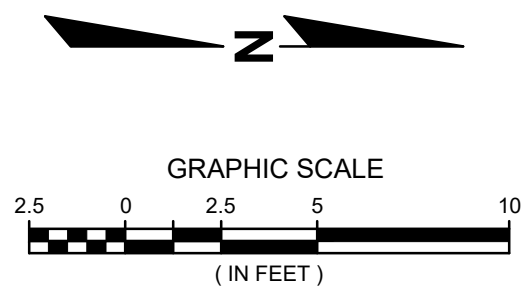
STRUCTURE LEGEND

- ① X" ADJUSTING STORM MANHOLES (CHIMNEY DEPTH)
- ② X" ADJUSTING INLETS (CHIMNEY DEPTH)
- ③ X" ADJUSTING SANITARY MANHOLES (CHIMNEY DEPTH)
- ④ TUCKPOINTING STORM STRUCTURES
- ⑤ TUCKPOINTING SANITARY MANHOLES

LEGEND:

- VALVES
- HYDRANTS
- WATER MAINS
- CATCH BASINS
- STORM SEWER MANHOLES
- STORM SEWER
- SANITARY MANHOLE
- SANITARY SEWER
- EXISTING CURB & GUTTER
- SAWCUT PAVEMENT
- CONCRETE CURB AND GUTTER SPOT REPLACEMENT (LF)
- SIDEWALK REMOVAL (PAID AS REMOVING CONCRETE PAVEMENT)
- INLET PROTECTION TYPE B (SEE DETAIL)
- INLET PROTECTION TYPE C (SEE DETAIL)

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SHEET NUMBER					07



LEGEND	
XXX.XX	PROPOSED SPOT GRADE
XXX.XX (E)	EXISTING SPOT GRADE
F XXX.XX	PROPOSED FLANGE SPOT GRADE
G XXX.XX	PROPOSED GUTTER SPOT GRADE
$\frac{1.5\%}{\rightarrow}$	PROPOSED SLOPE/FLOW DIRECTION
(LL)	LANDING AREA
[Dotted Pattern]	PROPOSED CONCRETE SIDEWALK 4-INCH
[Grid Pattern]	PROPOSED DETECTABLE WARNING FIELD

DATE	DESCRIPTION

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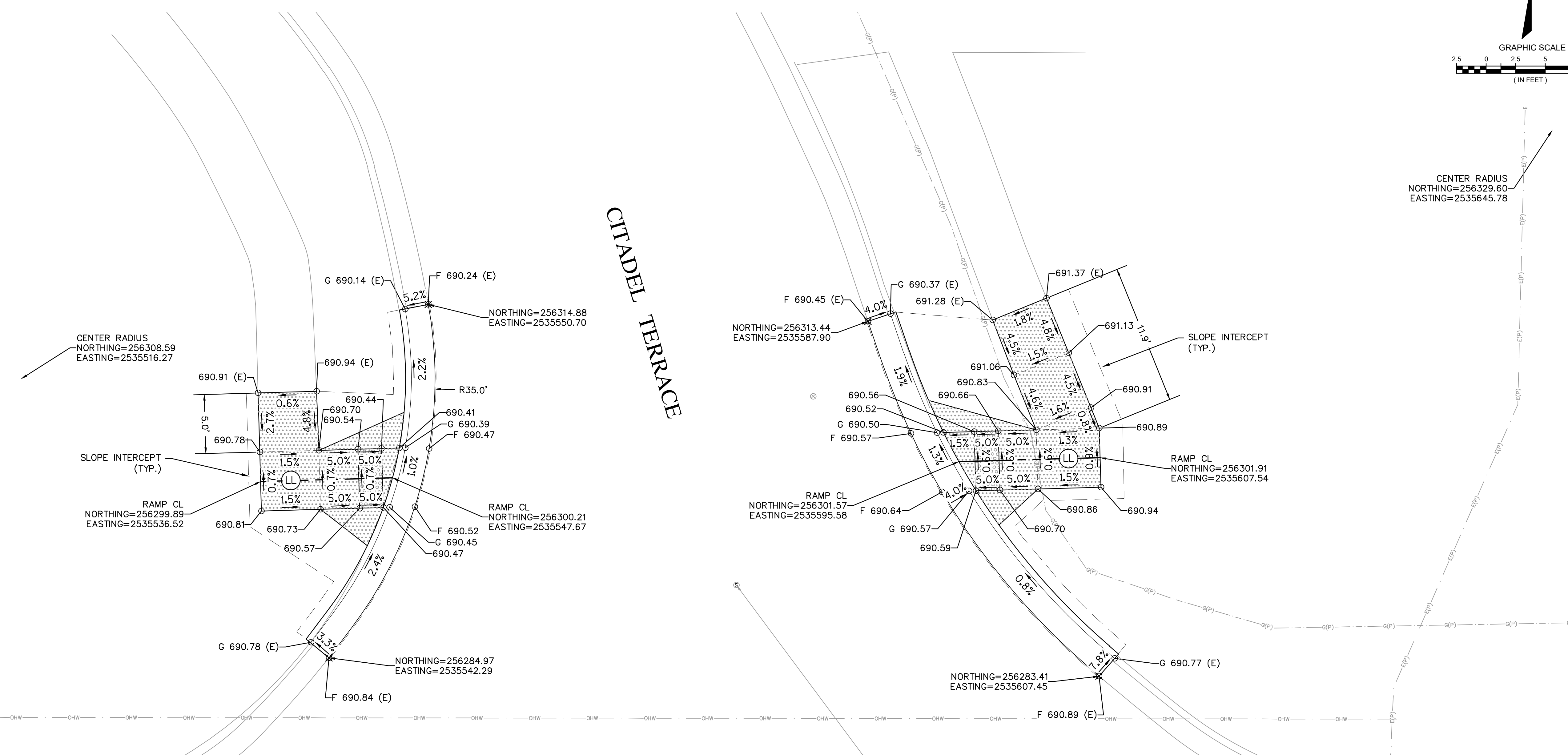
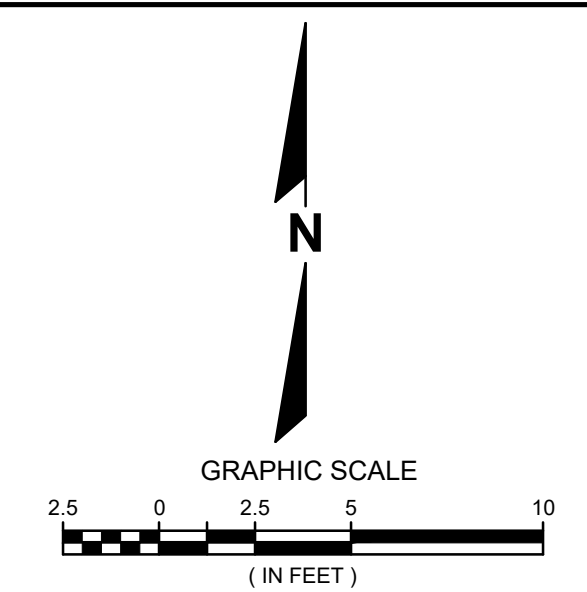
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VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
SECTION 1
CITADEL TERRACE - CURB RAMP DETAILS

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SCALE: 1"=5'
JOB NO. 2232100
PROJECT MANAGER: JASON FEUCHT, P.E.
DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
08



LEGEND	
XXX.XX	PROPOSED SPOT GRADE
XXX.XX (E)	EXISTING SPOT GRADE
F XXX.XX	PROPOSED FLANGE SPOT GRADE
G XXX.XX	PROPOSED GUTTER SPOT GRADE
1.5%	PROPOSED SLOPE/FLOW DIRECTION
Ⓛ	LANDING AREA
[Pattern]	PROPOSED CONCRETE SIDEWALK 4-INCH
[Pattern]	PROPOSED DETECTABLE WARNING FIELD

DATE	DESCRIPTION

16745 W. Bluemound Road
Brookfield, WI 53005-5938
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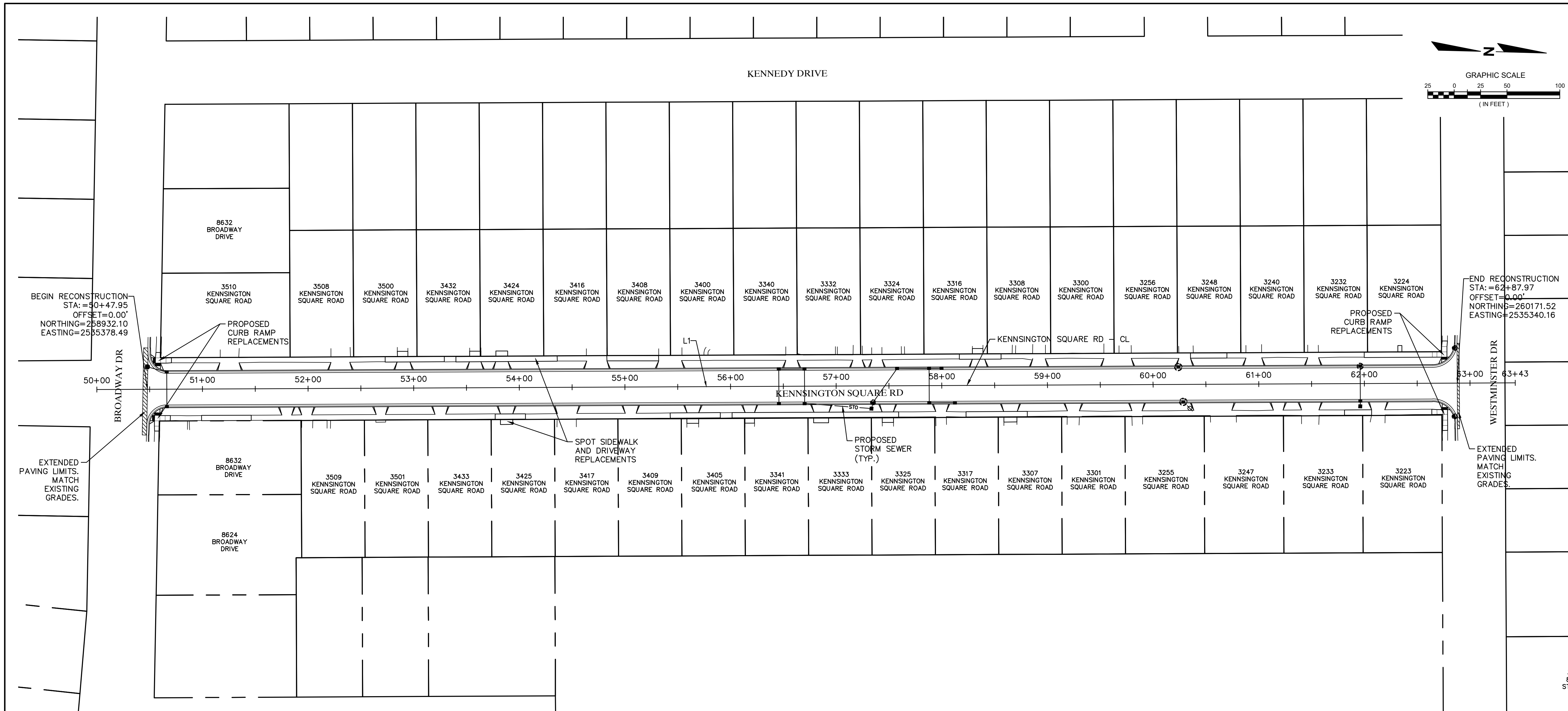
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VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN
2023 PAVING PROGRAM
SECTION 1
CITADEL TERRACE - CURB RAMP DETAILS

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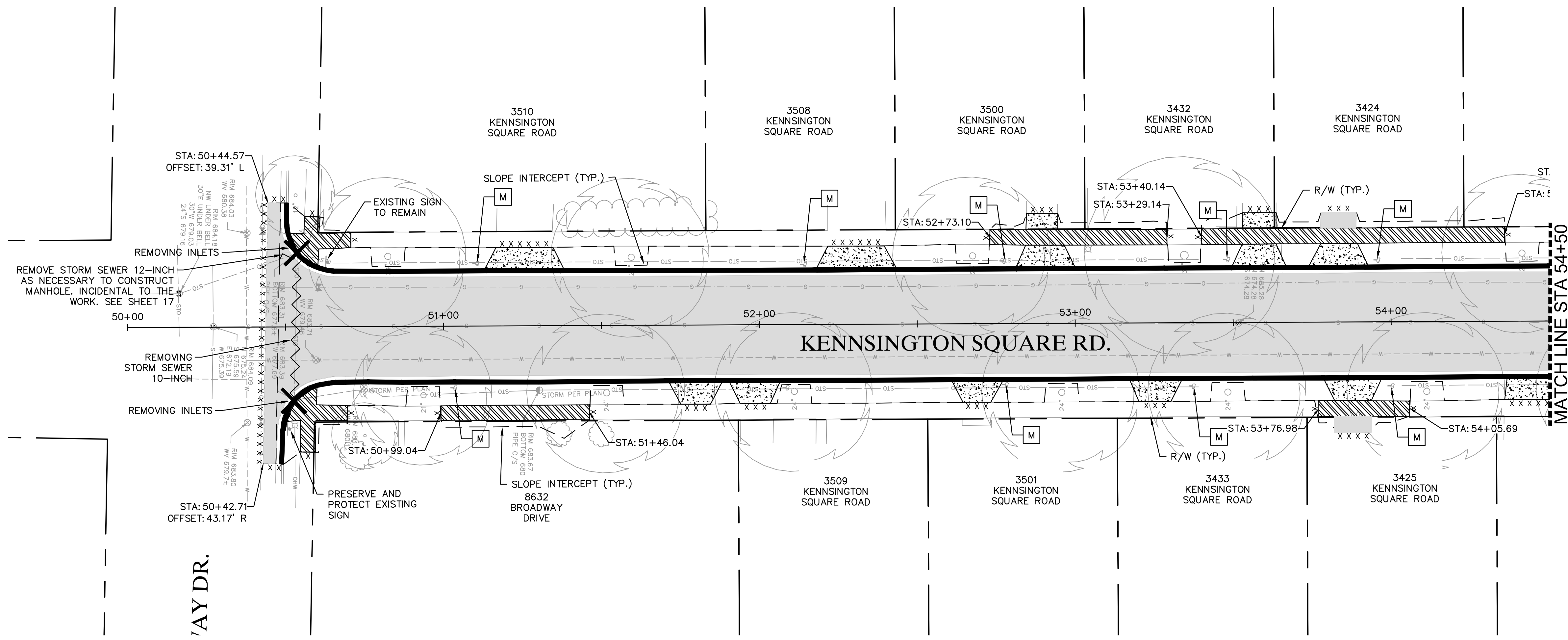
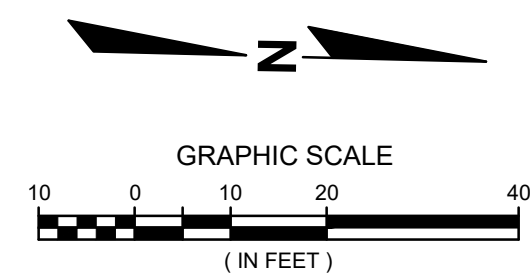
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PROJECT MANAGER: JASON FEUCHT, P.E.
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09



Kensington Square - CL																
Number	PI (STA)	PI (Y)	PI (X)	DELTA	DEGREE	TANGENT	LENGTH	EXTERNAL	RADIUS	PC (STA)	PT (STA)	DB	DA	END PI (STA)	END PI (Y)	END PI (X)
L1	50+00.00	258884.17	2535379.97										N1°46'17"W	63+42.89	260226.42	2535338.46

DESCRIPTION	
DATE	
<p>16745 W. Bluemound Road Brookfield, WI 53005-5938 (262) 781-1000 rasmith.com</p> <p>rasmith CREATIVITY BEYOND ENGINEERING</p> <p>Brookfield, WI Milwaukee, WI Appleton, WI Madison, WI Cedarburg, WI Naperville, IL Irvine, CA</p>	
<p>VILLAGE OF STURTEVANT RACINE COUNTY, WISCONSIN 2023 PAVING PROGRAM SECTION 2 KENNINGTON SQUARE RD - OVERVIEW</p>	
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<p>DATE: 05/02/2023</p>	
<p>SCALE: 1" = 50'</p>	
<p>JOB NO. 2232100</p>	
<p>PROJECT MANAGER: JASON FEUCHT, P.E.</p>	
<p>DESIGNED BY: RJW</p>	
<p>CHECKED BY: JMF</p>	
<p>SHEET NUMBER</p>	
<p>10</p>	

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LEGEND

	REMOVING CONCRETE PAVEMENT		REMOVING CURB & GUTTER
	ASPHALTIC PAVEMENT REMOVAL (PAID AS EXCAVATION COMMON)		PAVEMENT SAWCUTTING
	SIDEWALK REMOVAL (PAID AS REMOVING CONCRETE PAVEMENT)		REMOVAL ITEM AS DESIGNATED
	MAILBOX RELOCATION		REMOVING STORM SEWER

NOTES:

- ALL REMOVED SIGNS SHALL BE DELIVERED TO THE OWNER. COST INCIDENTAL TO REMOVING SIGNS.
- PROTECT ALL EXISTING FENCES, TREES, LANDSCAPING AND GROUND LIGHTS UNLESS DIRECTED BY ENGINEER TO REMOVE. COST INCIDENTAL TO THE WORK.

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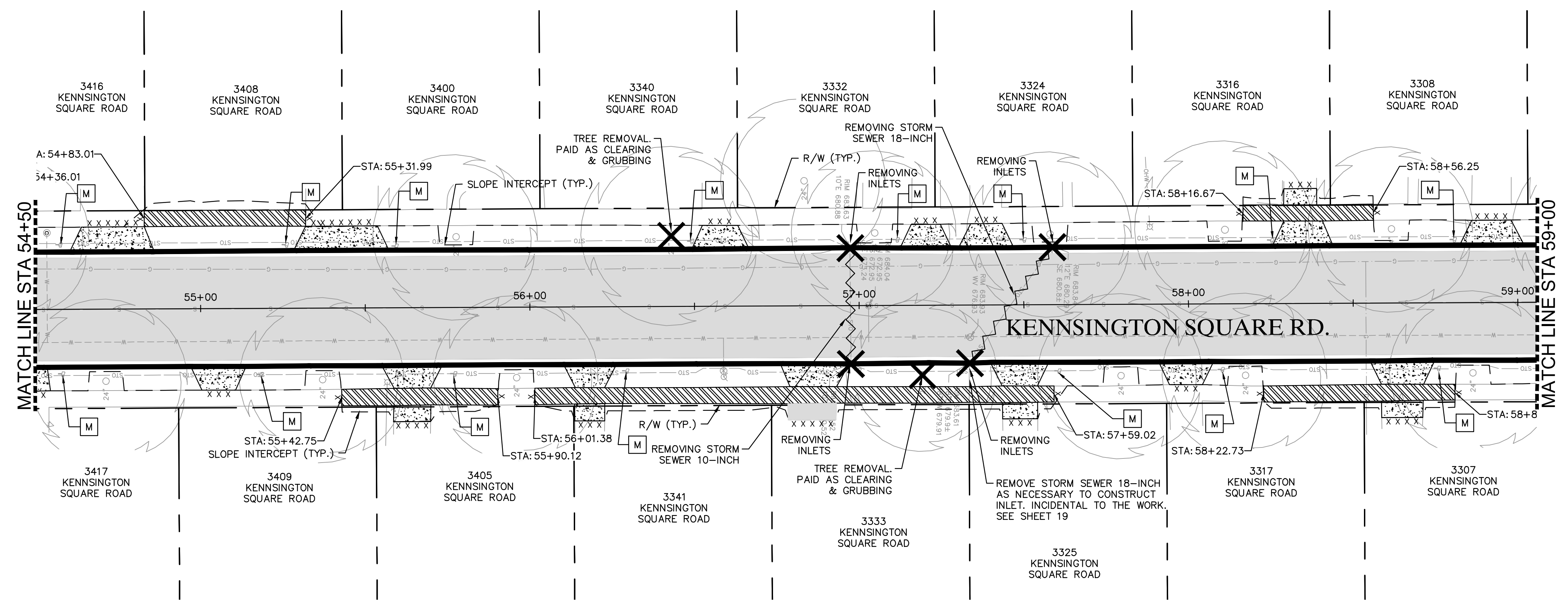
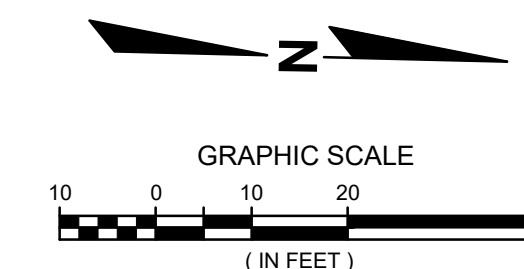
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RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
SECTION 2

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KENNINGTON SQUARE RD - REMOVALS

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DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
11



LEGEND

	REMOVING CONCRETE PAVEMENT		REMOVING CURB & GUTTER
	ASPHALTIC PAVEMENT REMOVAL (PAID AS EXCAVATION COMMON)		PAVEMENT SAWCUTTING
	SIDEWALK REMOVAL (PAID AS REMOVING CONCRETE PAVEMENT)		REMOVAL ITEM AS DESIGNATED
	MAILBOX RELOCATION		REMOVING STORM SEWER

NOTES:

- ALL REMOVED SIGNS SHALL BE DELIVERED TO THE OWNER. COST INCIDENTAL TO REMOVING SIGNS.
- PROTECT ALL EXISTING FENCES, TREES, LANDSCAPING AND GROUND LIGHTS UNLESS DIRECTED BY ENGINEER TO REMOVE. COST INCIDENTAL TO THE WORK.

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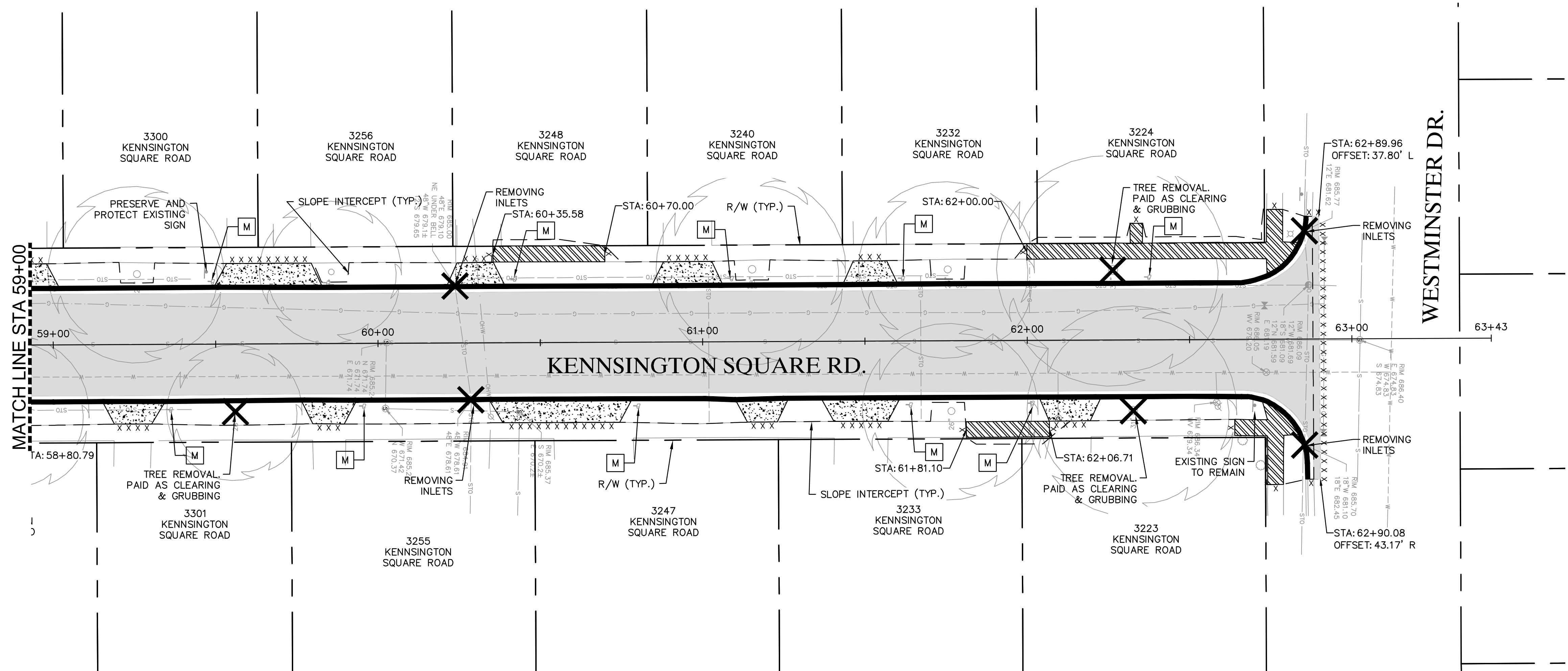
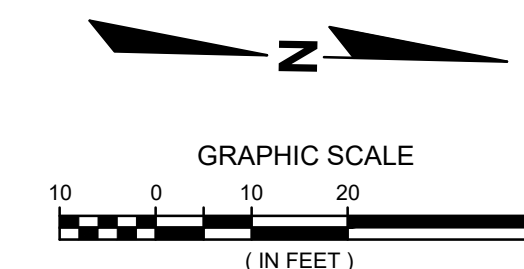
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RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
SECTION 2
KENNSINGTON SQUARE RD - REMOVALS

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JOB NO. 2232100
PROJECT MANAGER: JASON FEUCHT, P.E.
DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
12

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LEGEND

	REMOVING CONCRETE PAVEMENT		REMOVING CURB & GUTTER
	ASPHALTIC PAVEMENT REMOVAL (PAID AS EXCAVATION COMMON)		PAVEMENT SAWCUTTING
	SIDEWALK REMOVAL (PAID AS REMOVING CONCRETE PAVEMENT)		REMOVAL ITEM AS DESIGNATED
	MAILBOX RELOCATION		REMOVING STORM SEWER

NOTES:

- ALL REMOVED SIGNS SHALL BE DELIVERED TO THE OWNER. COST INCIDENTAL TO REMOVING SIGNS.
- PROTECT ALL EXISTING FENCES, TREES, LANDSCAPING AND GROUND LIGHTS UNLESS DIRECTED BY ENGINEER TO REMOVE. COST INCIDENTAL TO THE WORK.

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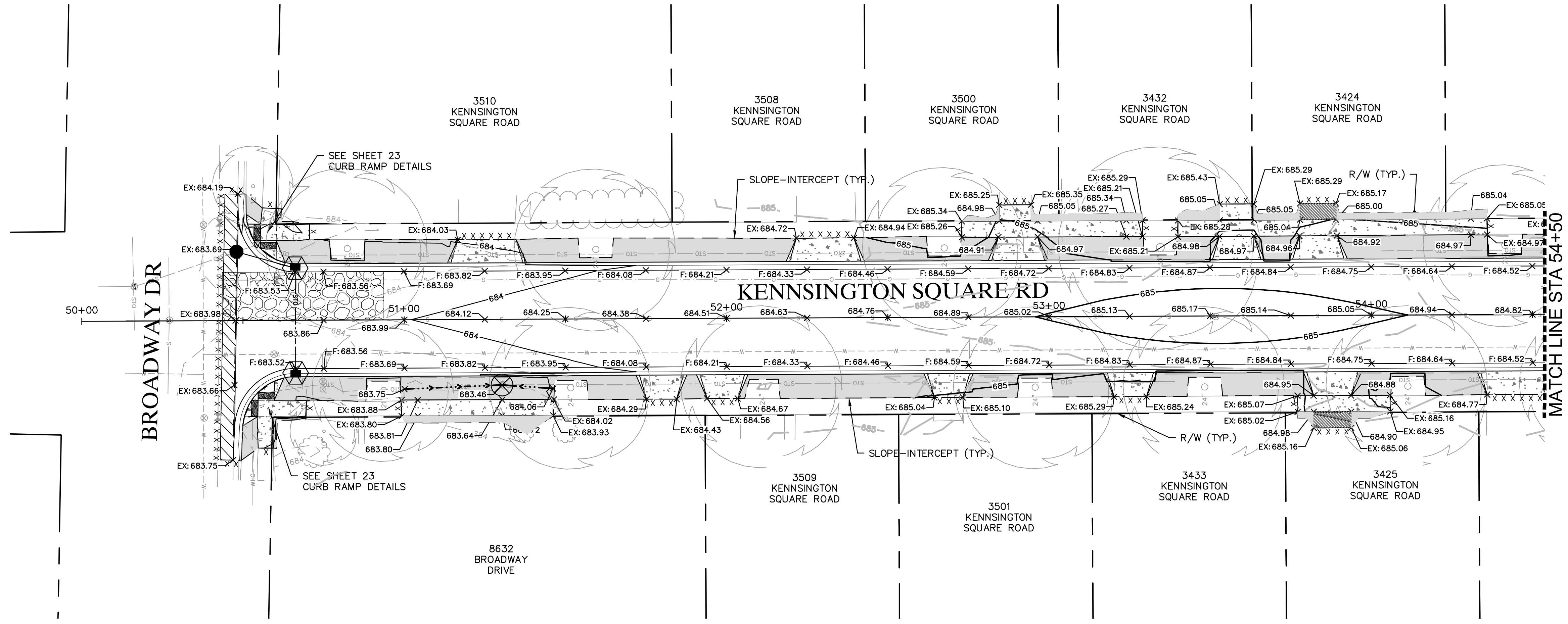
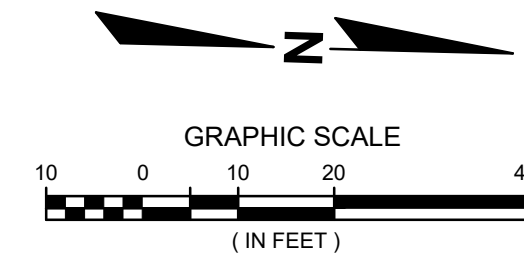
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VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
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KENNINGSINGTON SQUARE RD - REMOVALS

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DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
13



LEGEND

	INLET PROTECTION (TYPES A&C) (SEE DETAIL)		TRACKING PAD
	INLET PROTECTION (TYPES A&D) (SEE DETAIL)		PROPOSED SWALE/FLOW LINE
	INLET PROTECTION (TYPE B) (SEE DETAIL)		EXISTING CONTOUR
	PROPOSED SPOT ELEVATION		PROPOSED CONTOUR
	PROPOSED FLANGE GRADE SPOT ELEVATION		EROSION MAT URBAN CLASS I TYPE B (DOUBLE NETTED)
	EXISTING SPOT ELEVATION		

DATE	DESCRIPTION

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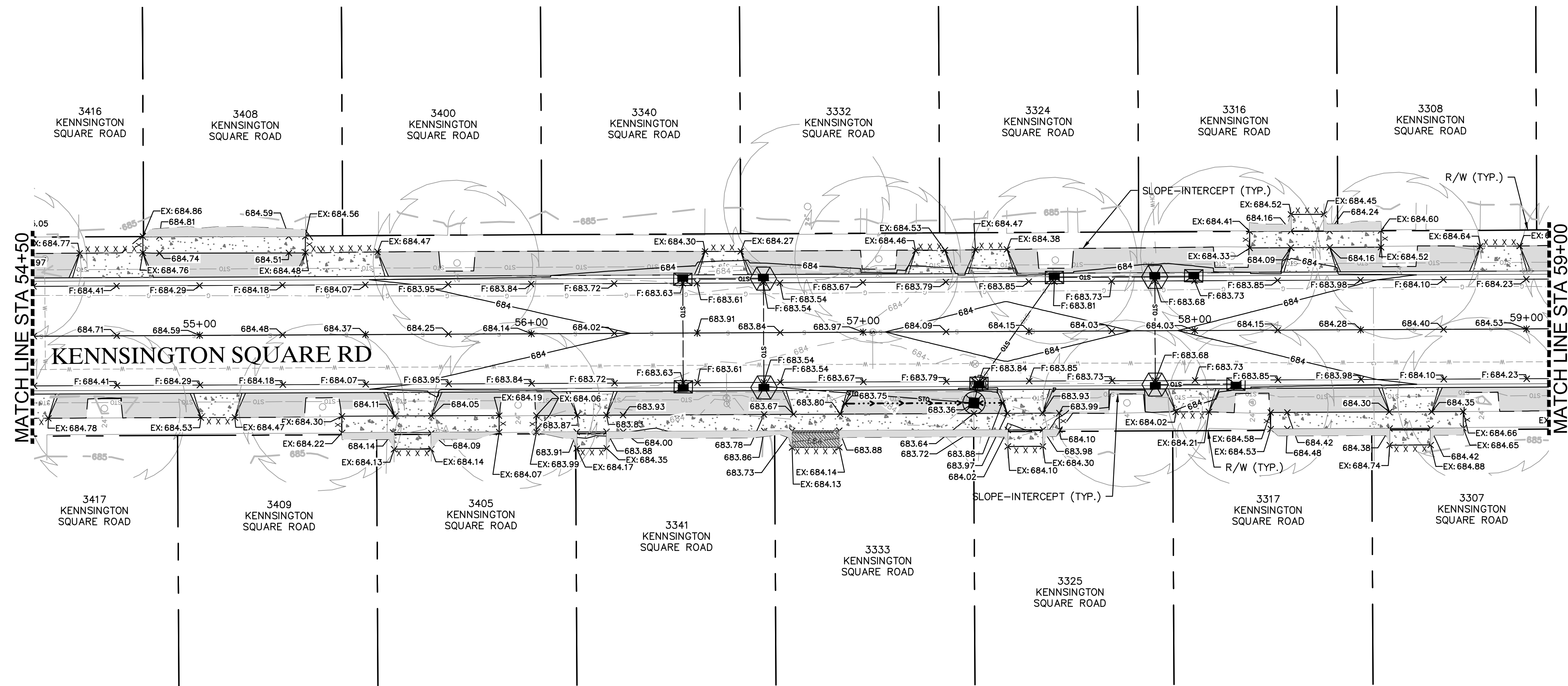
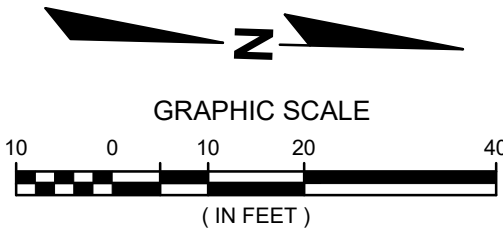
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VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
SECTION 2 - KENSINGTON SQUARE RD
GRADING & EROSION CONTROL PLAN

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PROJECT MANAGER: JASON FEUCHT, P.E.
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SHEET NUMBER 14



LEGEND

	INLET PROTECTION (TYPES A&C) (SEE DETAIL)		TRACKING PAD
	INLET PROTECTION (TYPES A&D) (SEE DETAIL)		PROPOSED SWALE/FLOW LINE
	INLET PROTECTION (TYPE B) (SEE DETAIL)		EXISTING CONTOUR
	PROPOSED SPOT ELEVATION		PROPOSED CONTOUR
	PROPOSED FLANGE GRADE SPOT ELEVATION		EROSION MAT URBAN CLASS I TYPE B (DOUBLE NETTED)
	EXISTING SPOT ELEVATION		

DESCRIPTION

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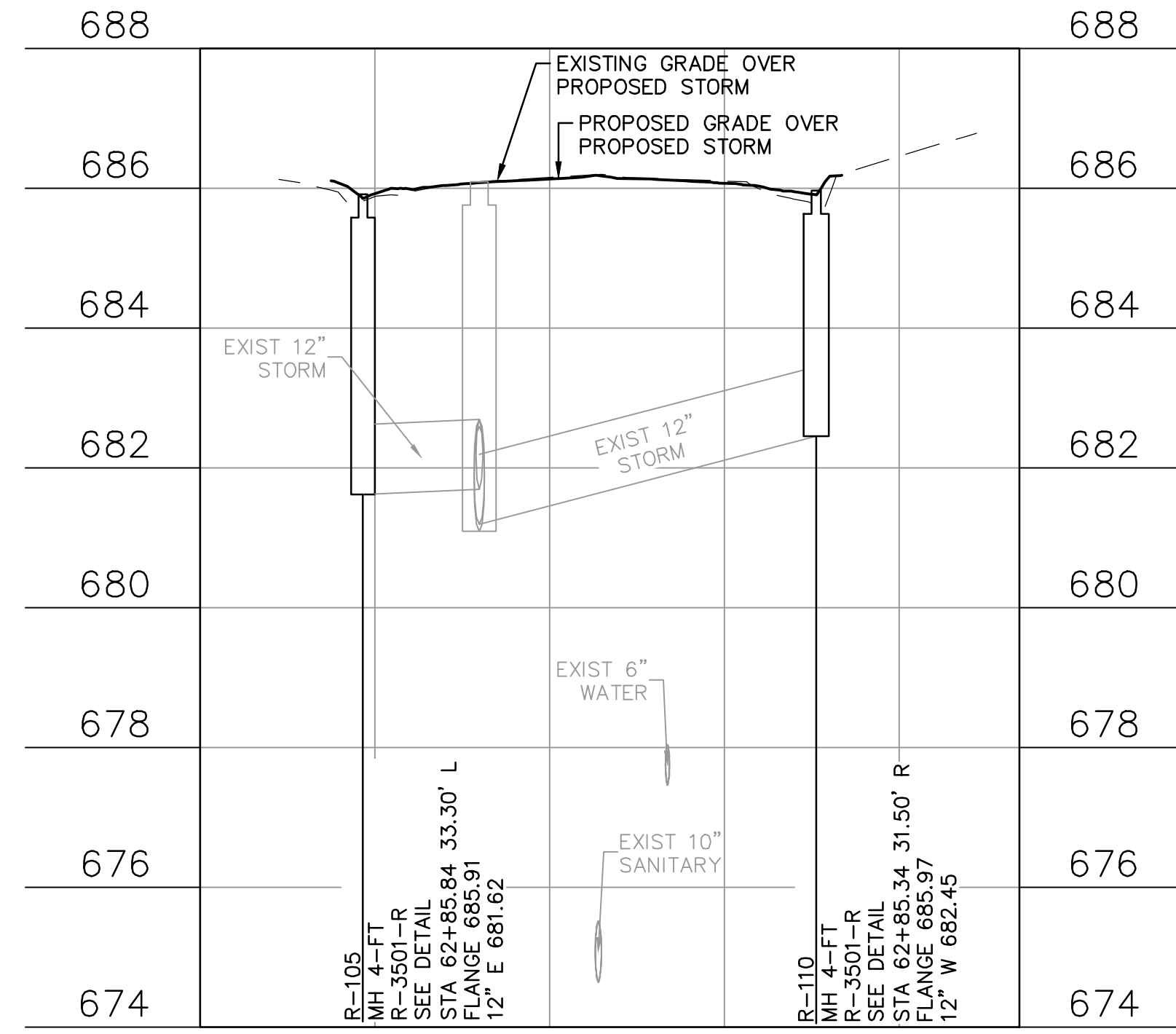
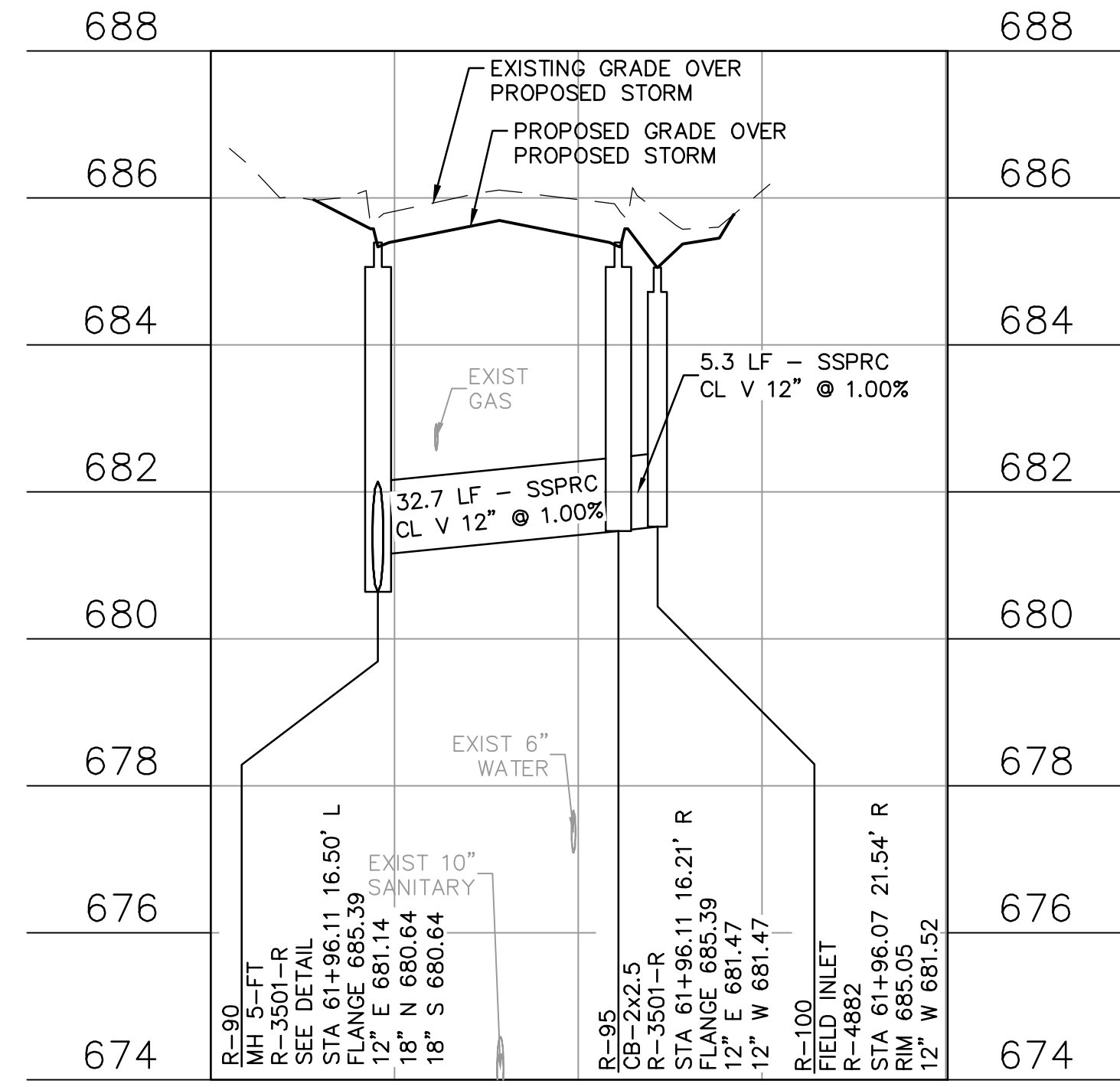
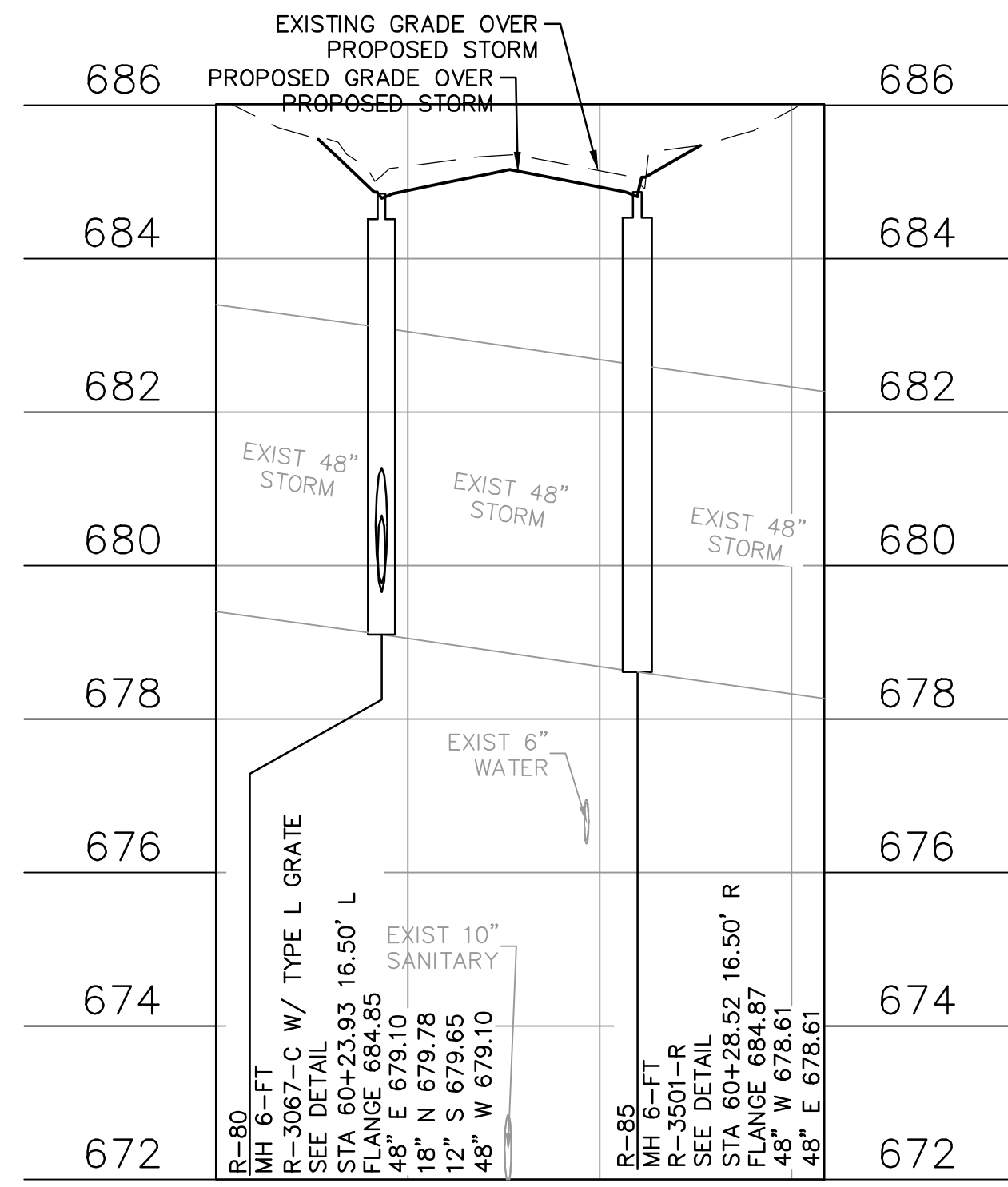
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VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
SECTION 2 - KENNINGSON SQUARE RD
GRADING & EROSION CONTROL PLAN

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JOB NO. 2232100
PROJECT MANAGER: JASON FEUCHT, P.E.
DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
15



DESCRIPTION

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RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
KENNINGTON SQUARE RD
PLAN & PROFILE

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JOB NO. 2232100

PROJECT MANAGER:
JASON FEUCHT, P.E.

DESIGNED BY: RJW

CHECKED BY: JMF

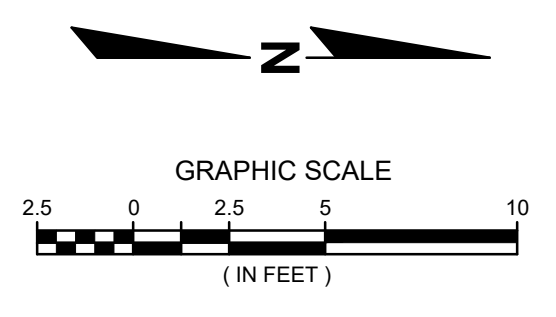
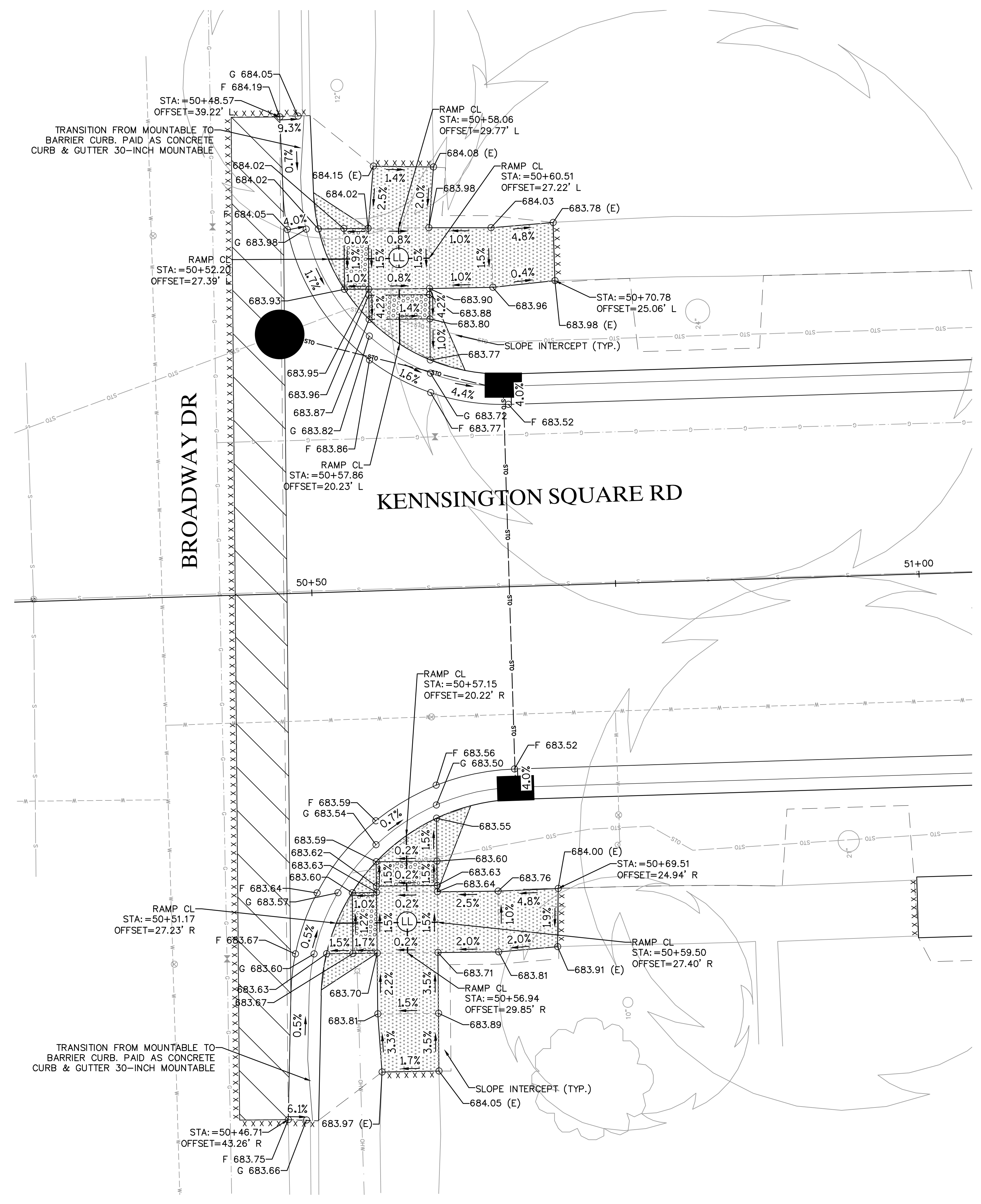
SHEET NUMBER

22

LEGEND	
XXX.XX	PROPOSED SPOT GRADE
XXX.XX (E)	EXISTING SPOT GRADE
F XXX.XX	PROPOSED FLANGE SPOT GRADE
G XXX.XX	PROPOSED GUTTER SPOT GRADE
1.5%	PROPOSED SLOPE/FLOW DIRECTION
⊕	LANDING AREA
[Pattern]	PROPOSED CONCRETE SIDEWALK 4-INCH
[Pattern]	PROPOSED DETECTABLE WARNING FIELD

NOTES:

- ALL CURB RAMP OPENINGS SHOULD BE 5.5' ALONG B.O.C. TO ACCOMMODATE ROOM FOR DETECTABLE WARNING FIELDS.
- CONTRACTOR TO COORDINATE WITH PROPERTY OWNERS FOR REMOVAL OF LANDSCAPE ITEMS NEAR BACK OF WALK TO ALLOW FOR NEW SIDEWALK CONSTRUCTION.
- ALL EXISTING CURB AND SIDEWALK SHALL BE SAWCUT FULL DEPTH AND TO THE NEAREST JOINT.
- ALL NEW CONCRETE CURB & GUTTER SHALL BE 30" TYPE D CURB UNLESS OTHERWISE NOTED.
- ALL SPOT GRADES ARE FINISHED GRADES. CURB GRADES ARE FLANGE GRADES UNLESS OTHERWISE NOTED.
- CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
- THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
- DASHED LINES SHOWN REPRESENT RAMP CENTERLINES, JOINTS, AND BREAK POINT LOCATIONS WITHIN THE RAMP.
- SEE DETAILS IN BACK OF PLAN FOR ADDITIONAL CURB RAMP DETAILS.



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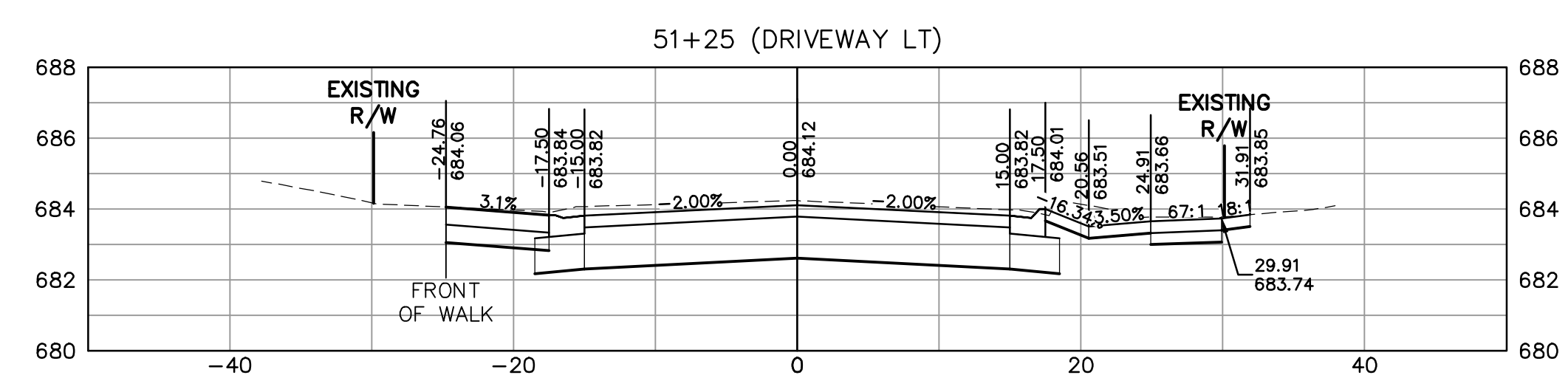
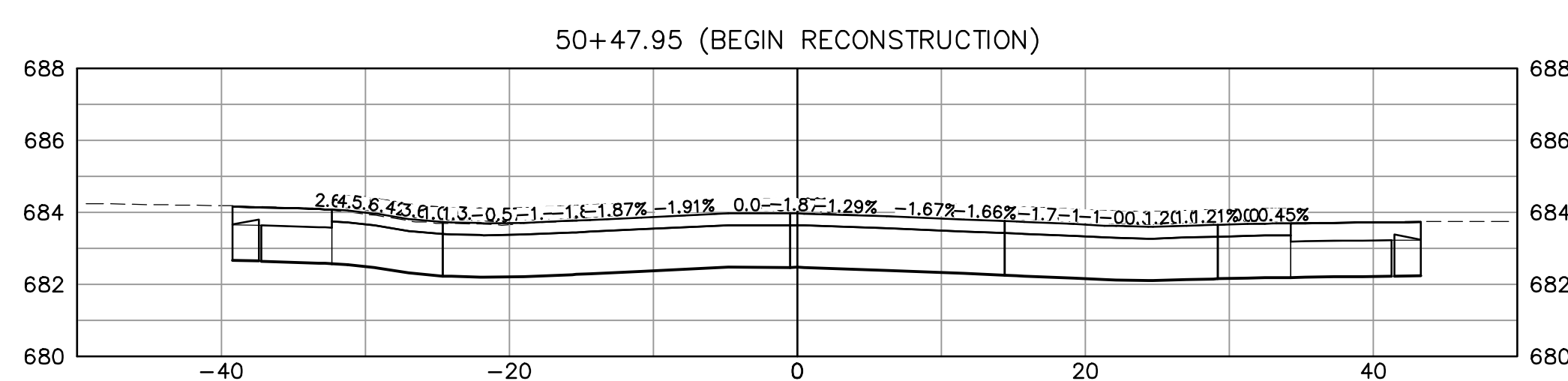
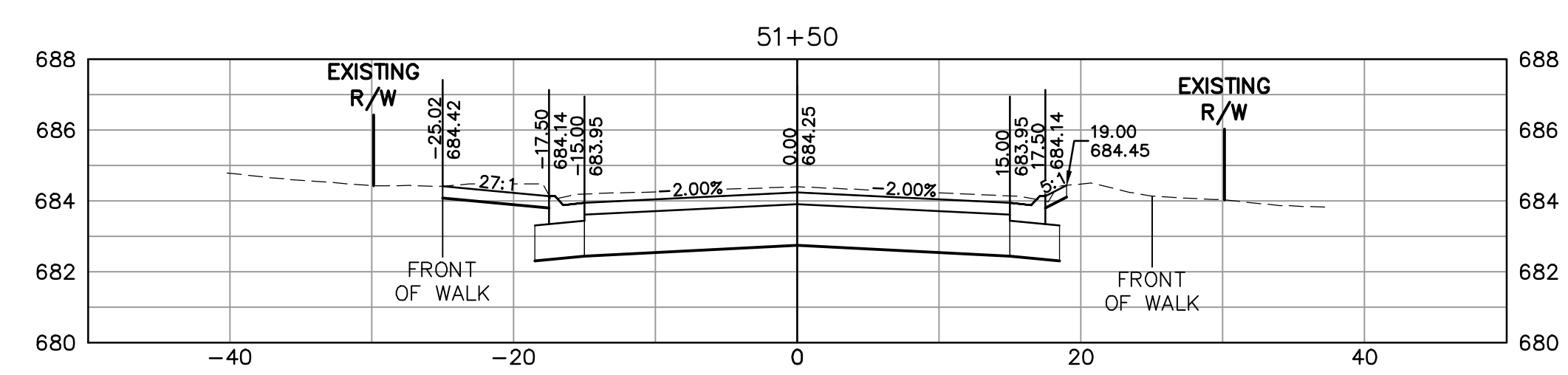
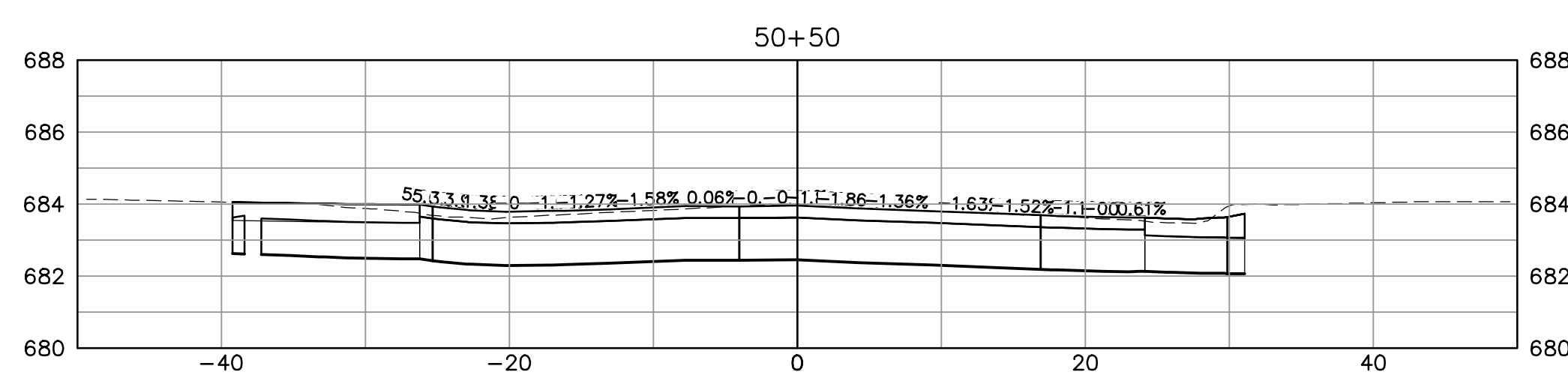
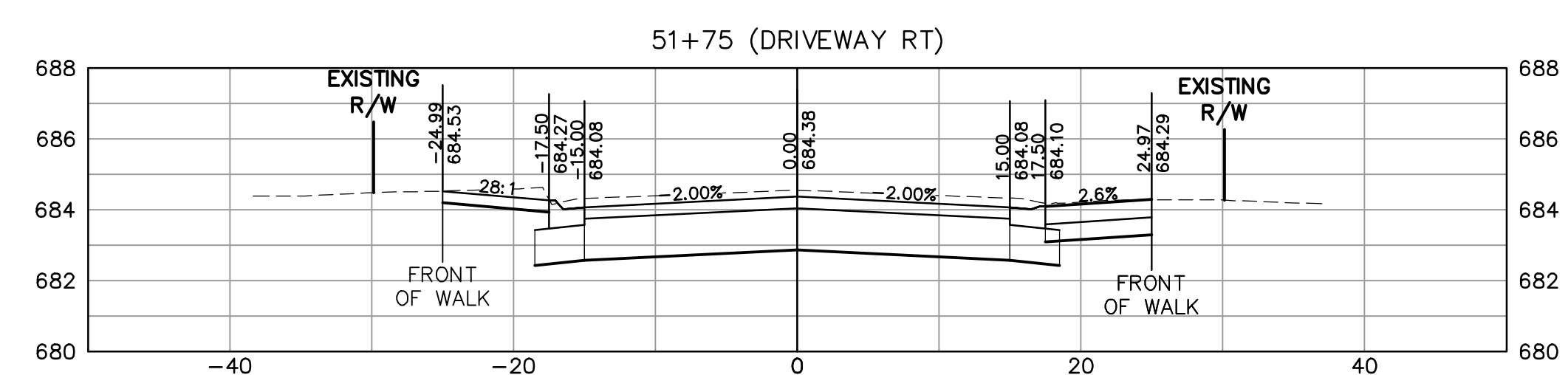
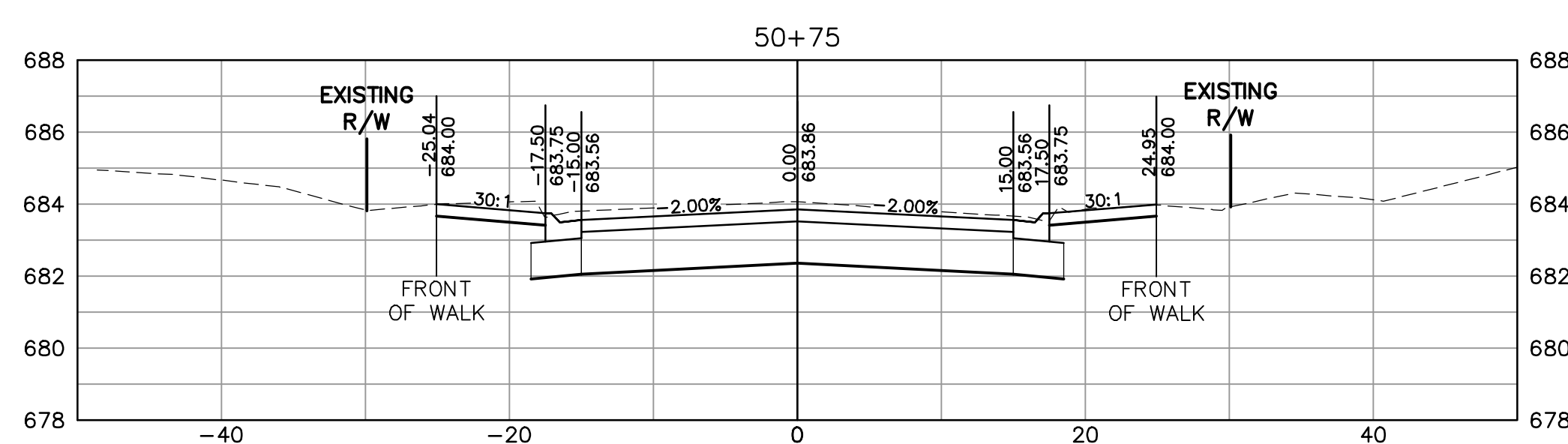
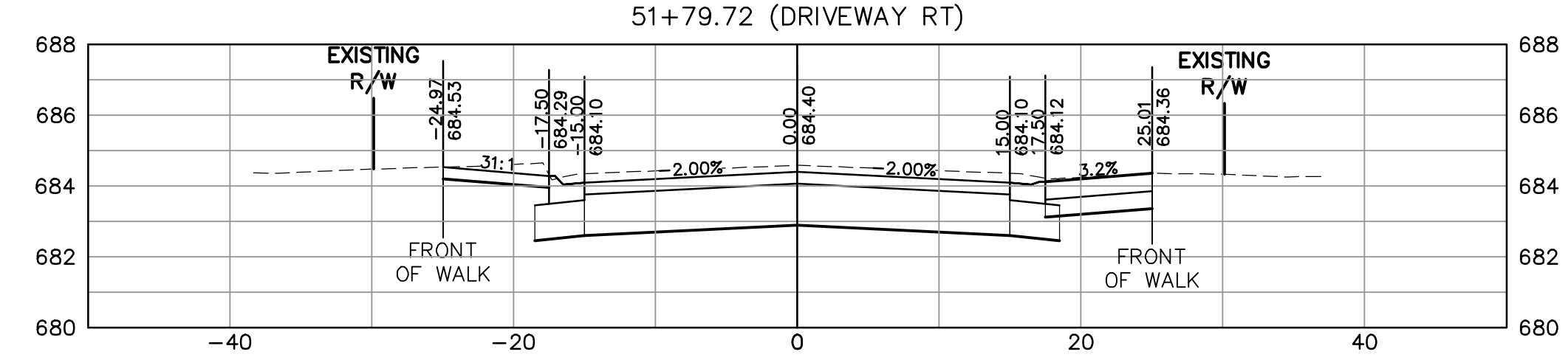
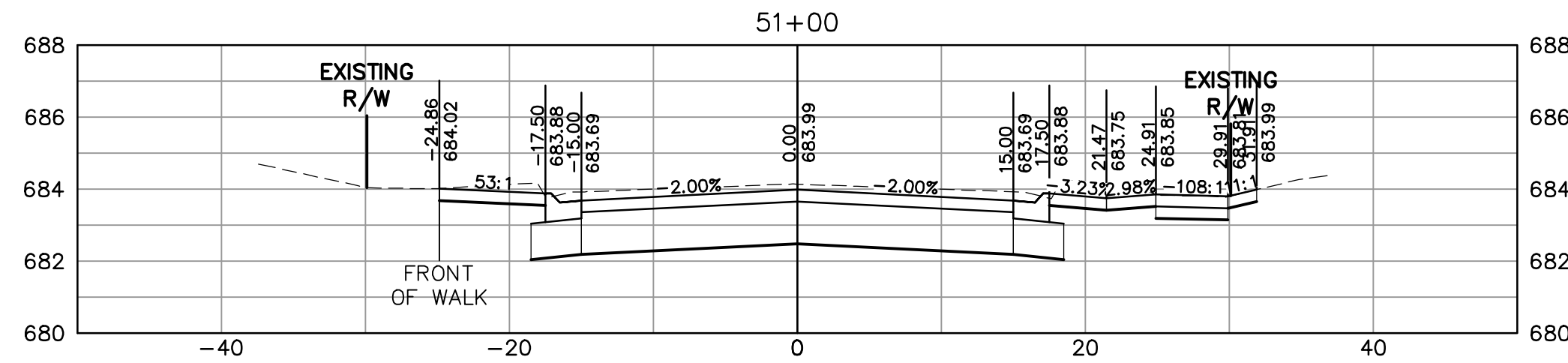
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 RACINE COUNTY, WISCONSIN
 2023 PAVING PROGRAM
 SECTION 2 - KENNINGTON SQUARE RD
 CURB RAMP DETAILS

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PROJECT MANAGER: JASON FEUCHT, P.E.
DESIGNED BY: RJW
CHECKED BY: JMF
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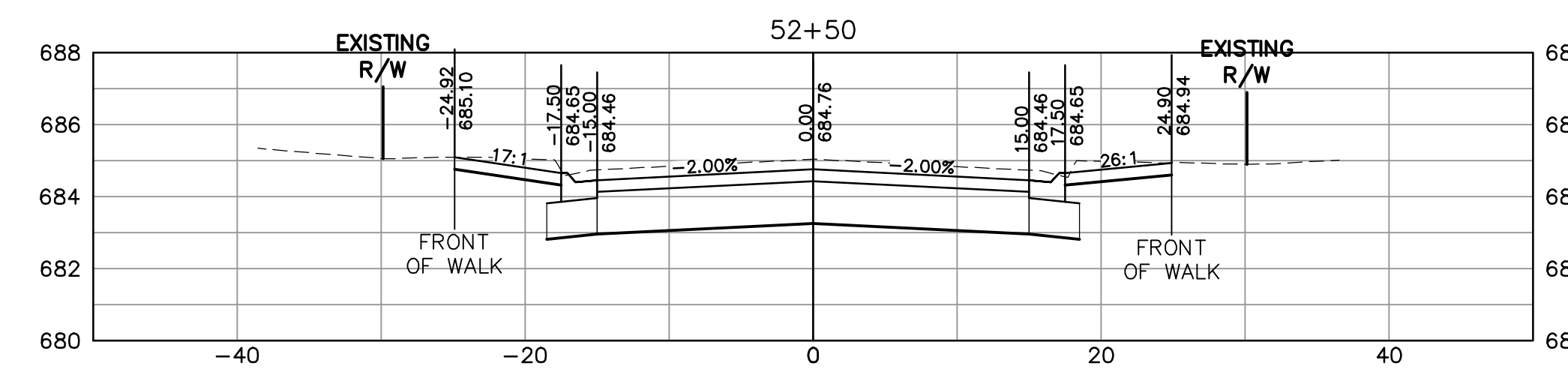
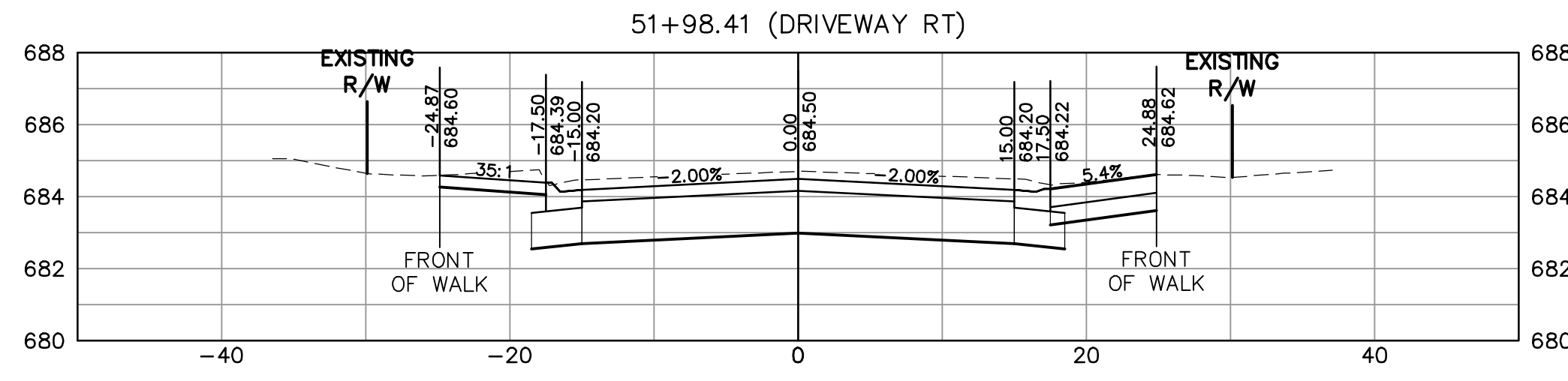
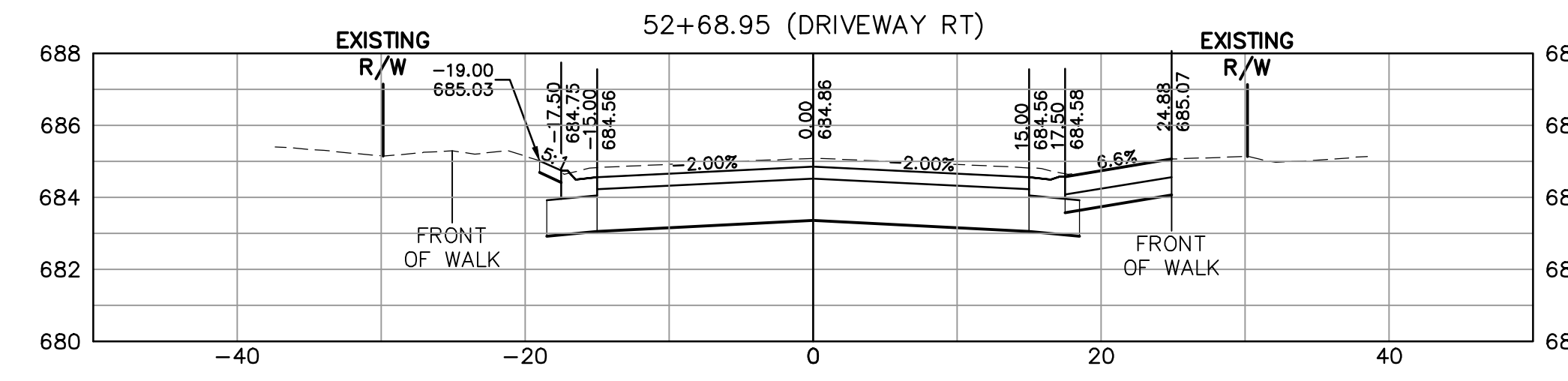
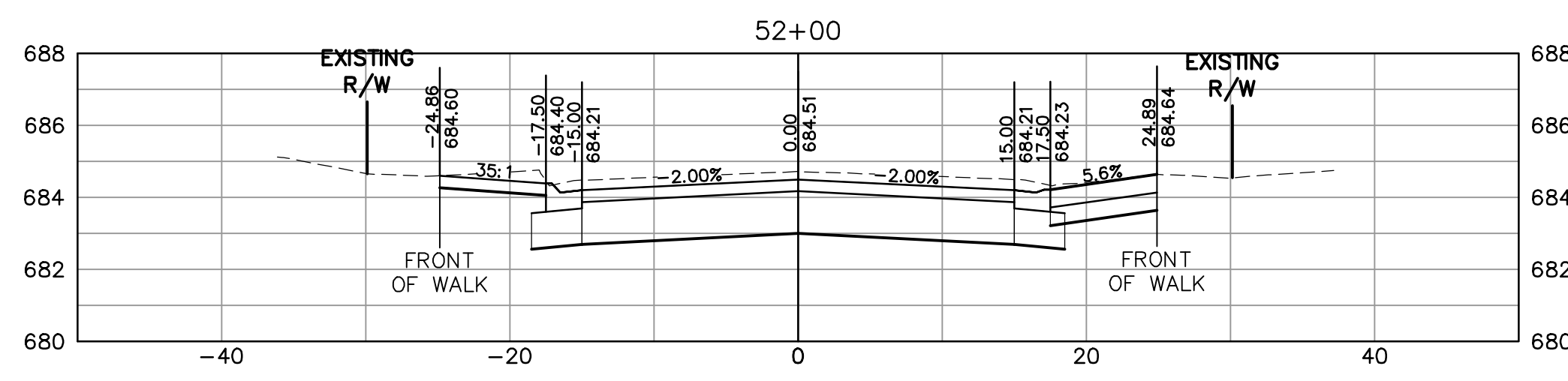
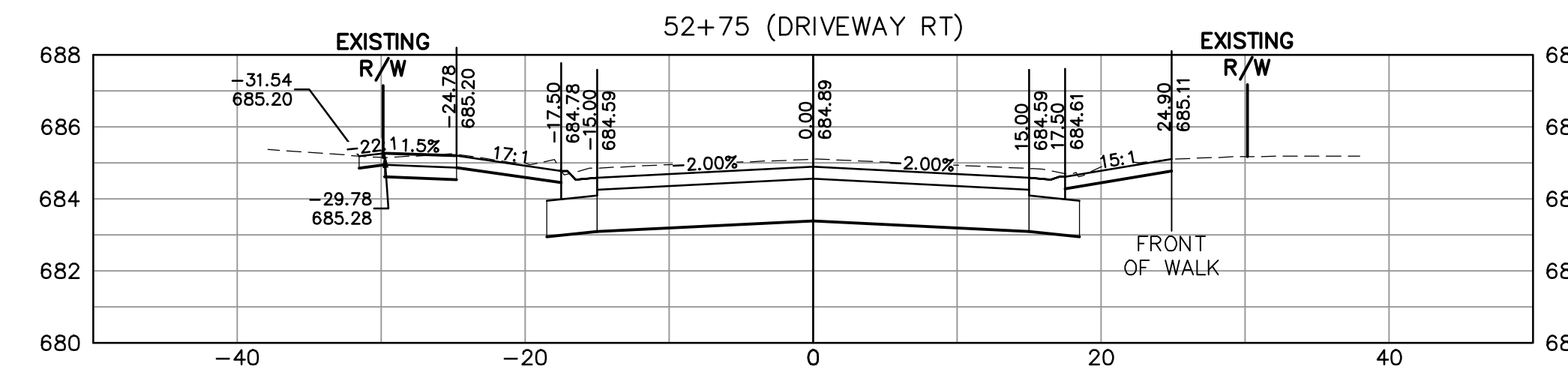
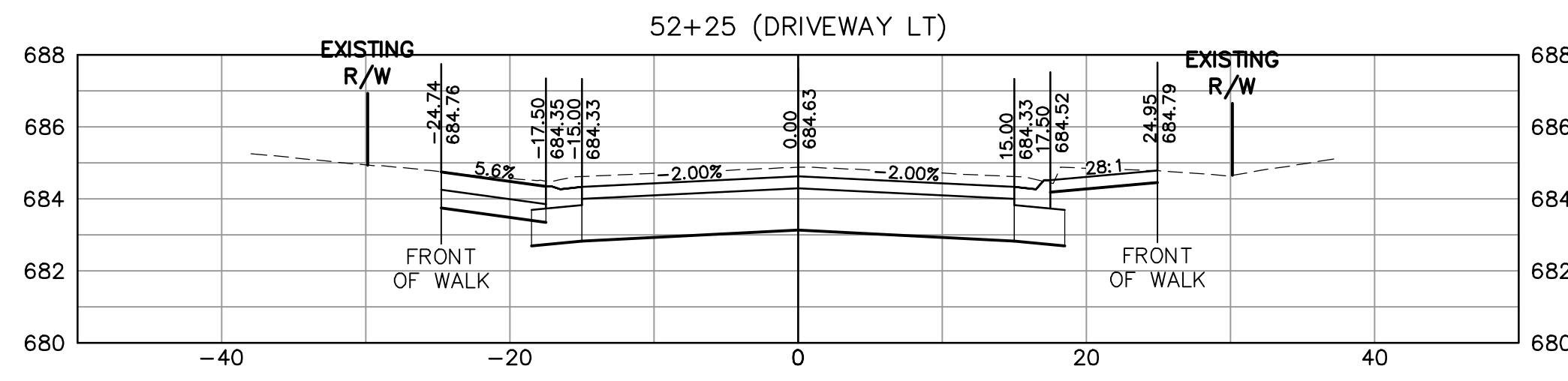
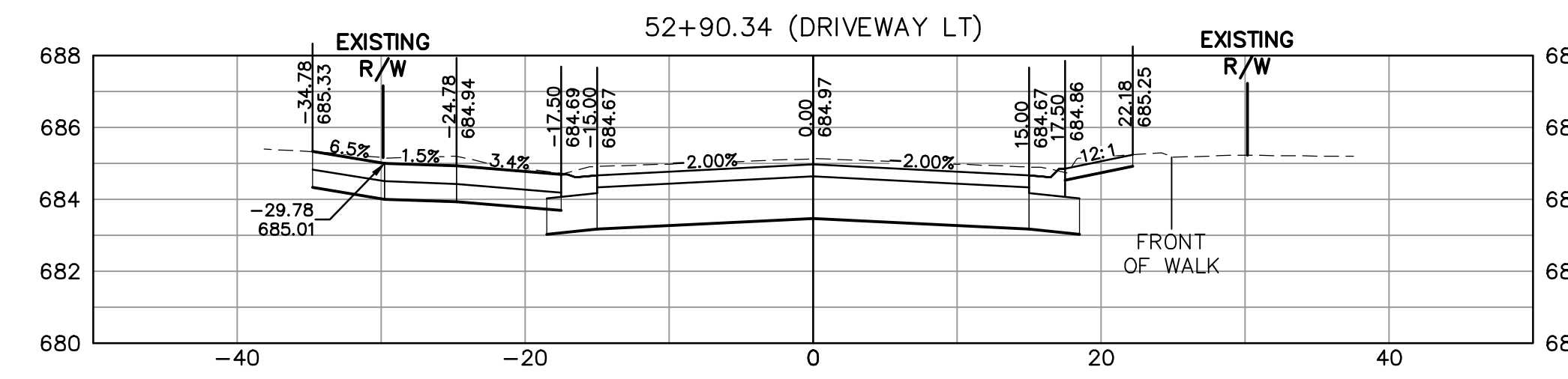
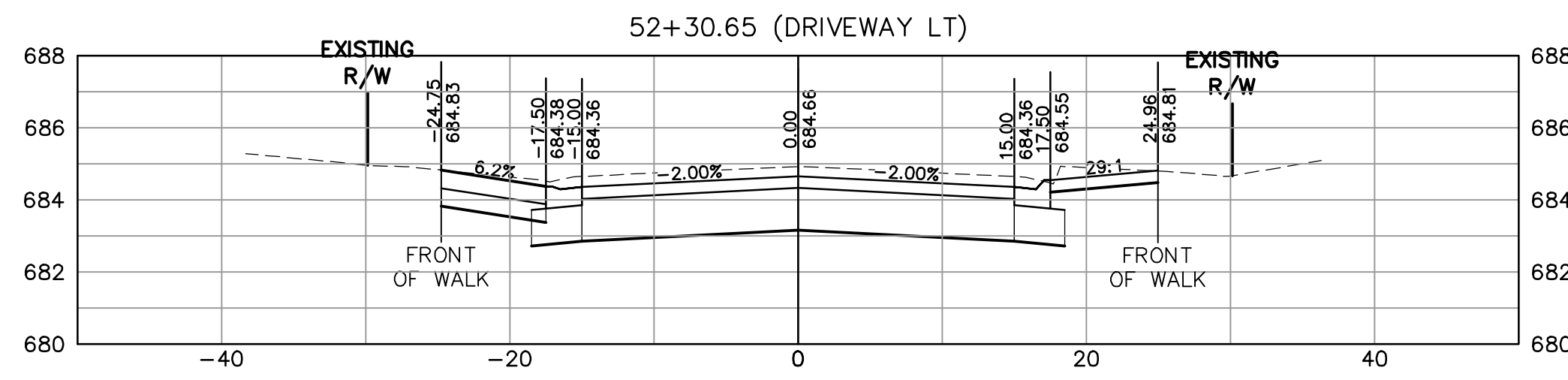
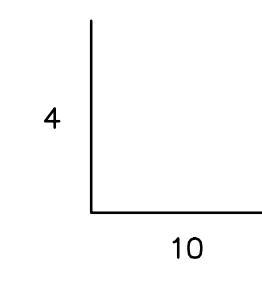
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SECTION 2 - KENNINGTON SQUARE RD
CROSS SECTIONS

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PROJECT MANAGER: JASON FEUCHT, P.E.
DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
25

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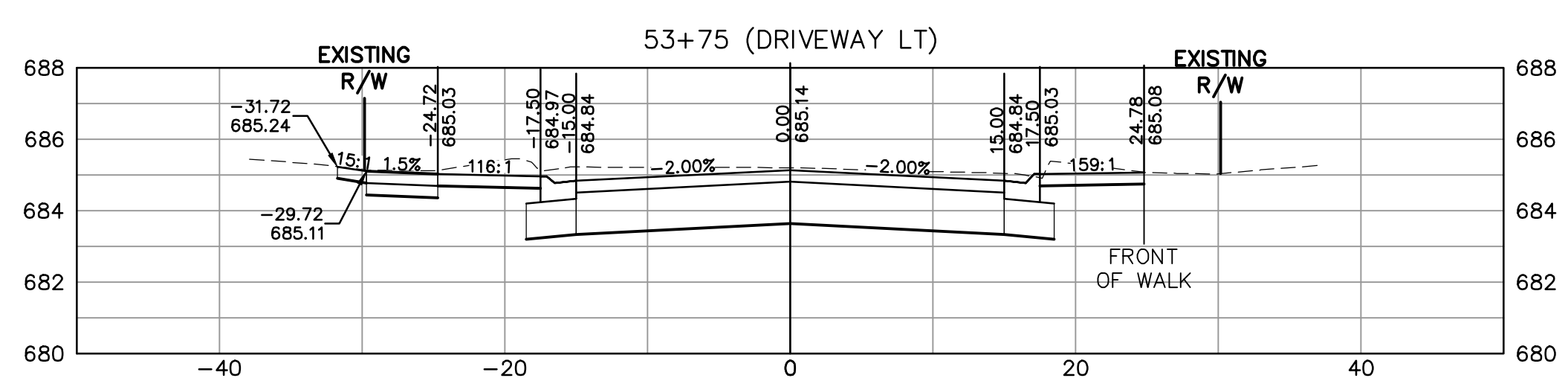
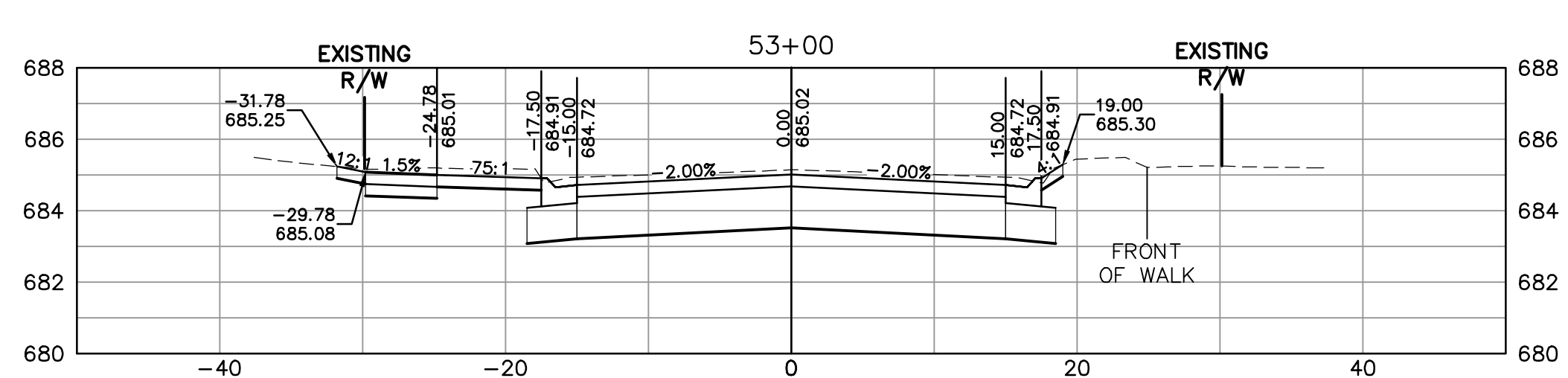
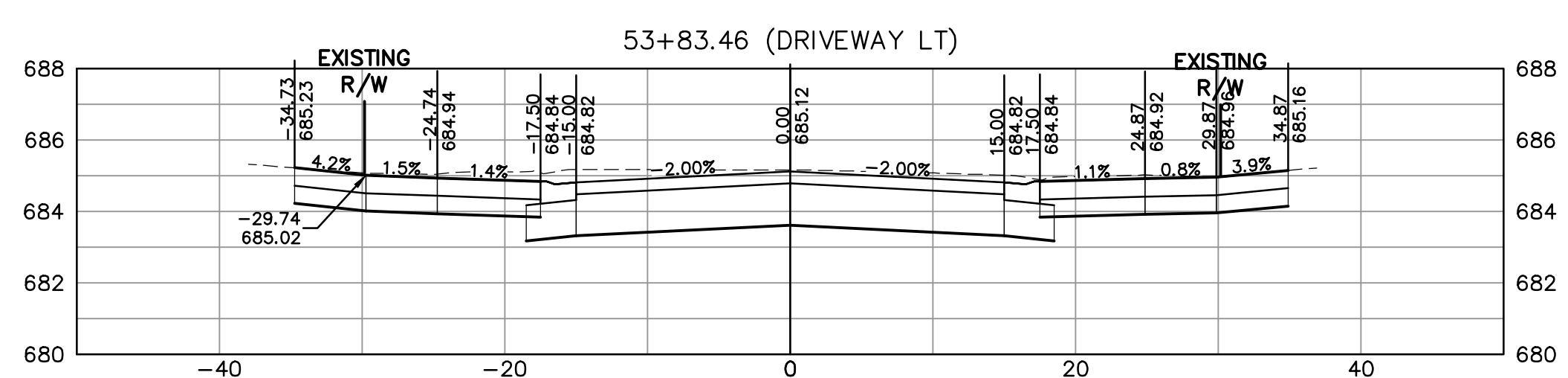
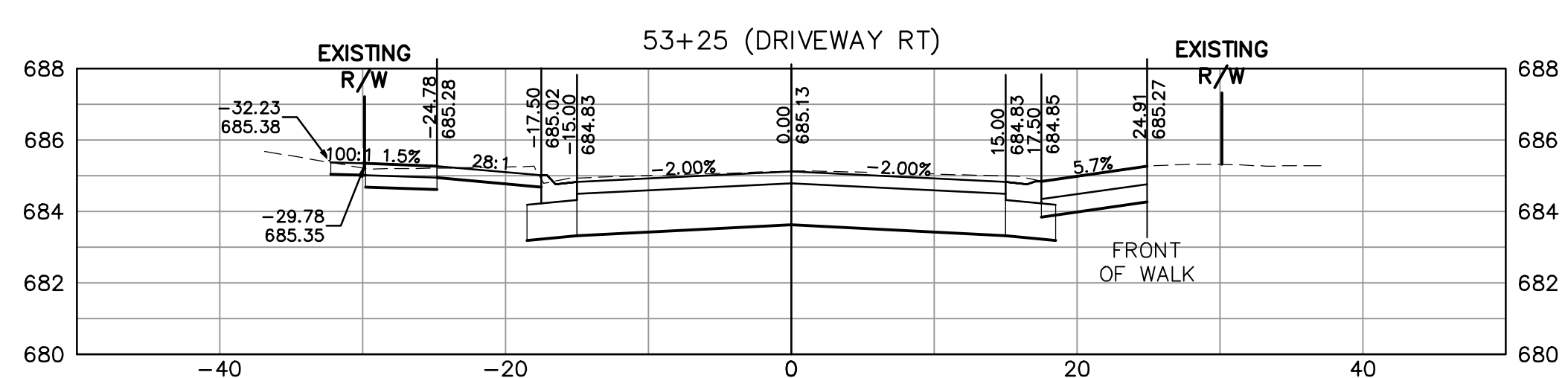
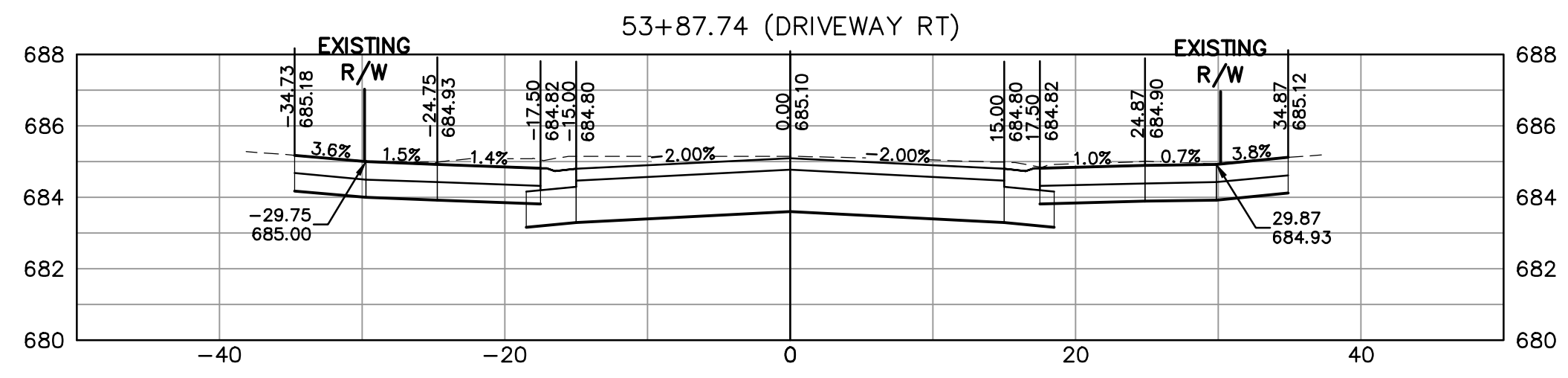
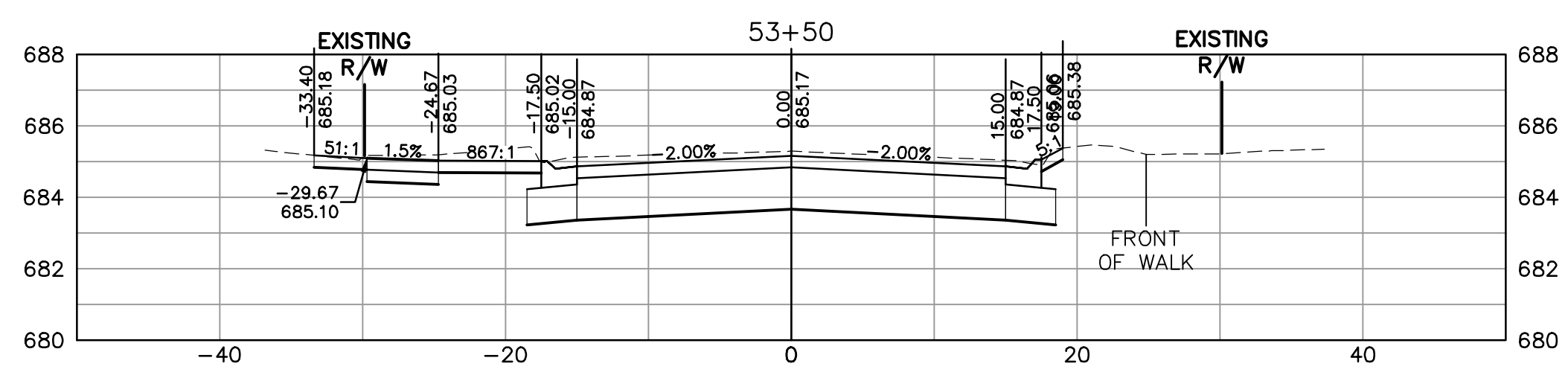
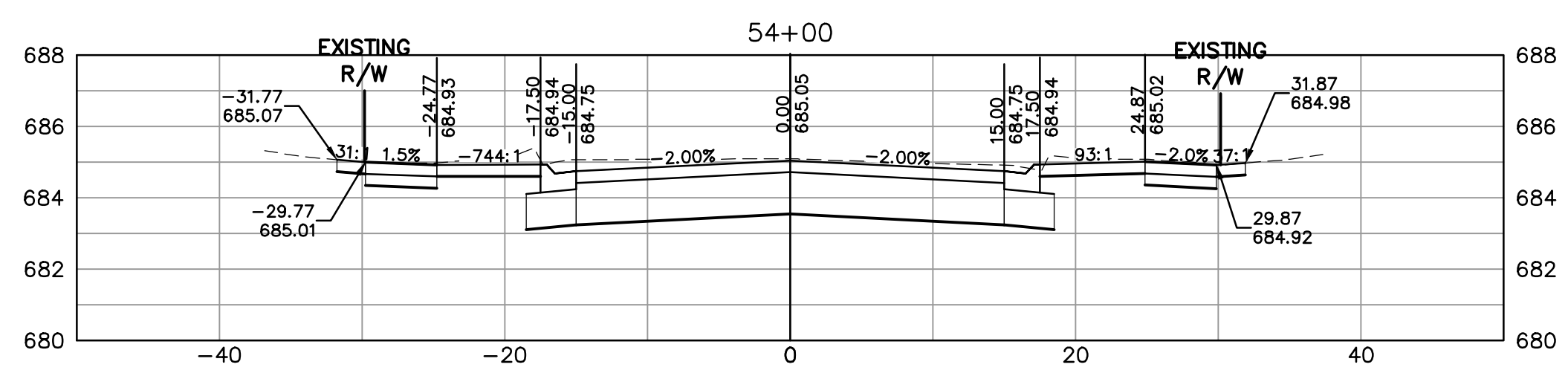
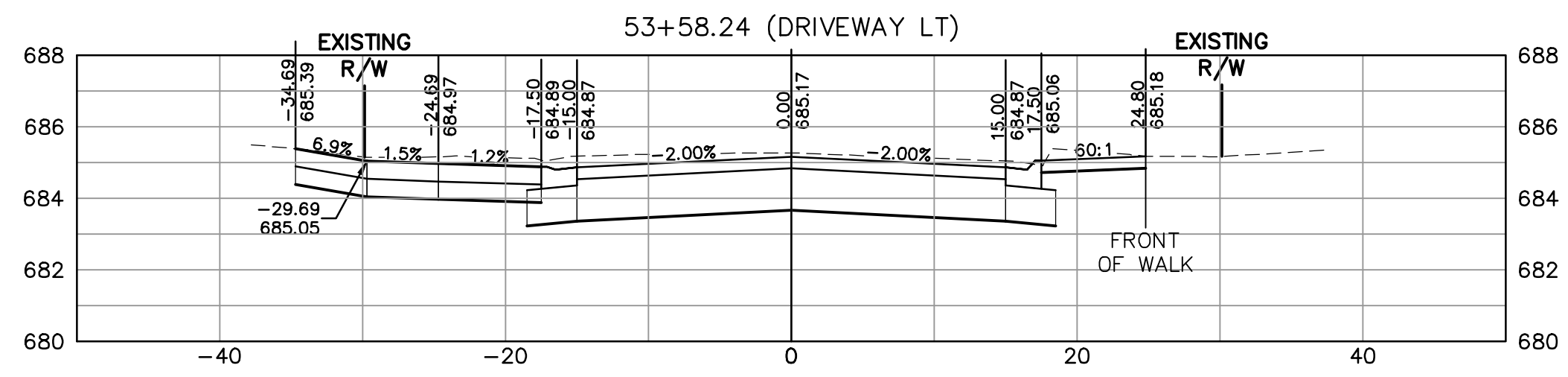
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PROJECT MANAGER: JASON FEUCHT, P.E.
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CHECKED BY: JMF
SHEET NUMBER 26



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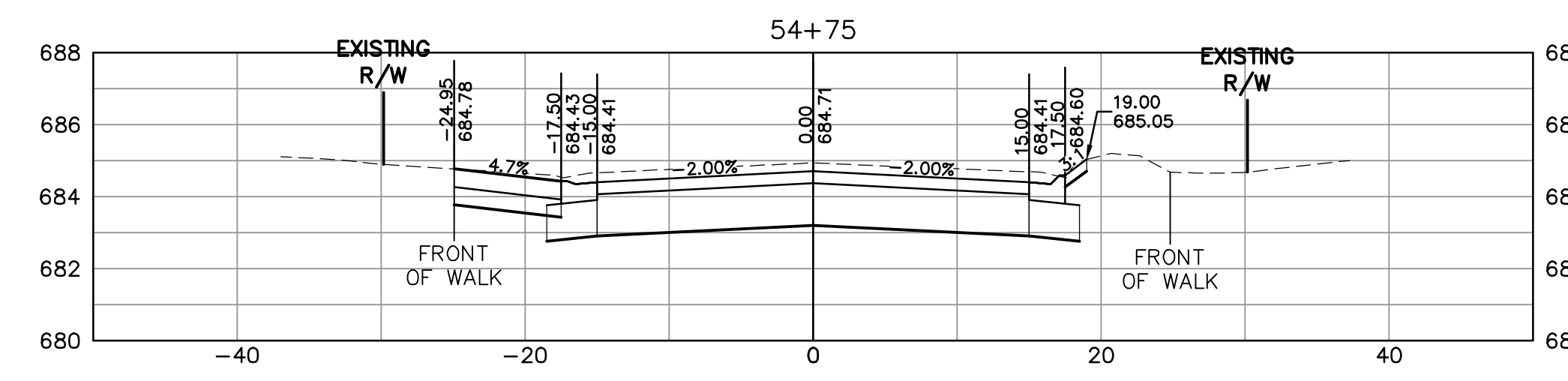
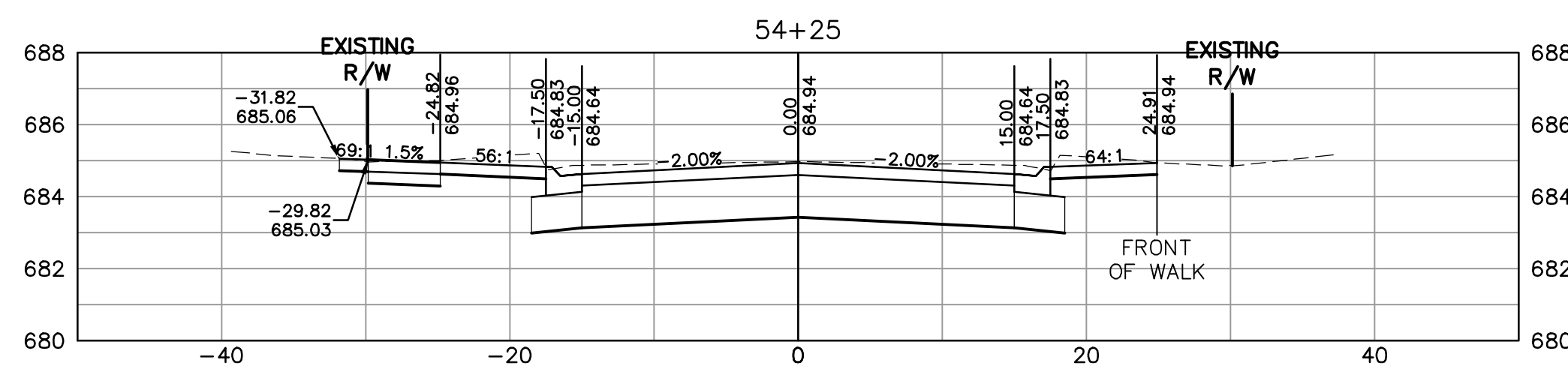
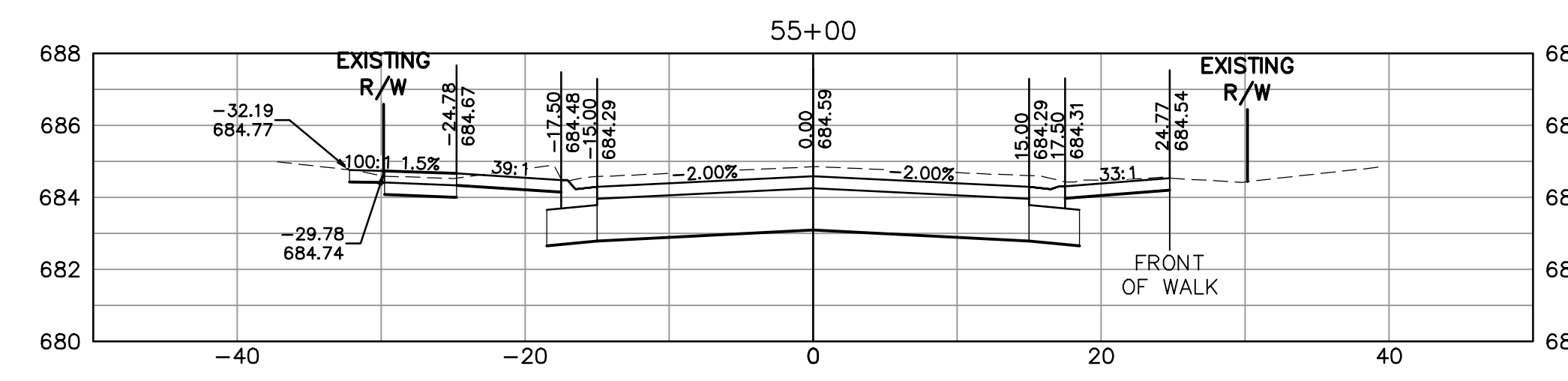
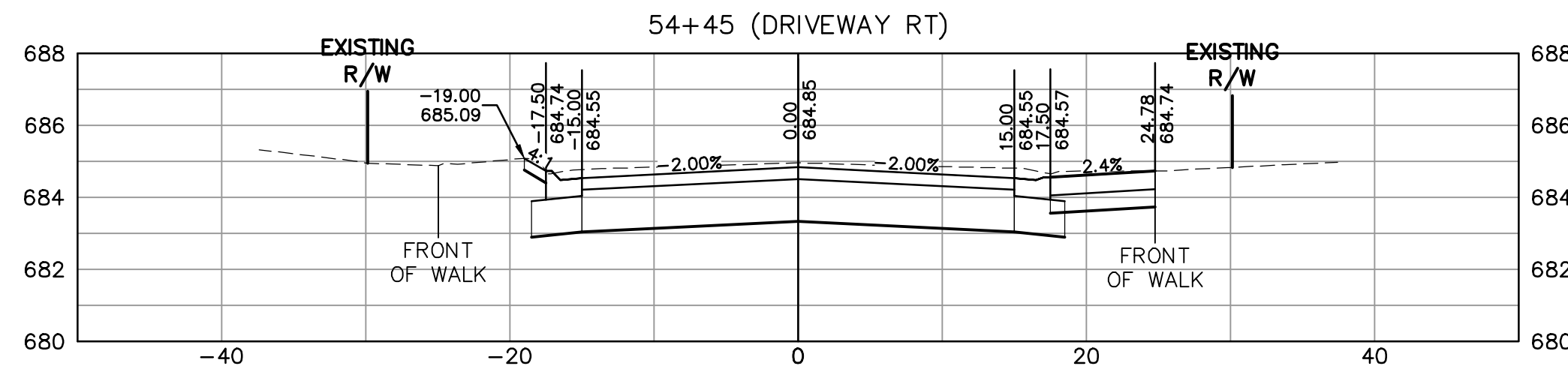
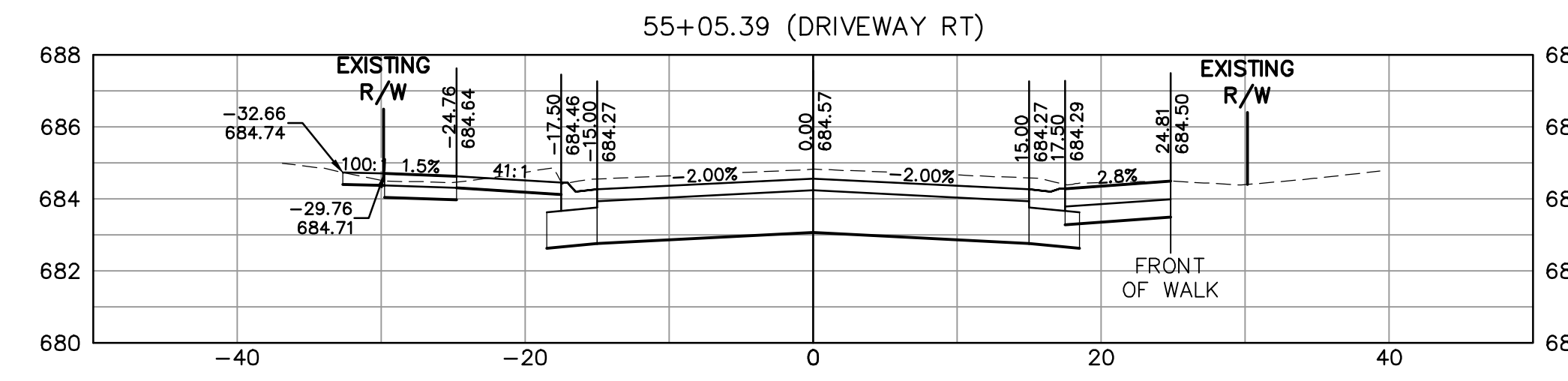
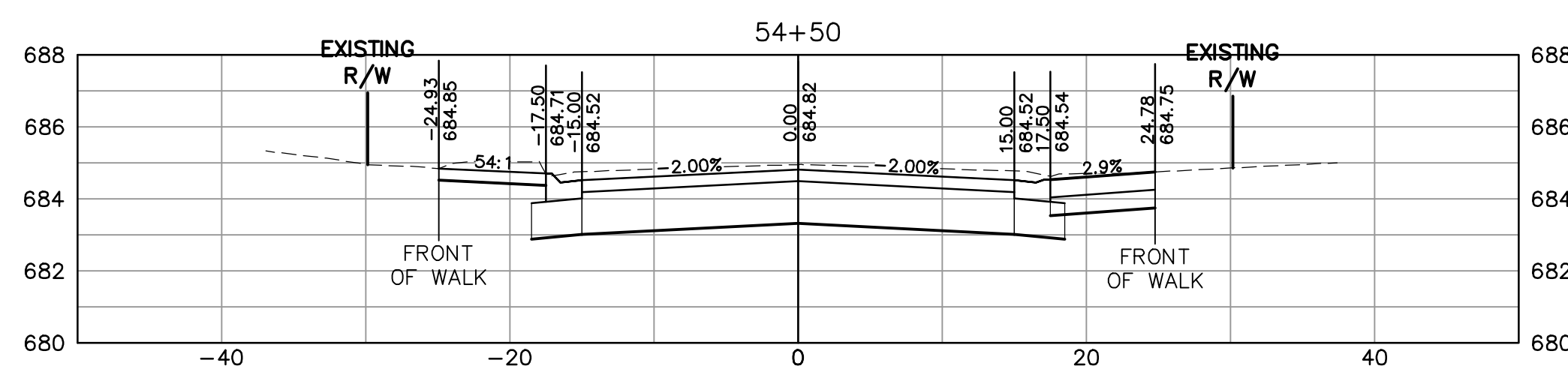
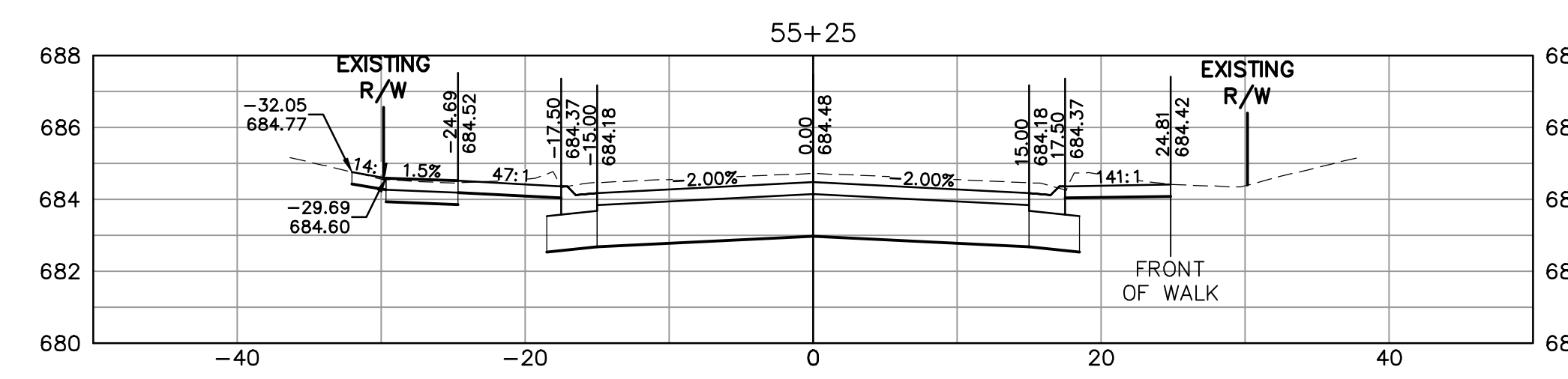
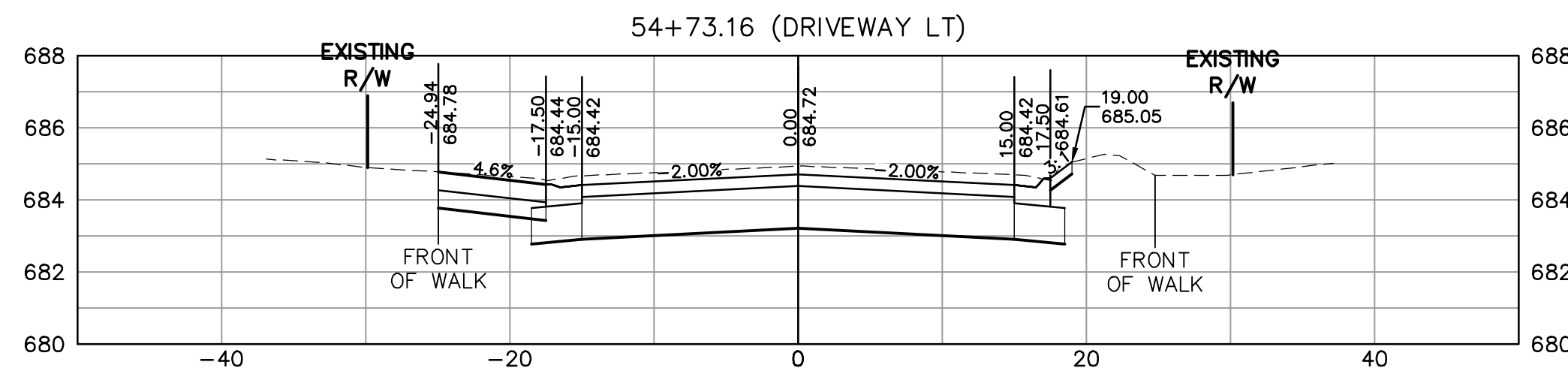
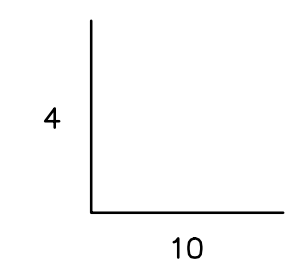
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SHEET NUMBER
27

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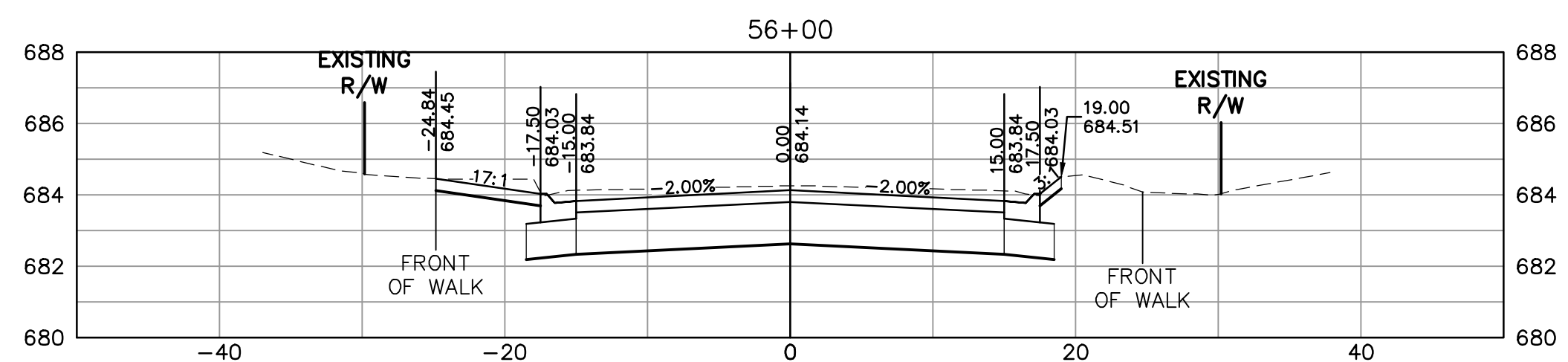
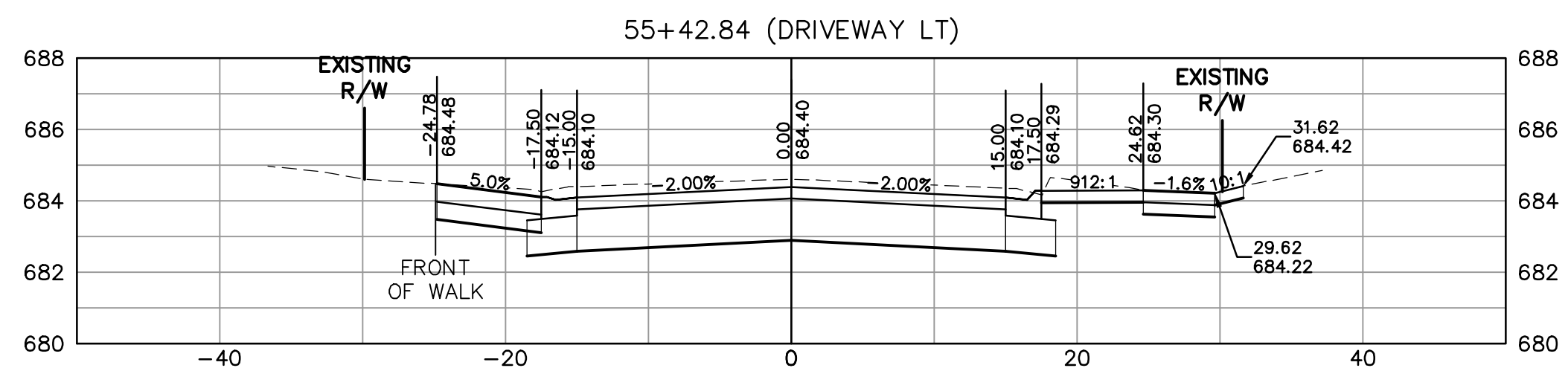
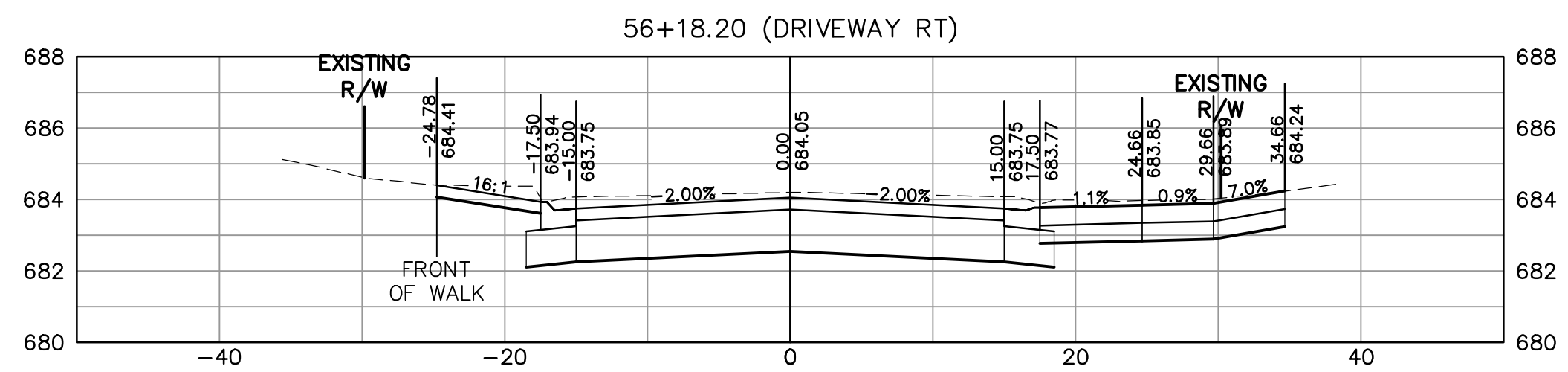
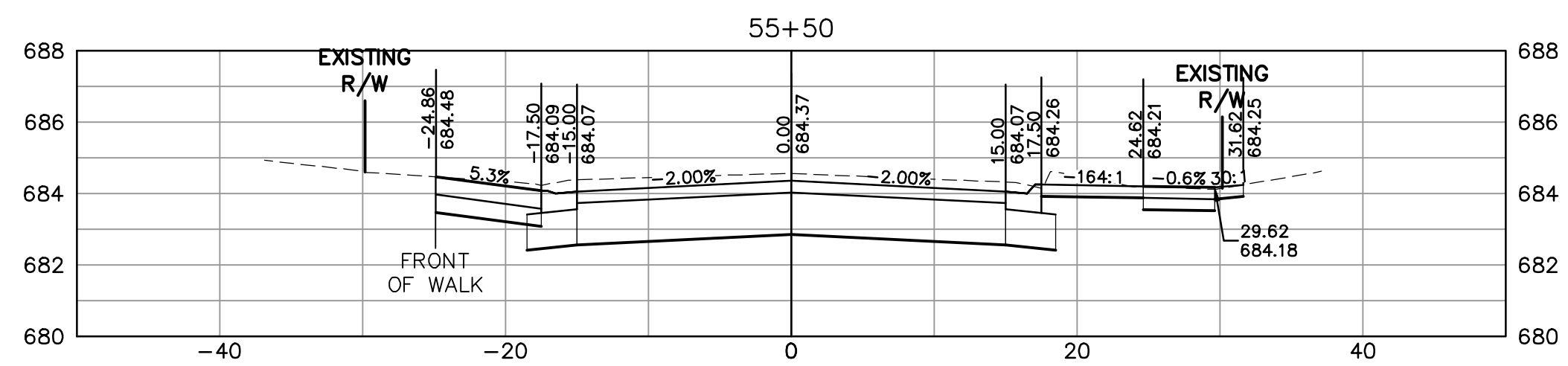
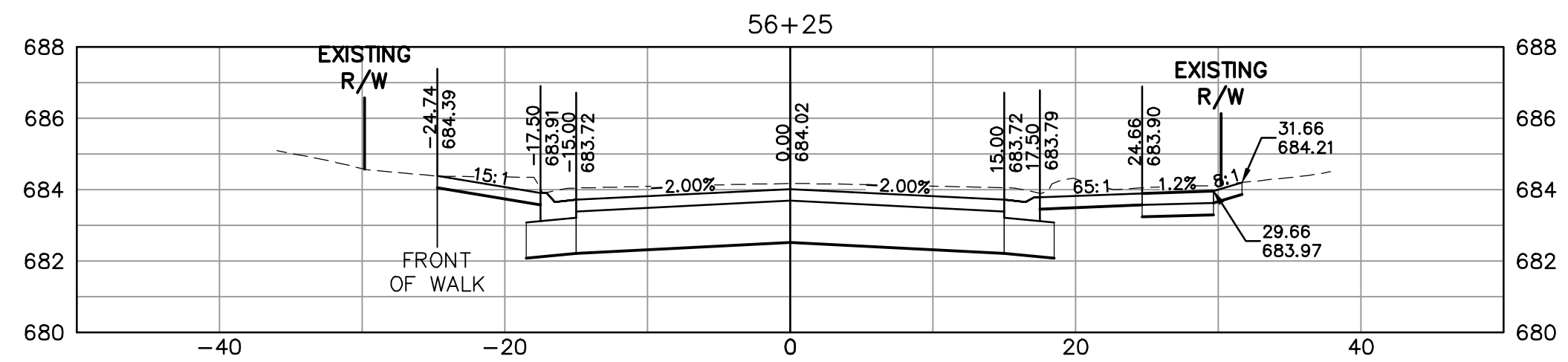
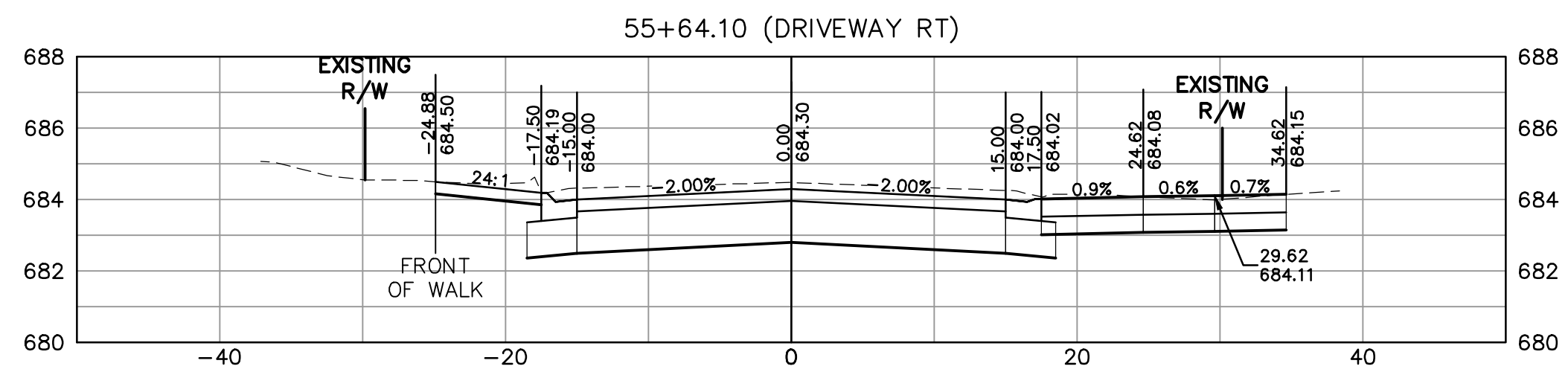
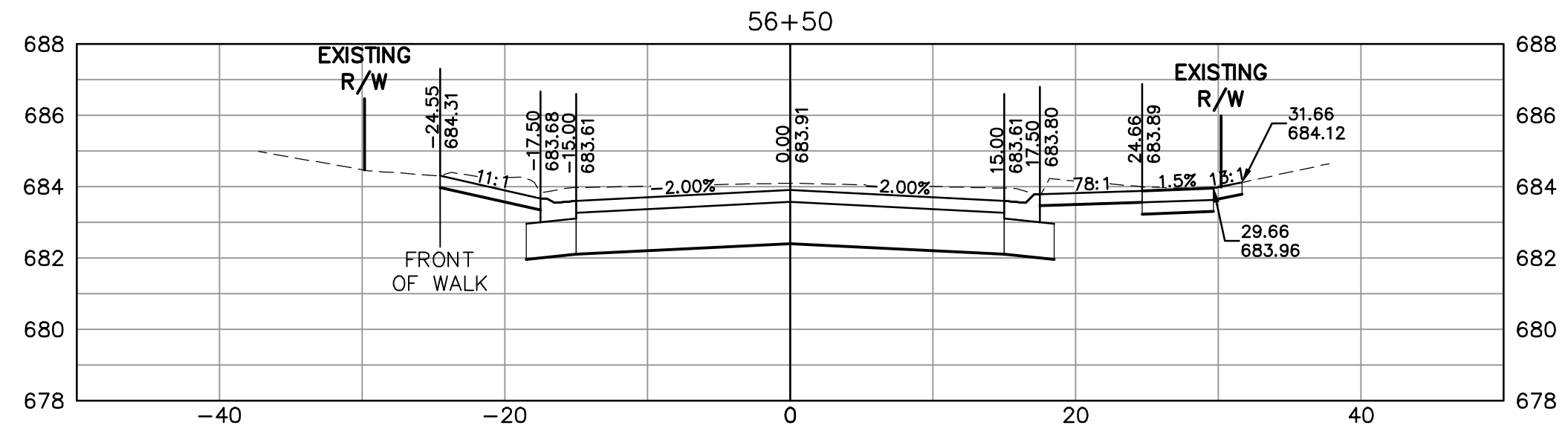
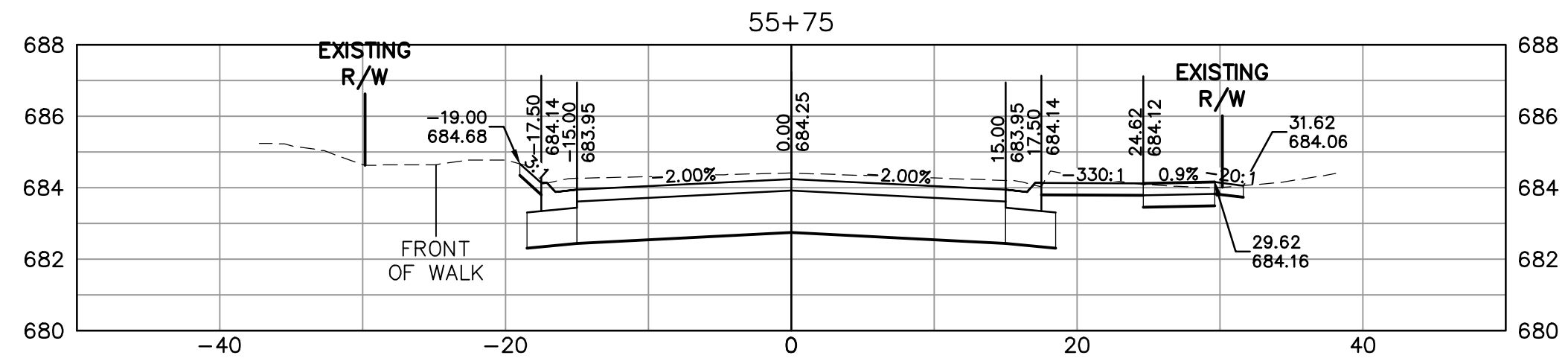
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28

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4
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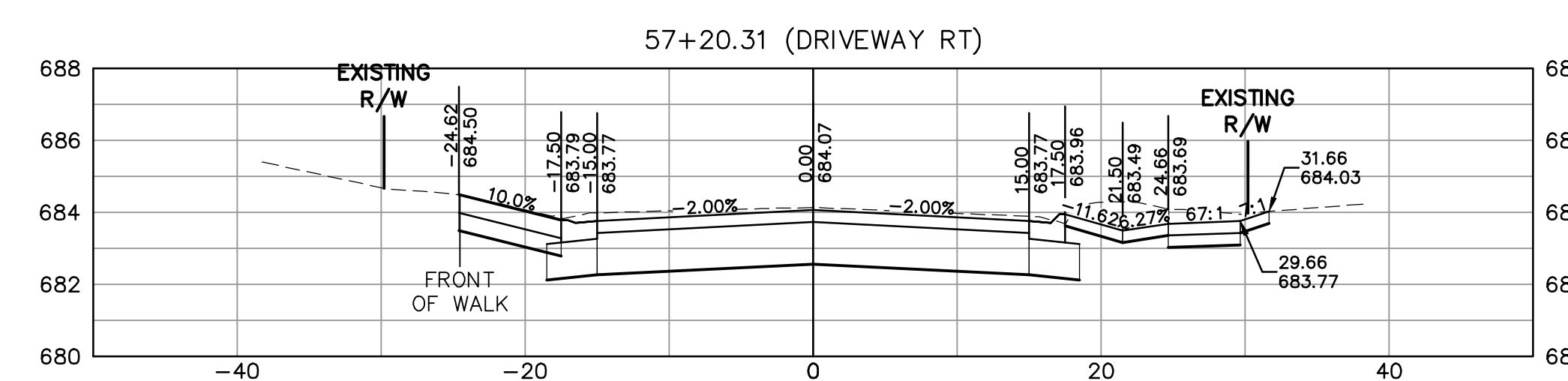
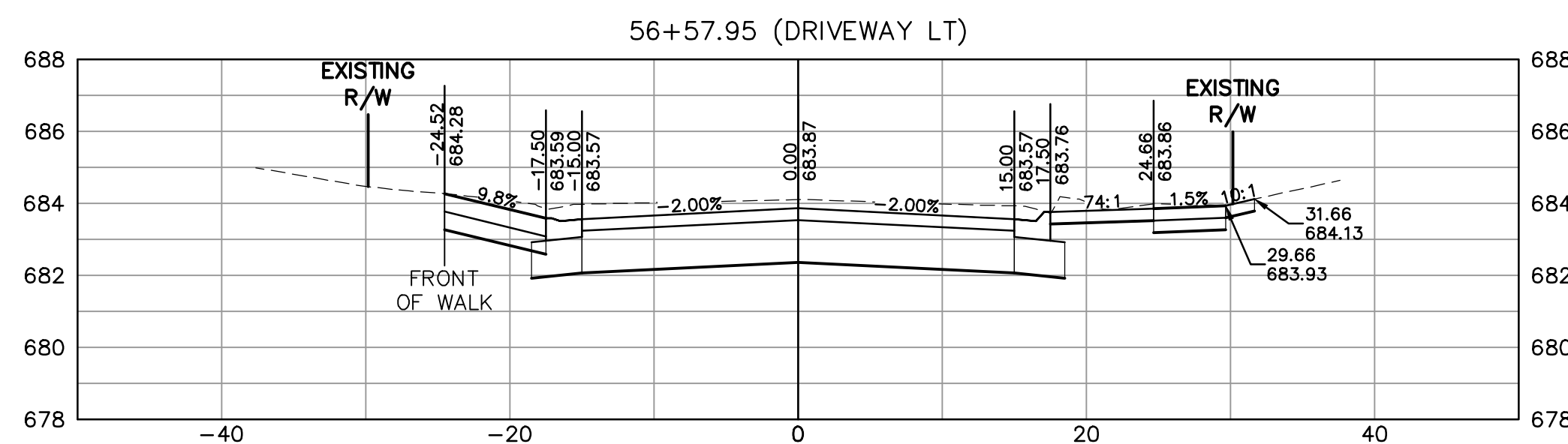
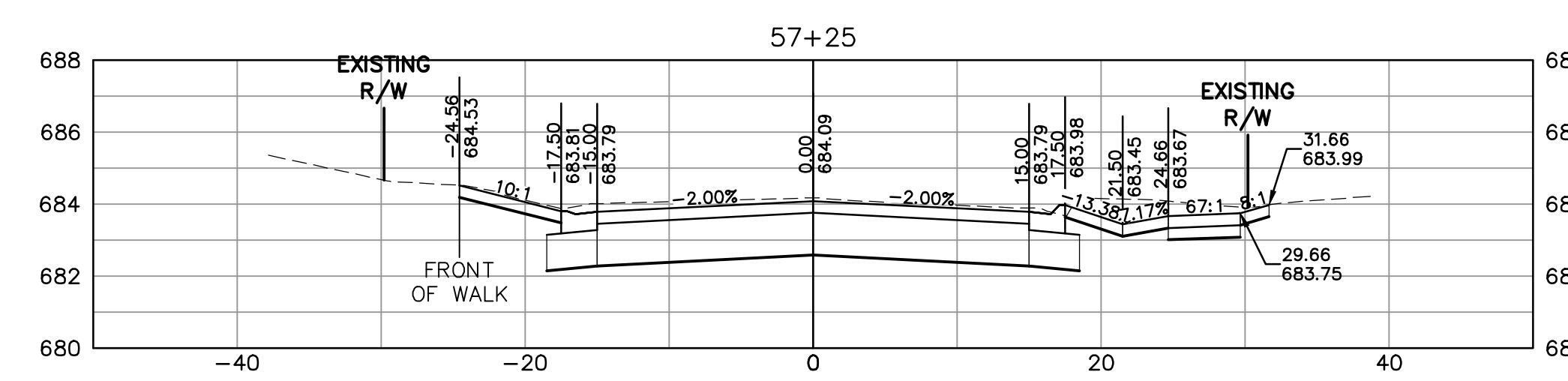
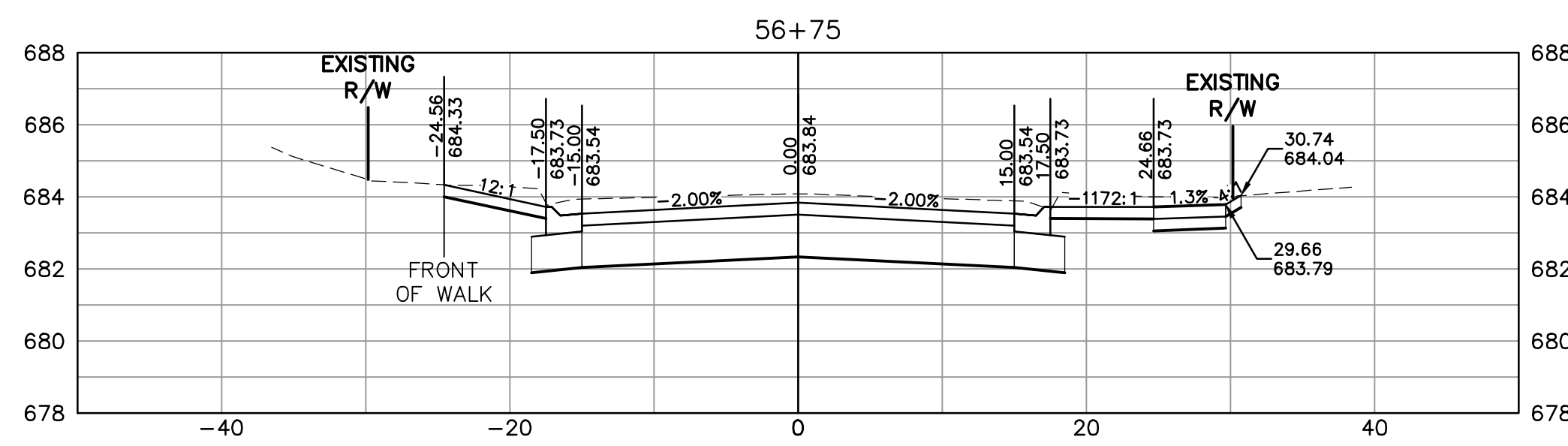
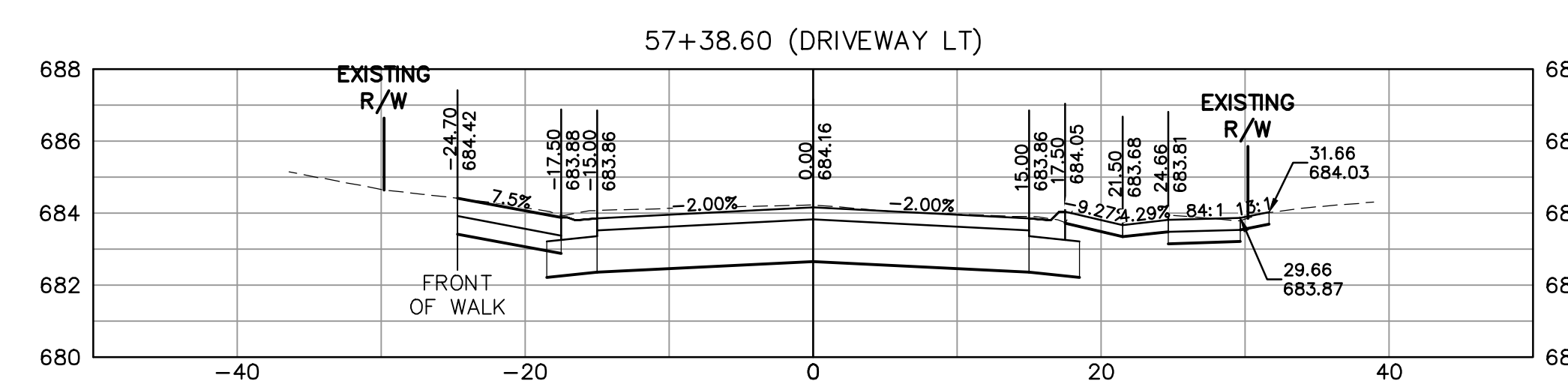
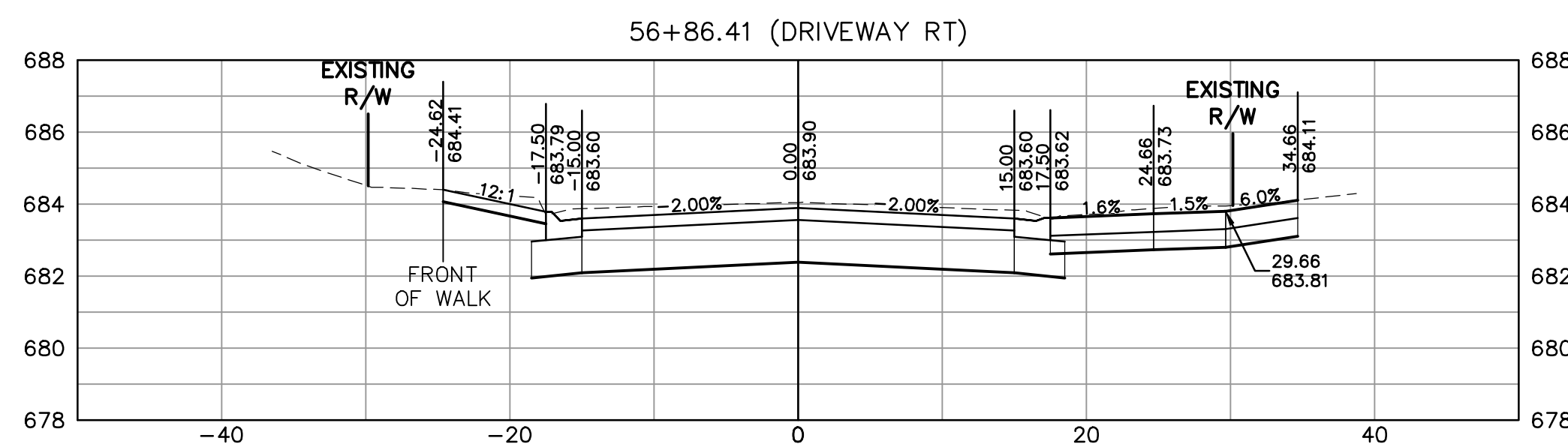
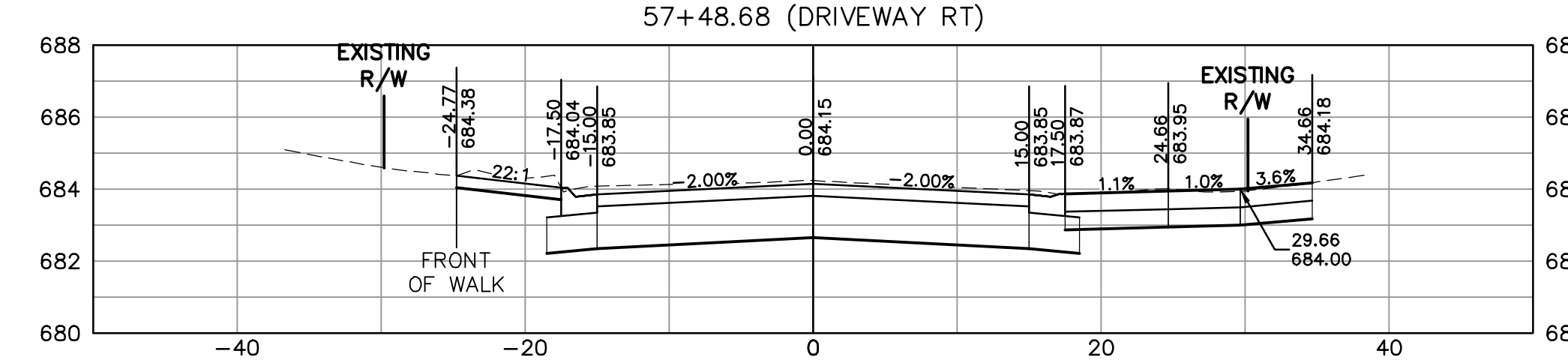
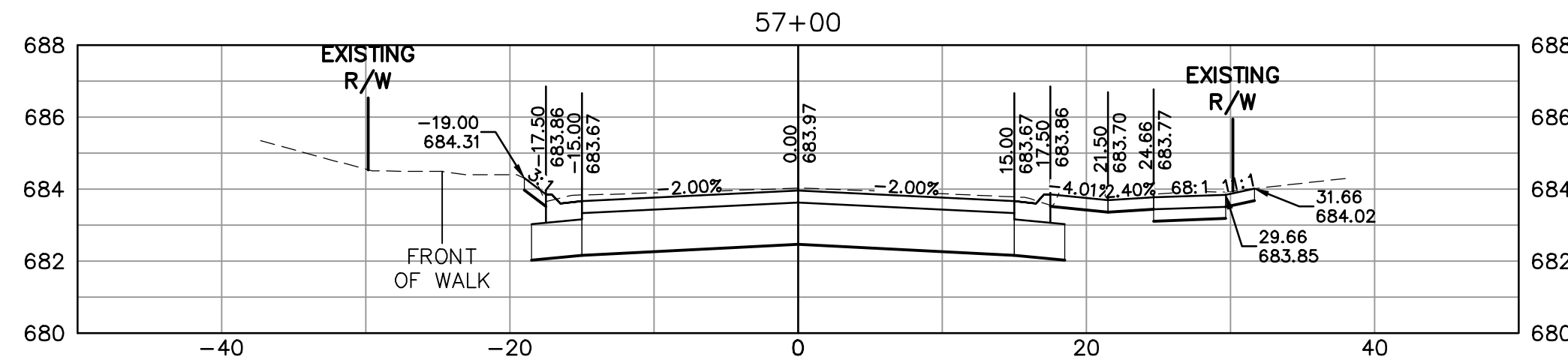
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DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
29

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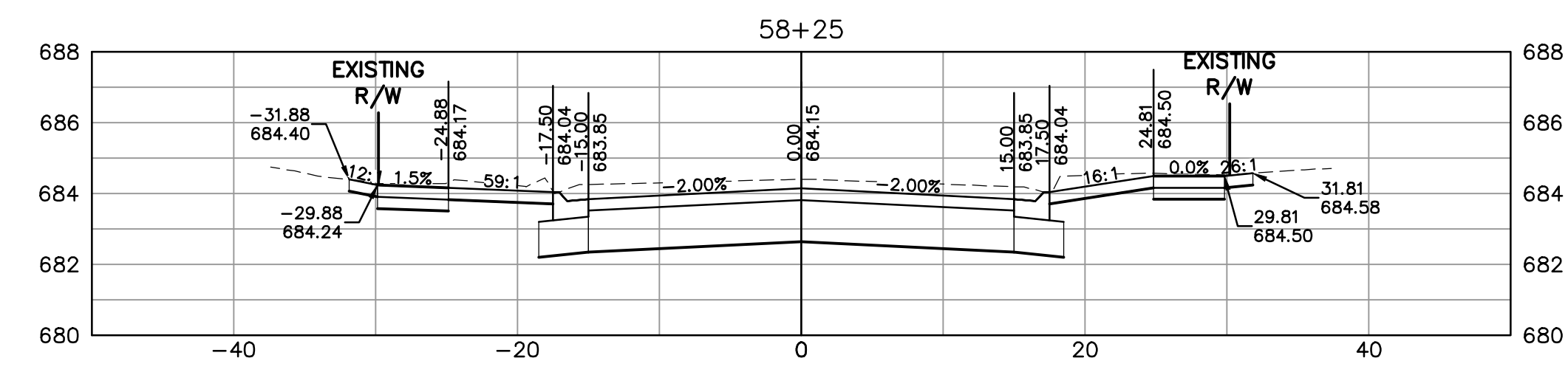
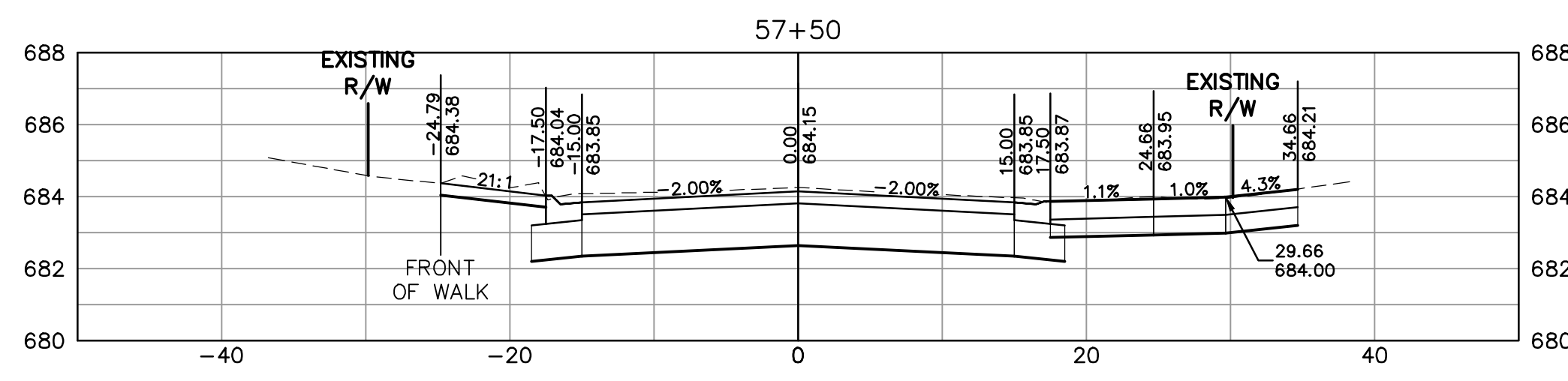
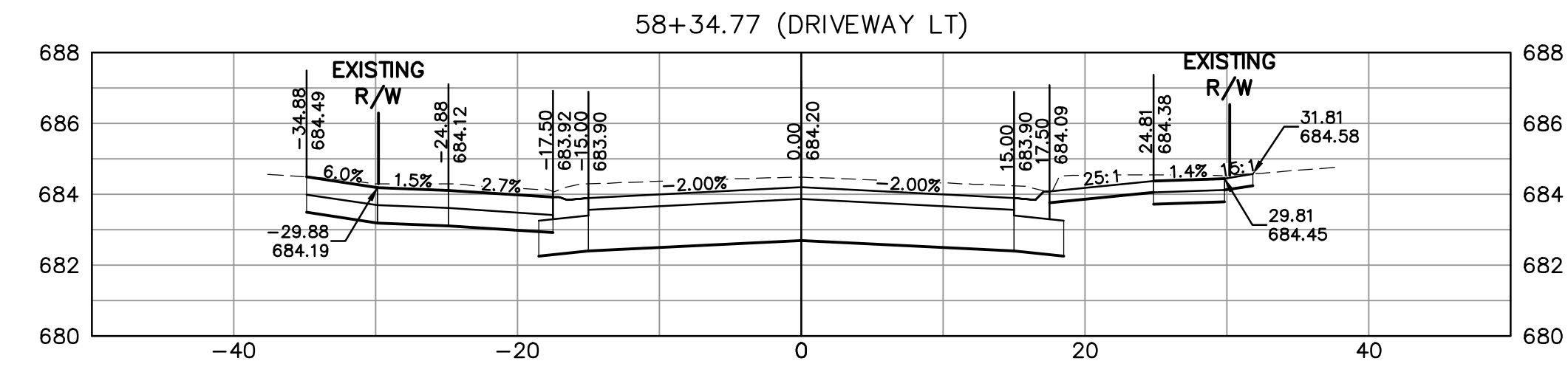
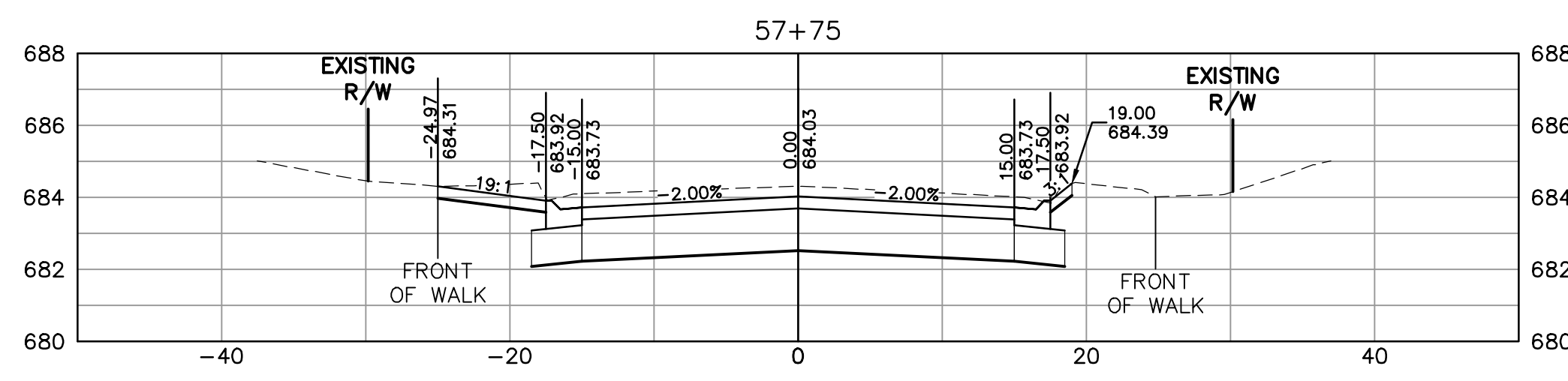
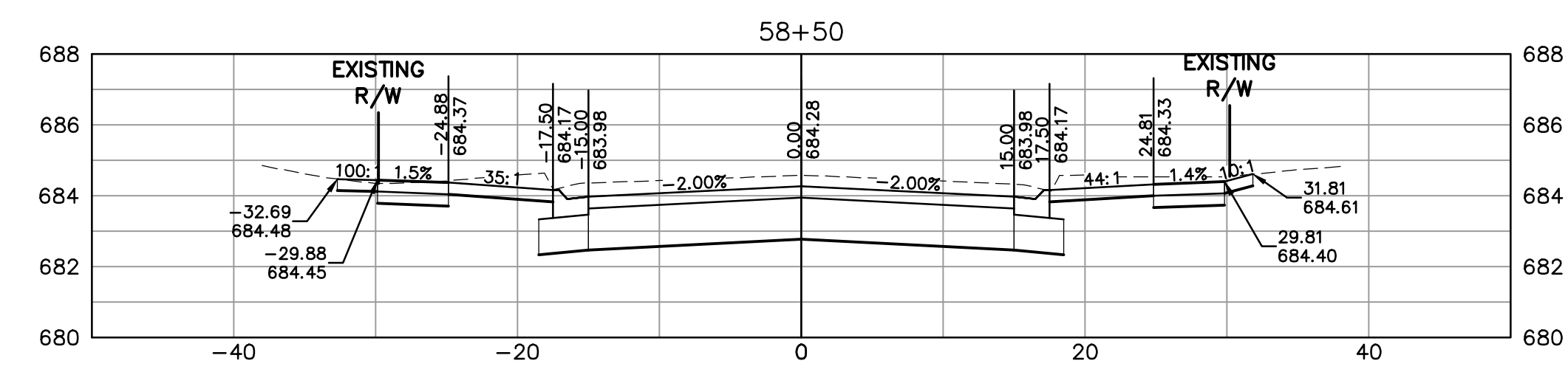
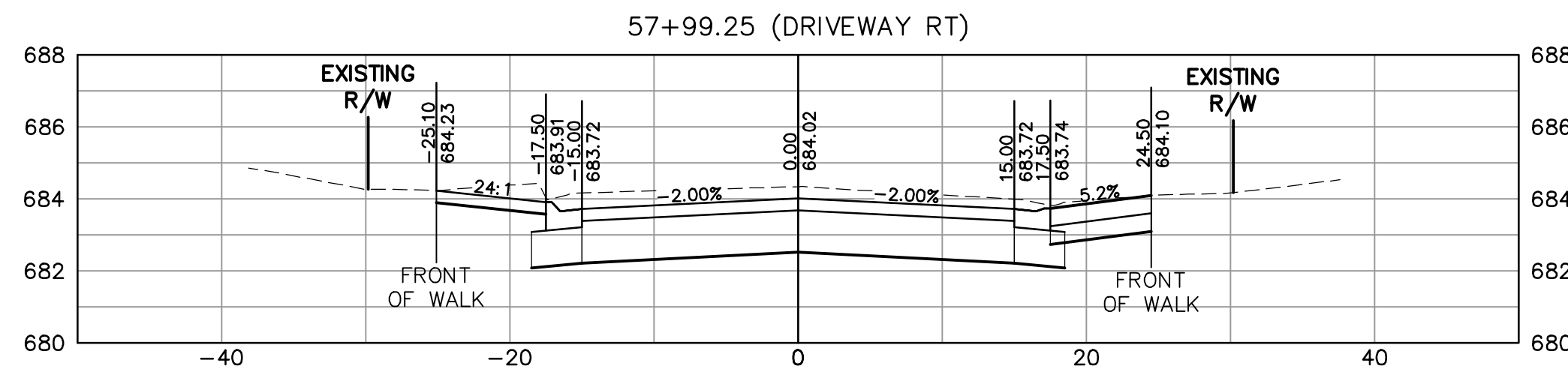
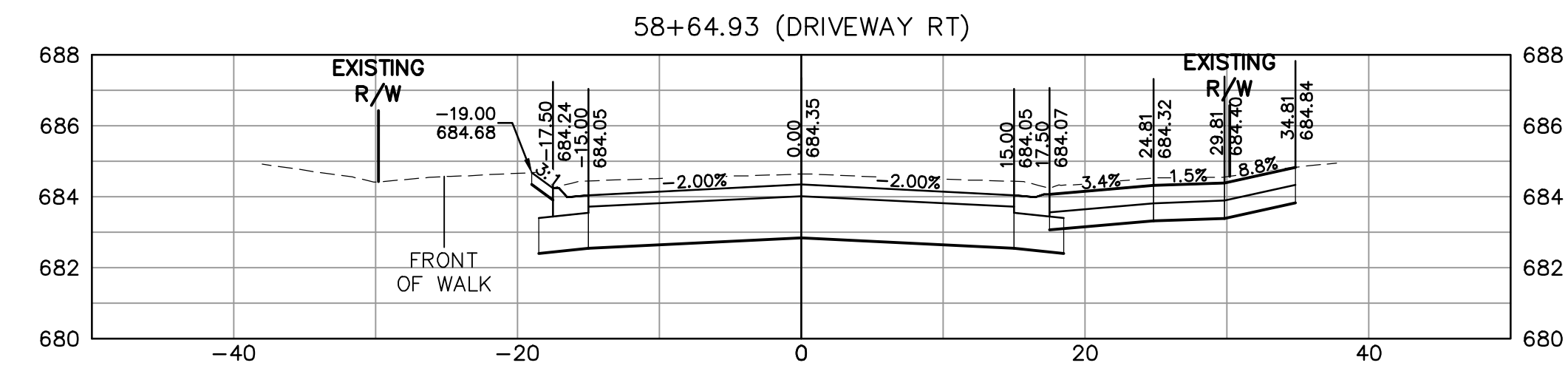
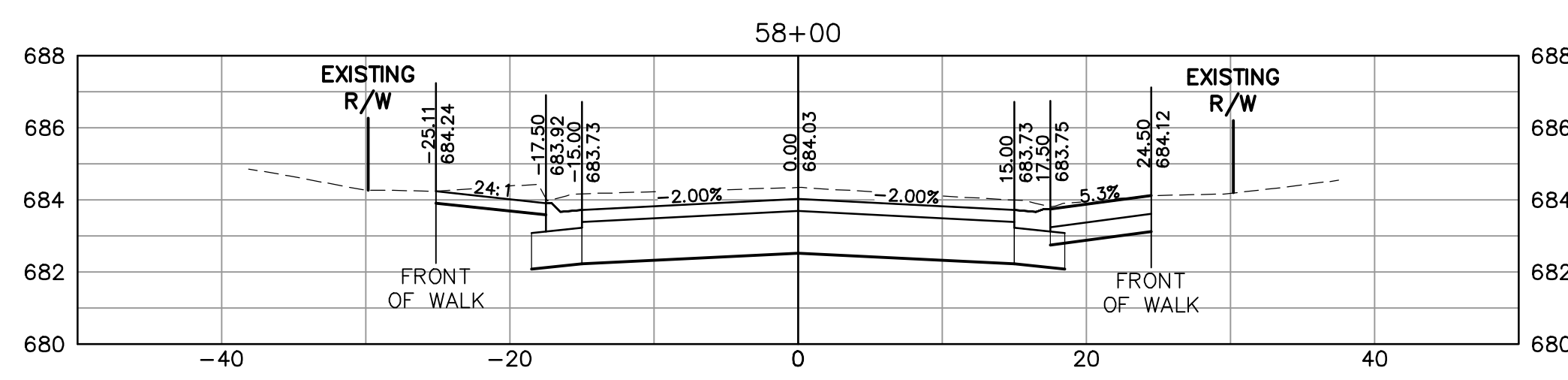
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SHEET NUMBER
30

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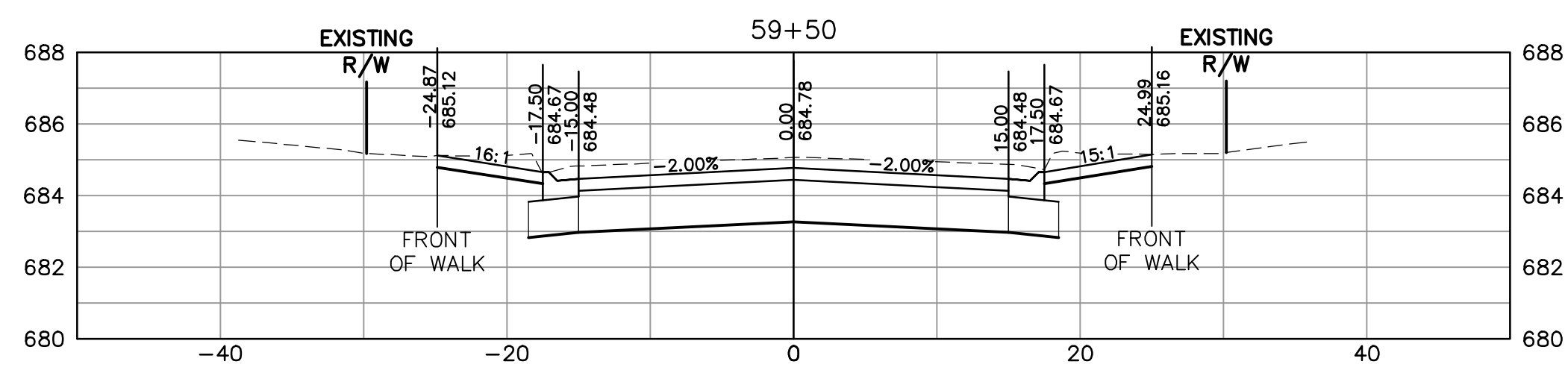
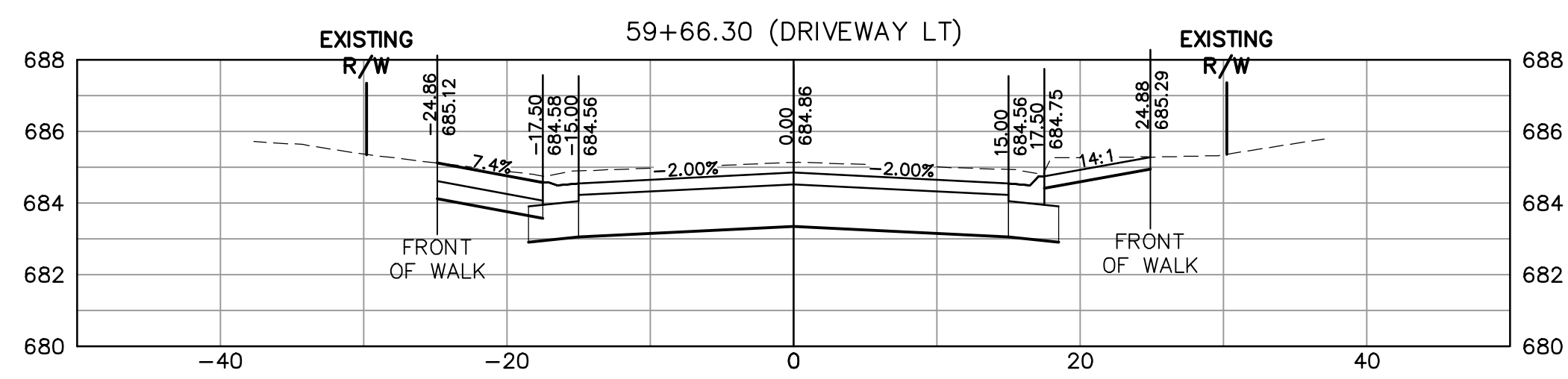
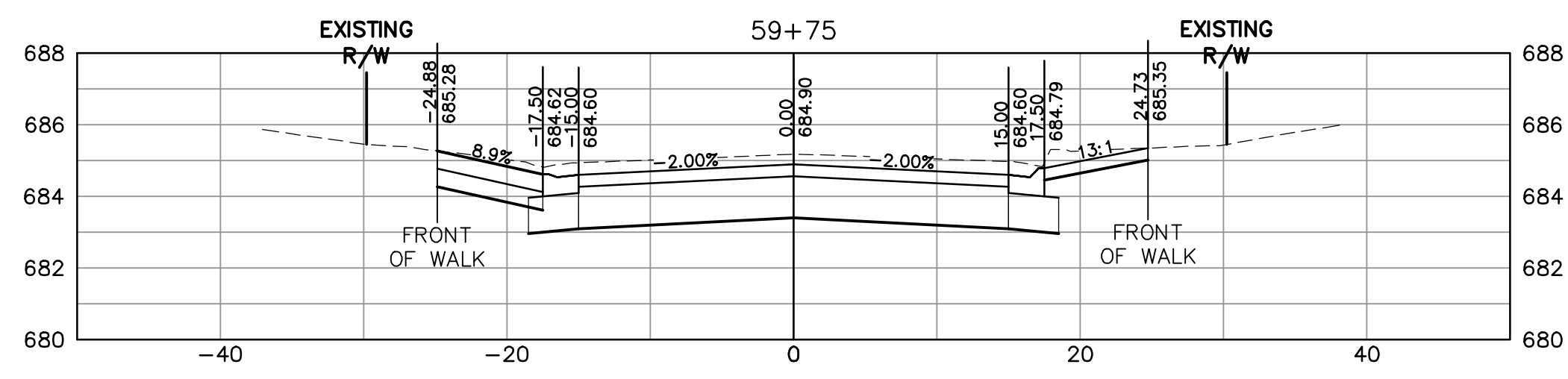
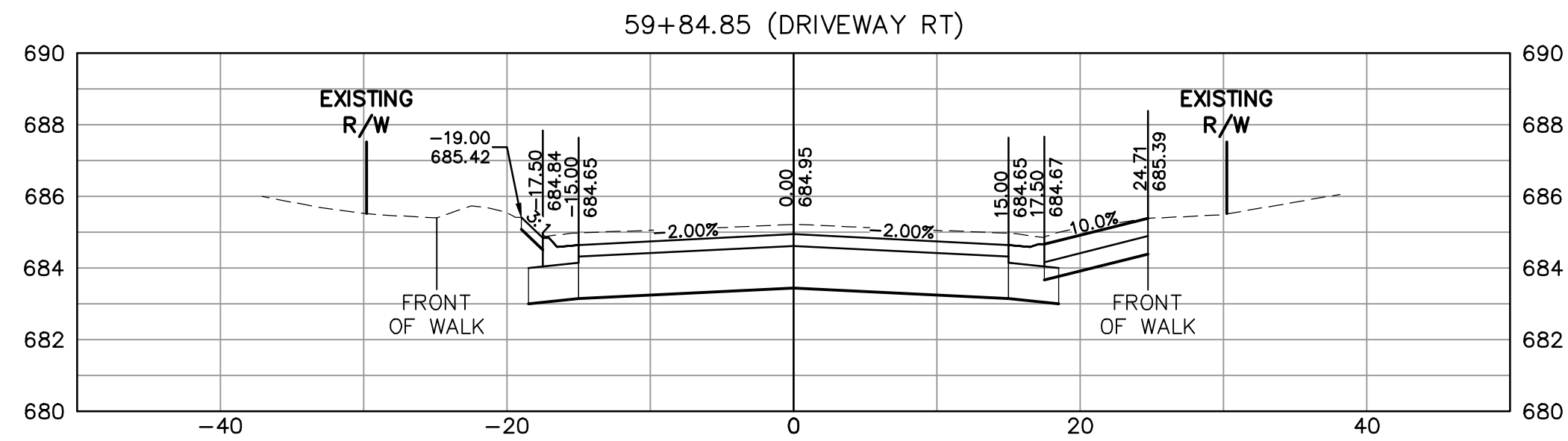
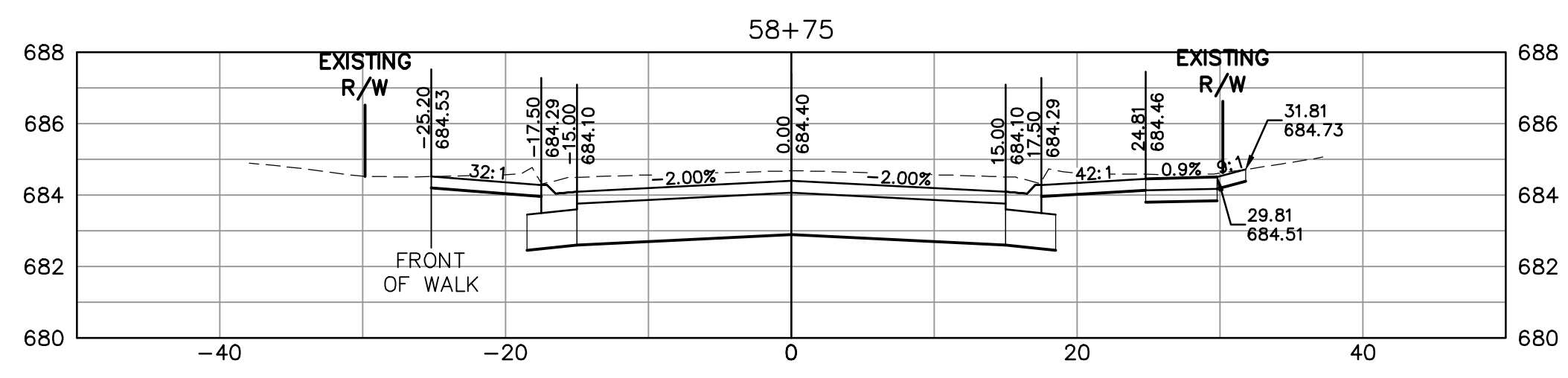
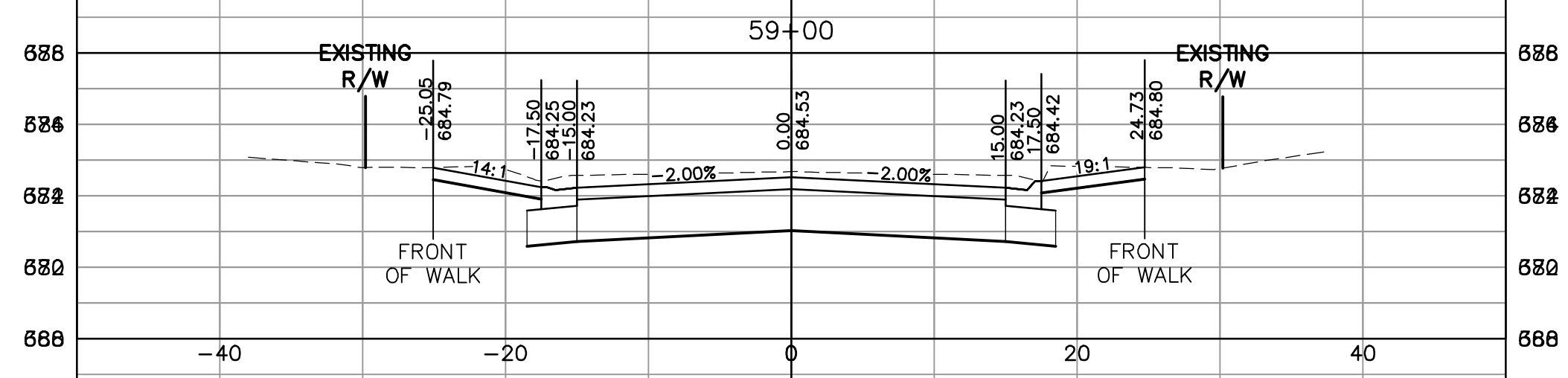
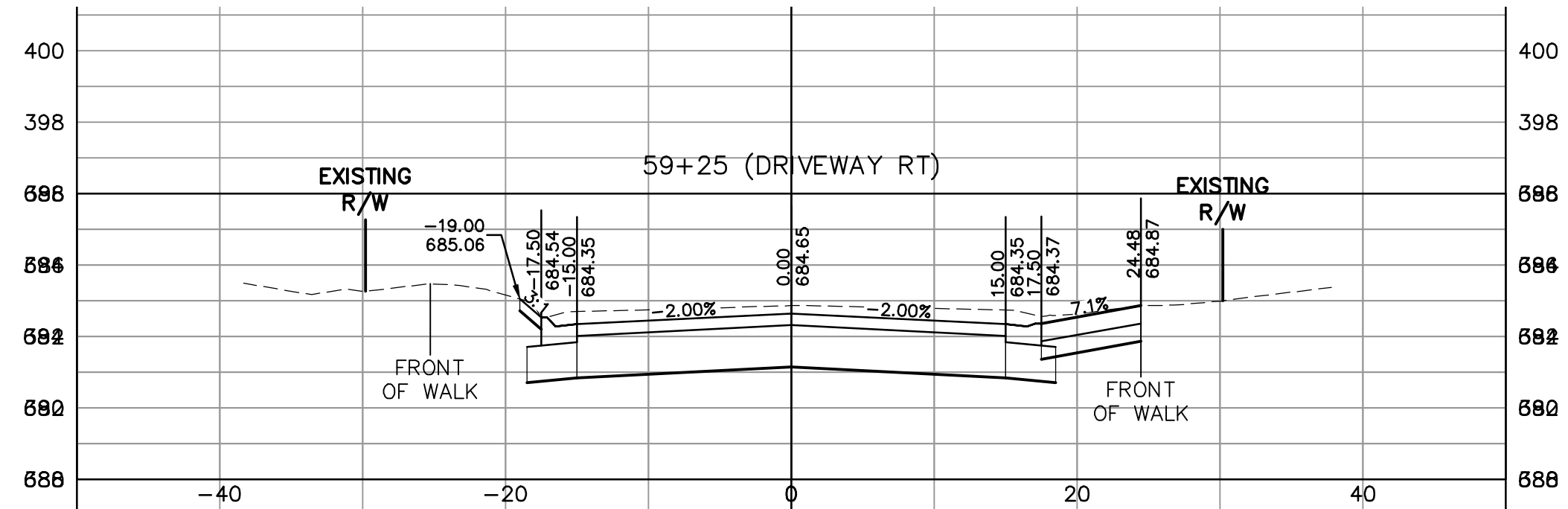
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SHEET NUMBER 31

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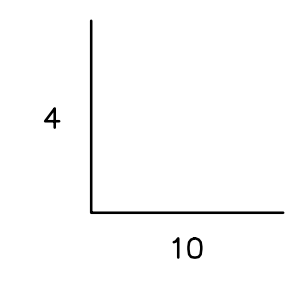
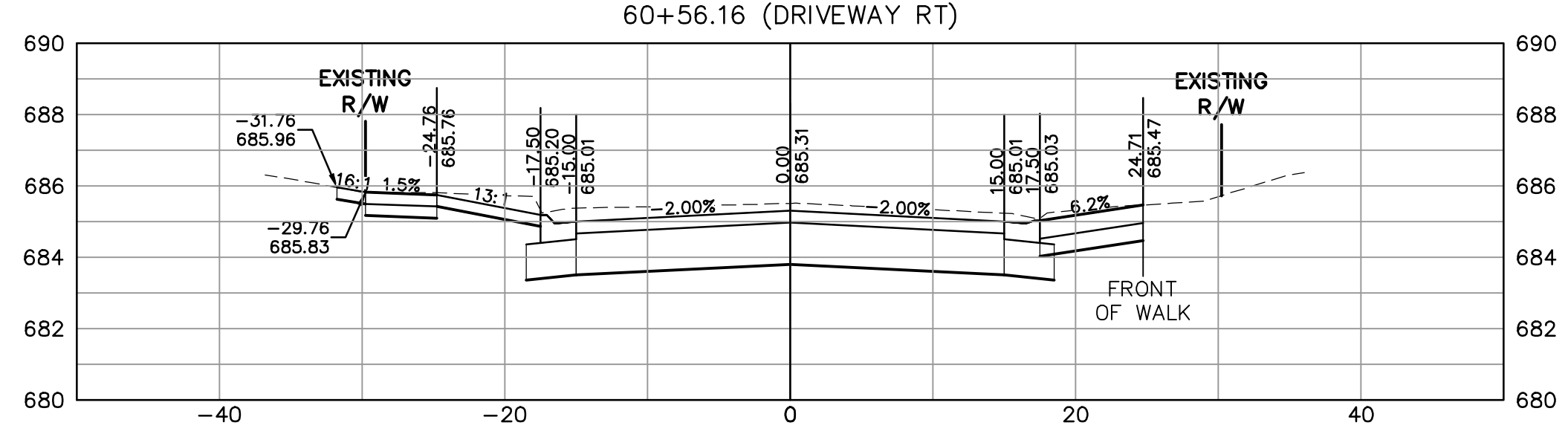
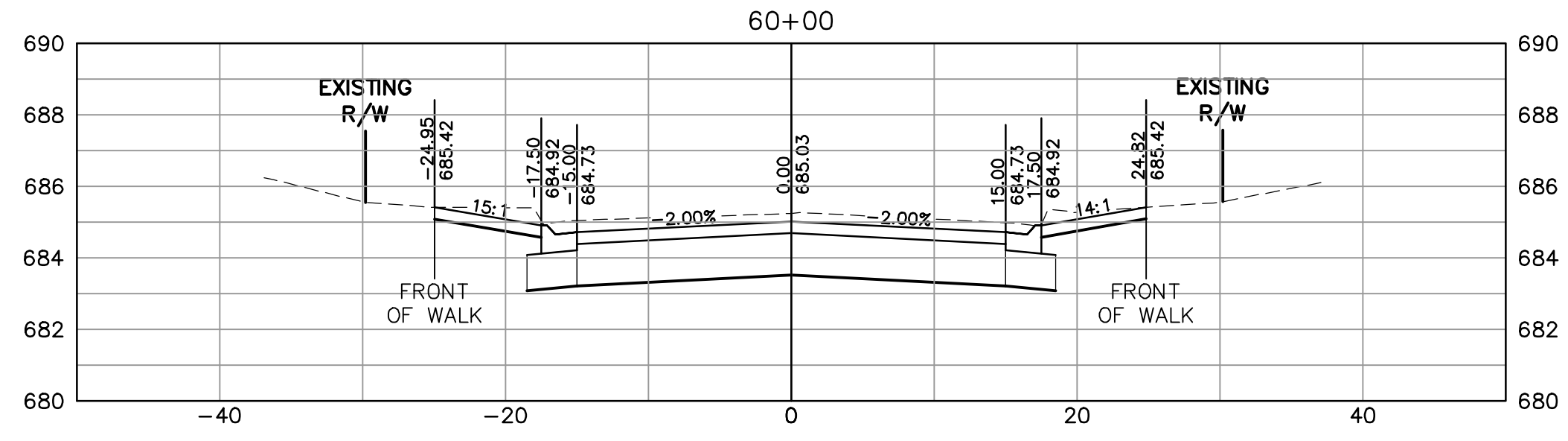
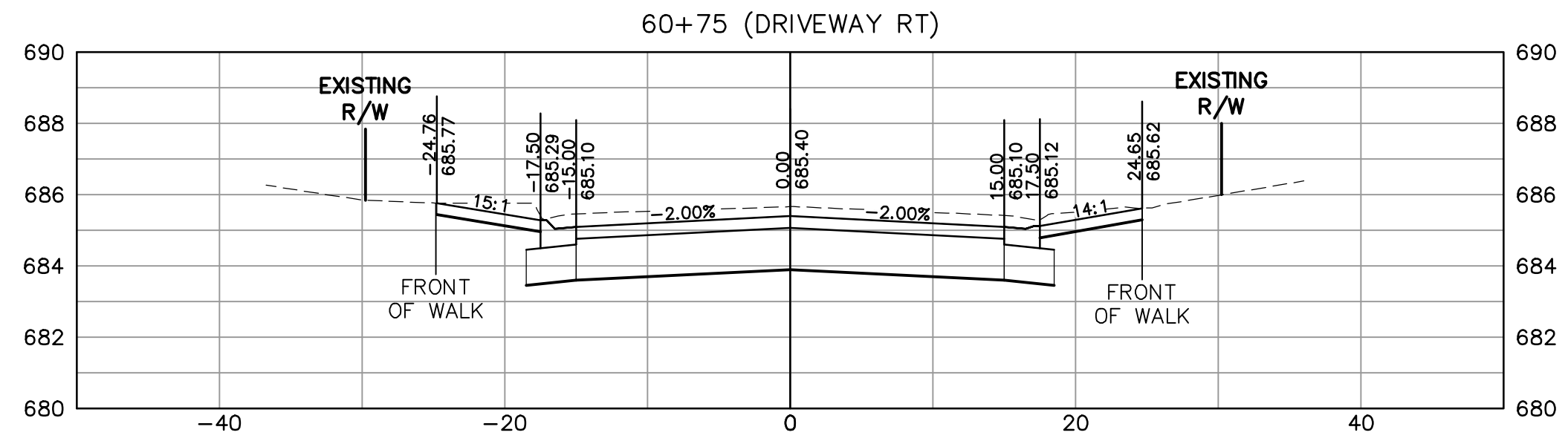
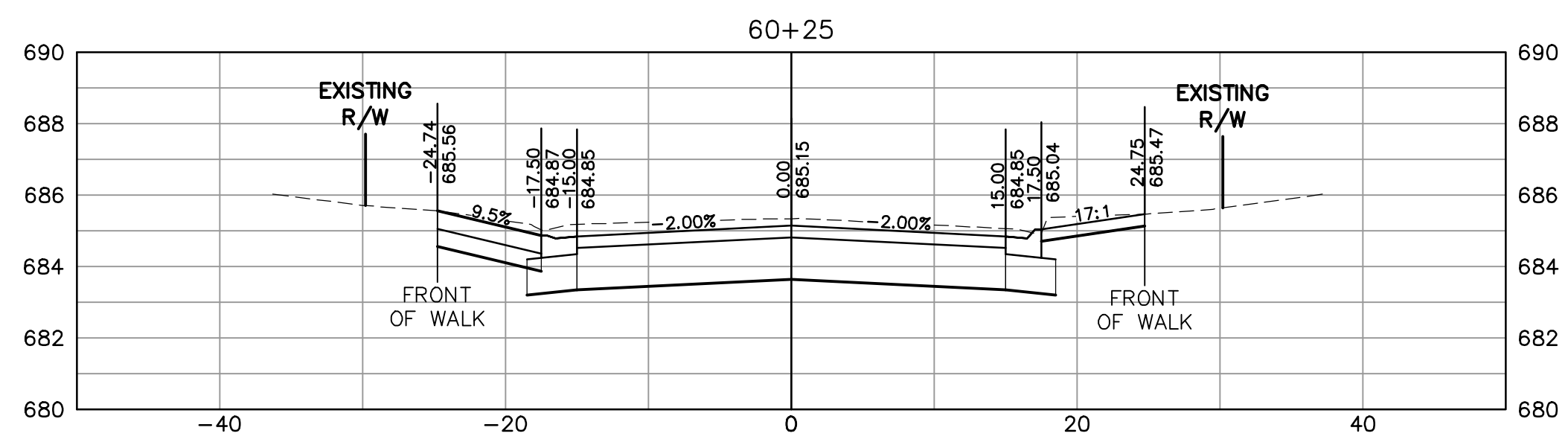
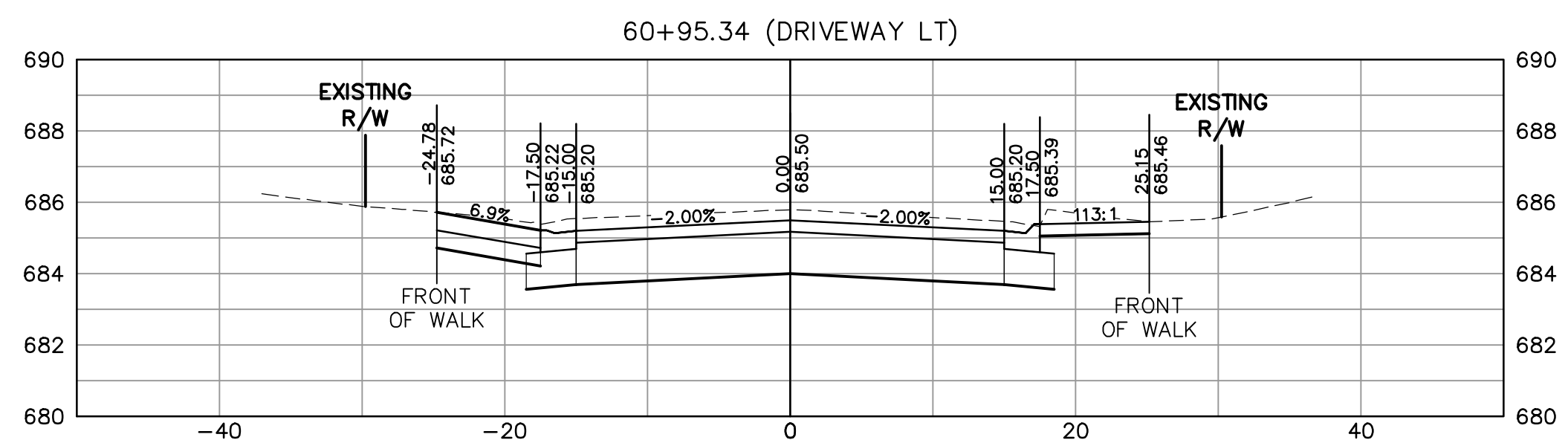
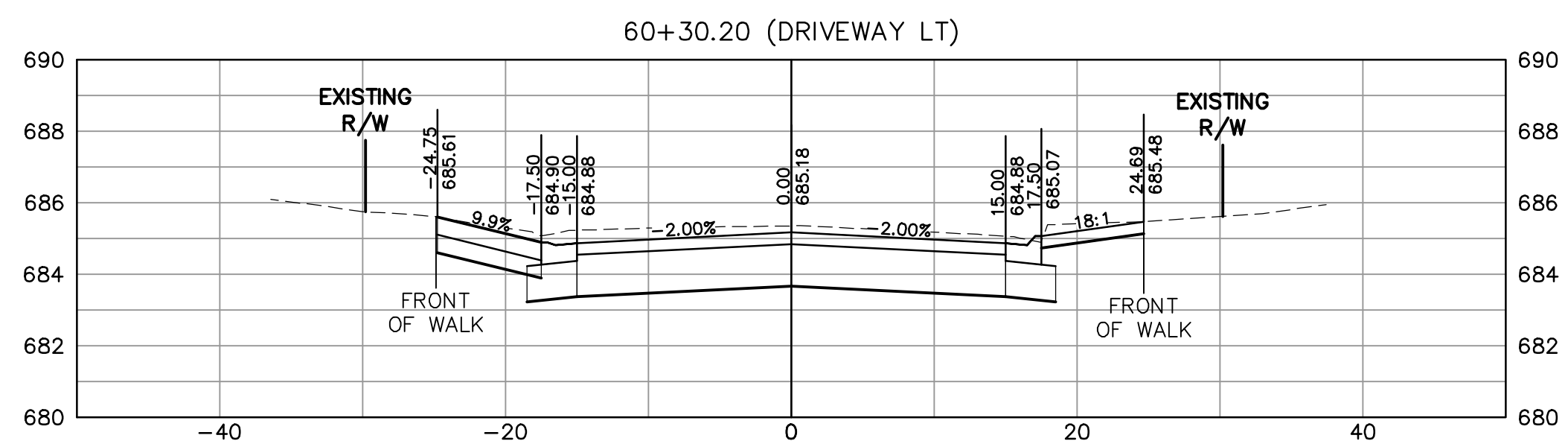
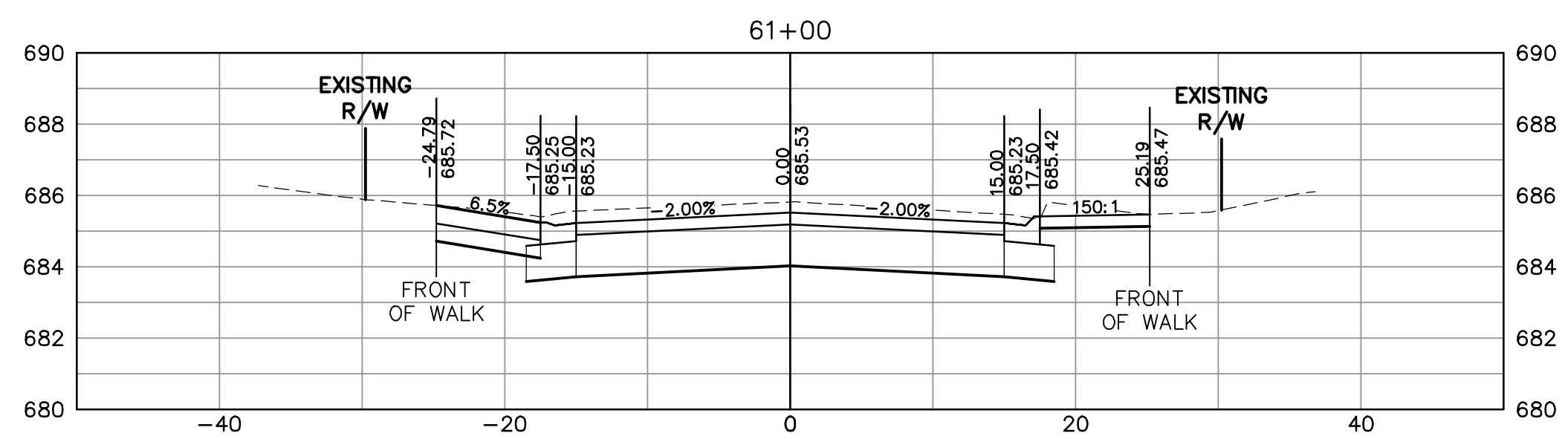
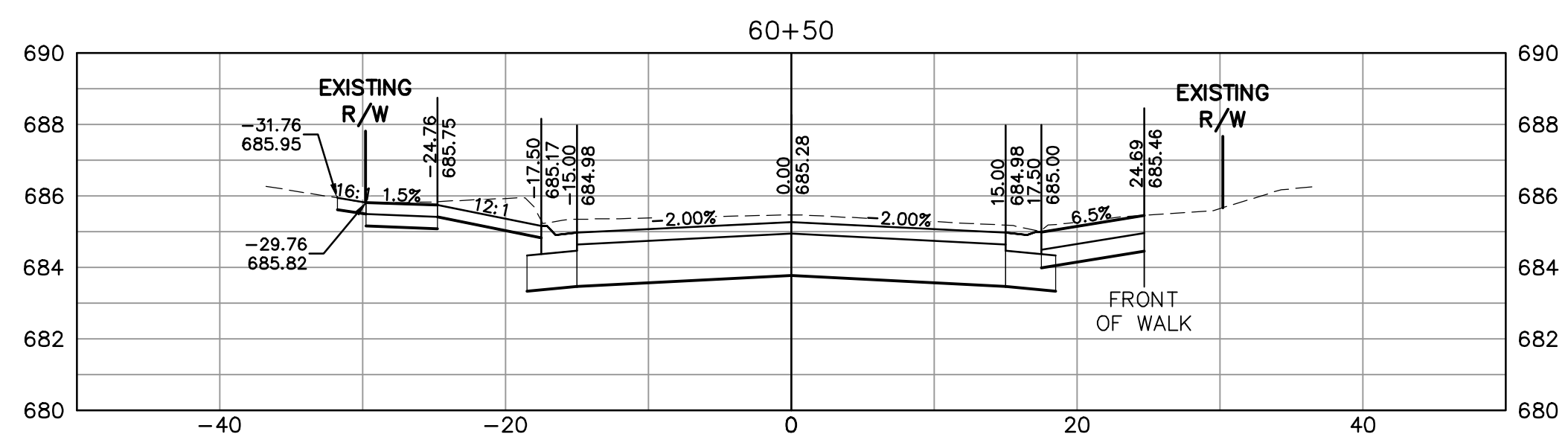

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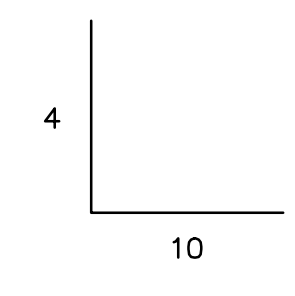
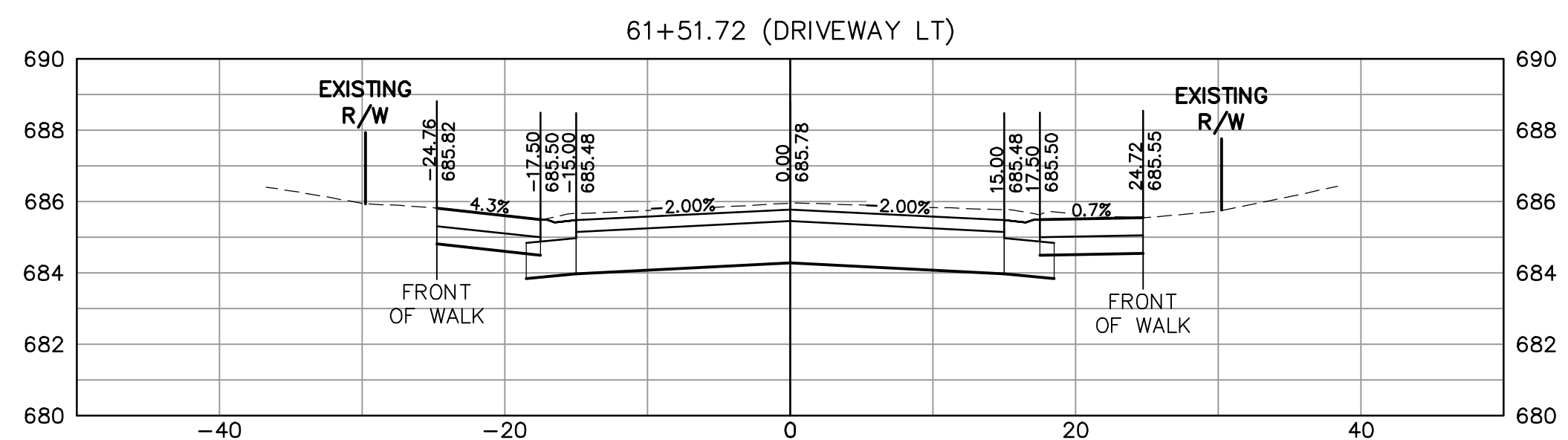
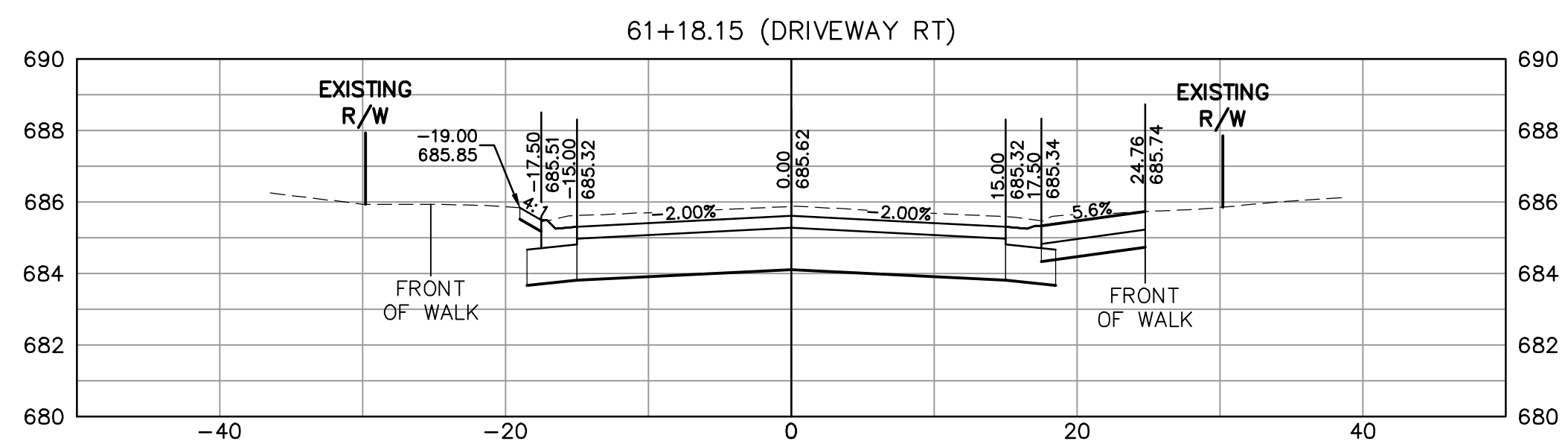
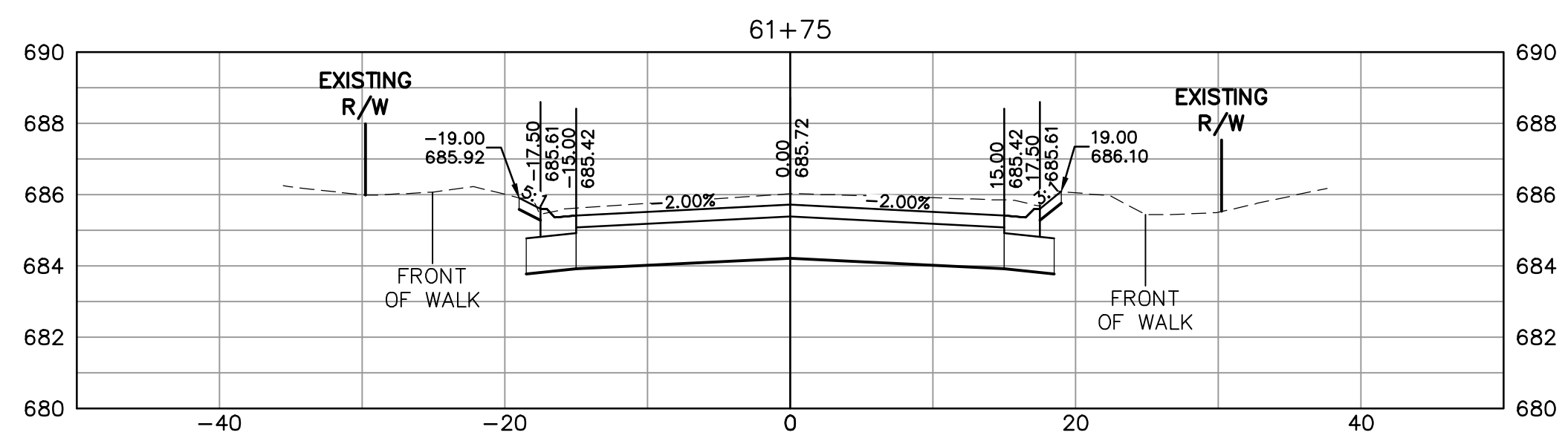
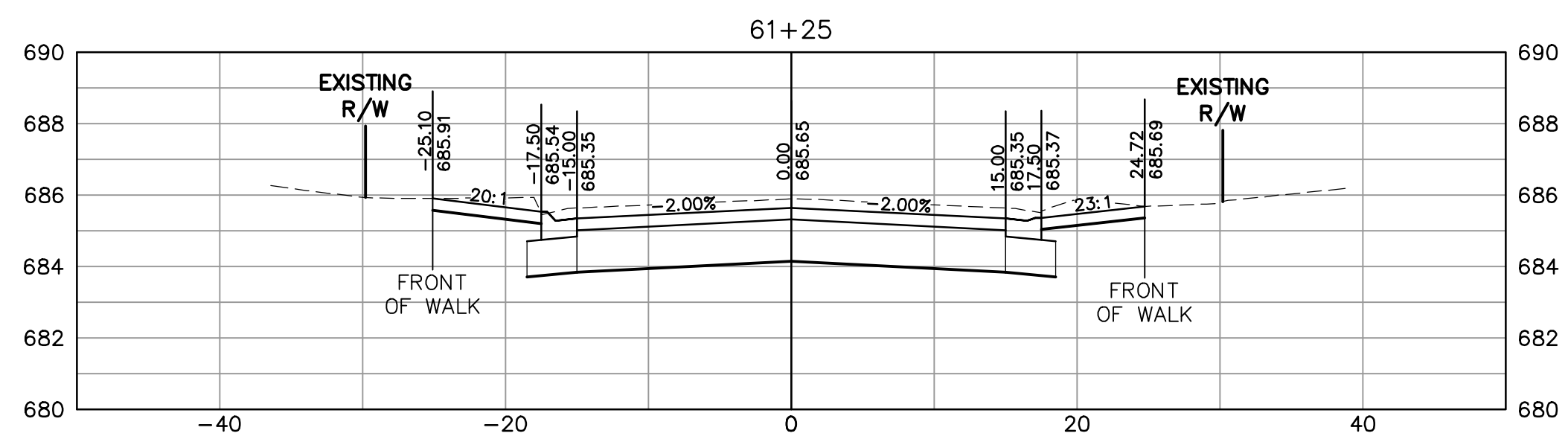
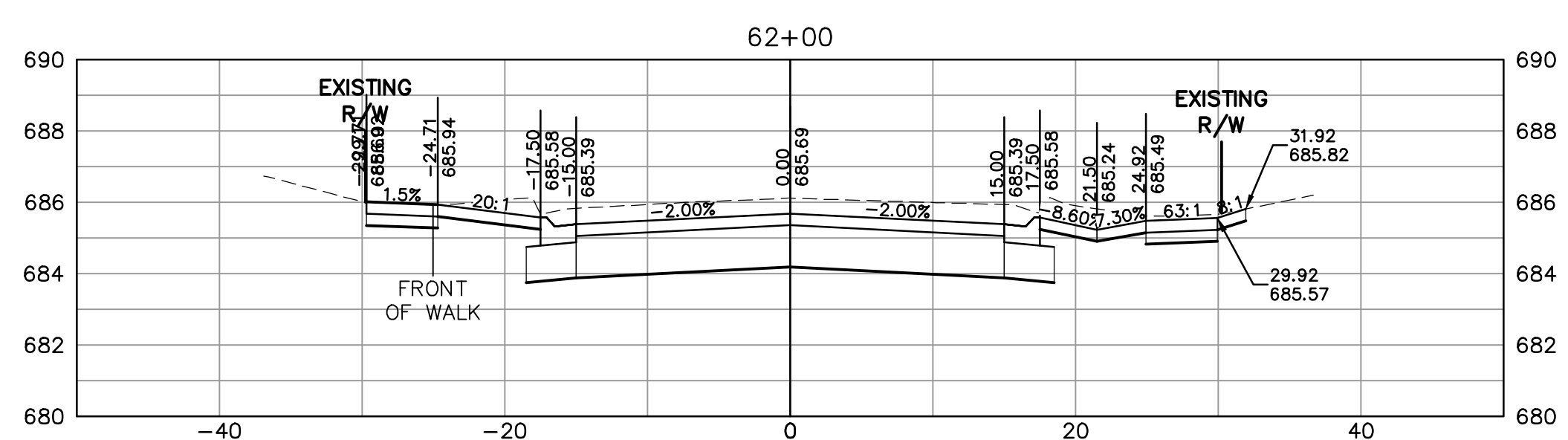
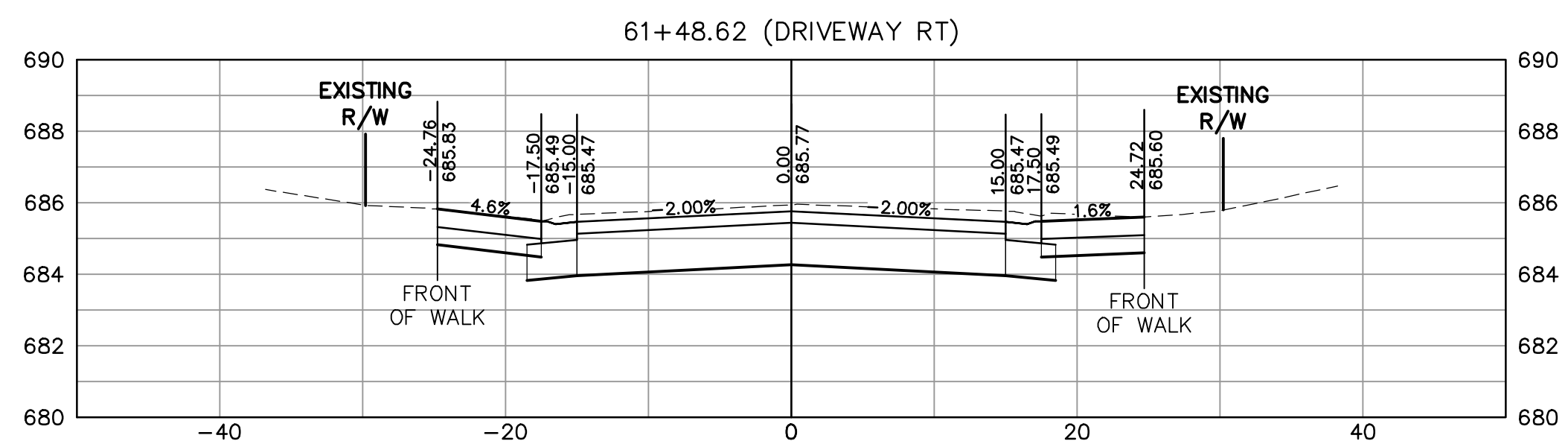
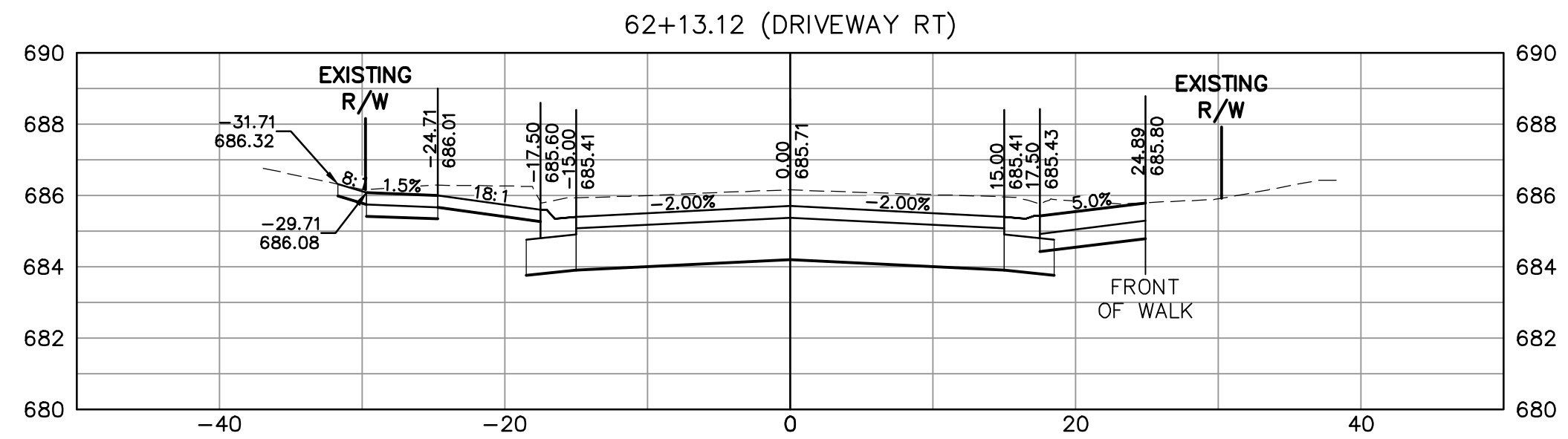
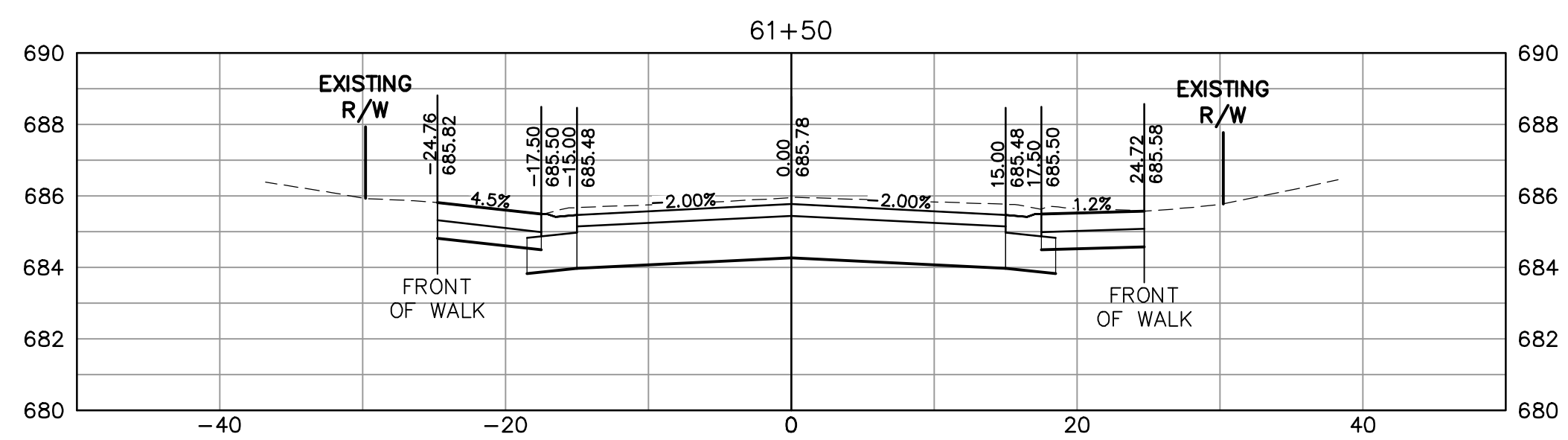
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PROJECT MANAGER: JASON FEUCHT, P.E.
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33

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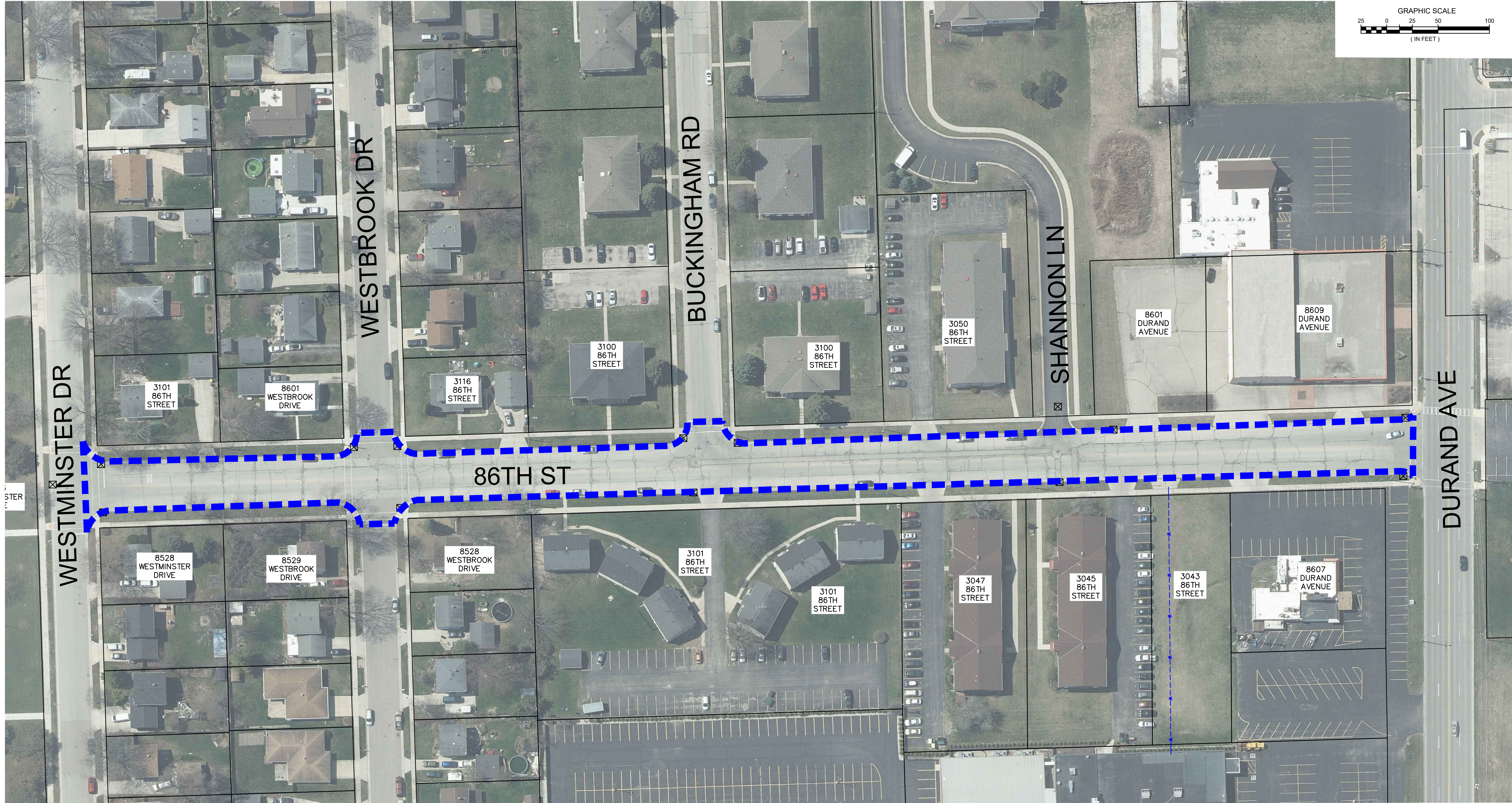
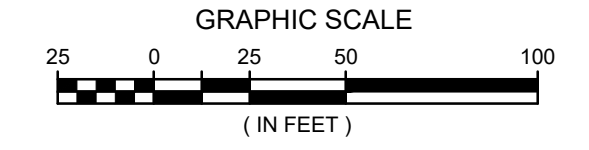
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CHECKED BY: JMF
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34

MEASUREMENTS		
STREET	LENGTH (FT)	WIDTH (FT)
86TH ST	1300	44



DATE	DESCRIPTION

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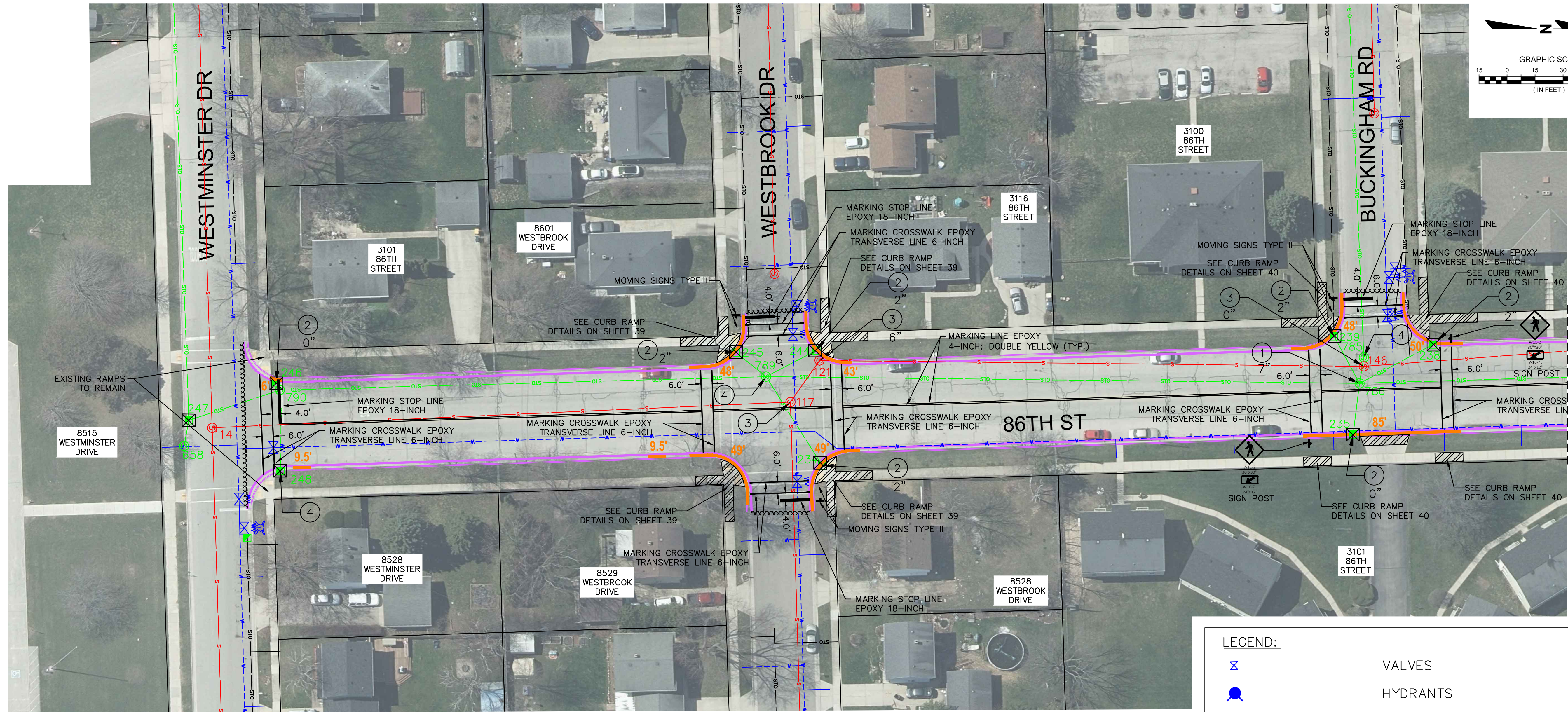
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VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN
2023 PAVING PROGRAM
SECTION 3
DRAFT
NOT FOR CONSTRUCTION 86TH STREET-OVERVIEW

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DATE: 05/02/2023
SCALE: 1" = 50'
JOB NO. 2232100
PROJECT MANAGER: JASON FEUCHT, P.E.
DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
36

H:\2232100\Draw\Sheets\2232100-E-002.dwg, 36 86TH STREET-OVERVIEW, 5/2/2023 12:28:05 PM, rjw



(SEE SHEET 38)

NOTE:

INLET PROTECTION, TYPE B REQUIRED AT ALL STORM MANHOLES WITH OPEN GRATES. INLET PROTECTION, TYPE C REQUIRED AT ALL CURBSIDE INLETS.

THE ROADWAY MATCH POINTS WILL BE MARKED BY ENGINEER PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL COORDINATE WITH RACINE WATER UTILITY FOR WATER VALVE BOX ADJUSTMENTS.

REMOVED FRAMES AND GRATES/COVERS SHALL BE DELIVERED TO THE OWNER.

PROTECT DRIVEWAY AND LANDSCAPE WALLS/ITEMS DURING CONSTRUCTION.

CONTRACTOR'S RESPONSIBILITY TO STAKE CENTERLINE OF EXISTING PAVEMENT.

ALL CONCRETE CURB AND GUTTER SPOT REPLACEMENT SHALL MATCH THE EXISTING ADJACENT CURB AND GUTTER TYPE. CURB AND GUTTER TO BE MARKED IN THE FIELD BY THE ENGINEER.

CONTRACTOR SHALL MAINTAIN ACCESS TO DRIVEWAYS AT ALL TIMES.

STRUCTURE LEGEND

- | | | | |
|----------|---|---|--------------------------------|
| ①
"X" | ADJUSTING STORM MANHOLES (CHIMNEY DEPTH) | ④ | TUCKPOINTING STORM STRUCTURES |
| ②
"X" | ADJUSTING INLETS (CHIMNEY DEPTH) | ⑤ | TUCKPOINTING SANITARY MANHOLES |
| ③
"X" | ADJUSTING SANITARY MANHOLES (CHIMNEY DEPTH) | | |

LEGEND:

- | | |
|--|---|
| | VALVES |
| | HYDRANTS |
| | WATER MAINS |
| | CATCH BASINS |
| | STORM SEWER MANHOLES |
| | STORM SEWER |
| | SANITARY MANHOLE |
| | SANITARY SEWER |
| | EXISTING CURB & GUTTER |
| | SAWCUT PAVEMENT |
| | CONCRETE CURB AND GUTTER SPOT REPLACEMENT (LF) |
| | SIDEWALK REMOVAL (PAID AS REMOVING CONCRETE PAVEMENT) |
| | INLET PROTECTION TYPE B (SEE DETAIL) |
| | INLET PROTECTION TYPE C (SEE DETAIL) |

VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN
2023 PAVING PROGRAM
SECTION 3
86TH STREET

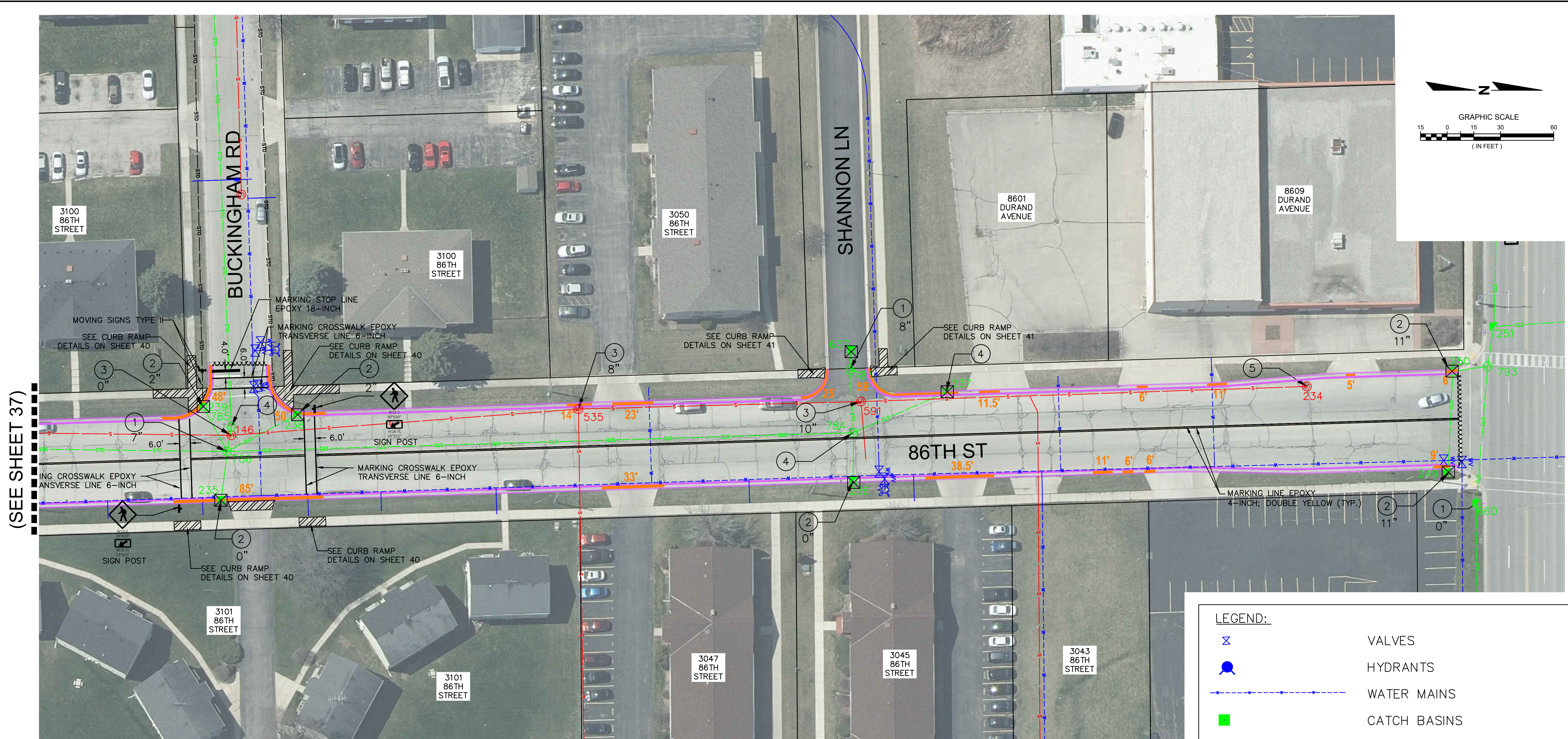
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DATE: 05/02/2023
SCALE: 1" = 30'
JOB NO. 2232100
PROJECT MANAGER:
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DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
37

raSmith
CREATIVITY BEYOND ENGINEERING
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(262) 781-1000
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Cedarburg, WI | Naperville, IL | Irvine, CA

DATE	DESCRIPTION

H:\2232100\Draw\Sheets\2232100-E301.dwg, 37 86TH STREET, 5/2/2023 12:28:16 PM, djw



(SEE SHEET 37)

NOTE:

INLET PROTECTION, TYPE B REQUIRED AT ALL STORM MANHOLES WITH OPEN GRATES. INLET PROTECTION, TYPE C REQUIRED AT ALL CURBSIDE INLETS.

THE ROADWAY MATCH POINTS WILL BE MARKED BY ENGINEER PRIOR TO CONSTRUCTION.

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CONTRACTOR SHALL MAINTAIN ACCESS TO DRIVEWAYS AT ALL TIMES.

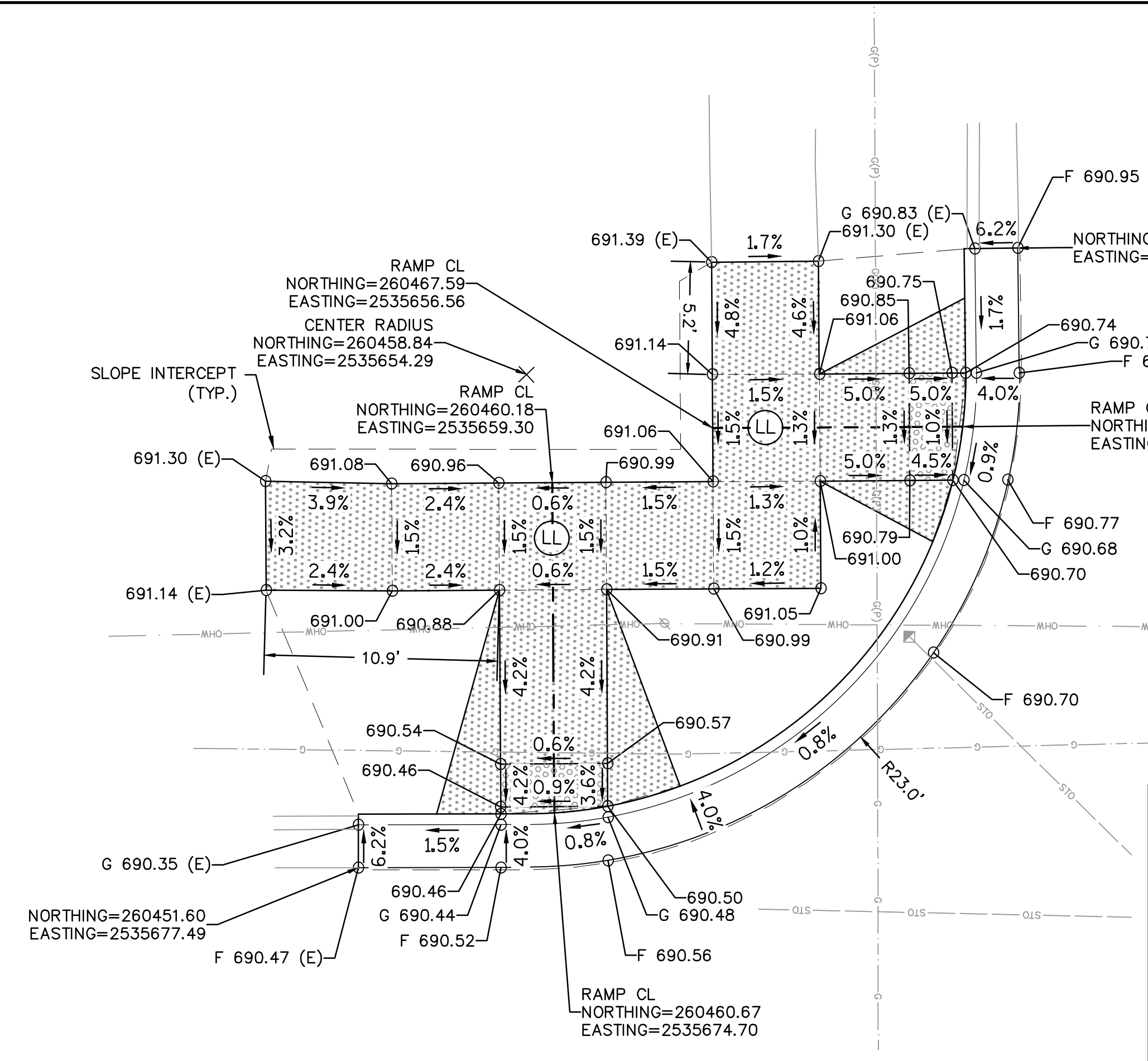
STRUCTURE LEGEND

① X"	ADJUSTING STORM MANHOLES (CHIMNEY DEPTH)	④	TUCKPOINTING STORM STRUCTURES
② X"	ADJUSTING INLETS (CHIMNEY DEPTH)	⑤	TUCKPOINTING SANITARY MANHOLES
③ X"	ADJUSTING SANITARY MANHOLES (CHIMNEY DEPTH)		

LEGEND:

	VALVES
	HYDRANTS
	WATER MAINS
	CATCH BASINS
	STORM SEWER MANHOLES
	STORM SEWER
	SANITARY MANHOLE
	SANITARY SEWER
	EXISTING CURB & GUTTER
	SAWCUT PAVEMENT
	CONCRETE CURB AND GUTTER SPOT REPLACEMENT (LF)
	SIDEWALK REMOVAL (PAID AS REMOVING CONCRETE PAVEMENT)
	INLET PROTECTION TYPE B (SEE DETAIL)
	INLET PROTECTION TYPE C (SEE DETAIL)

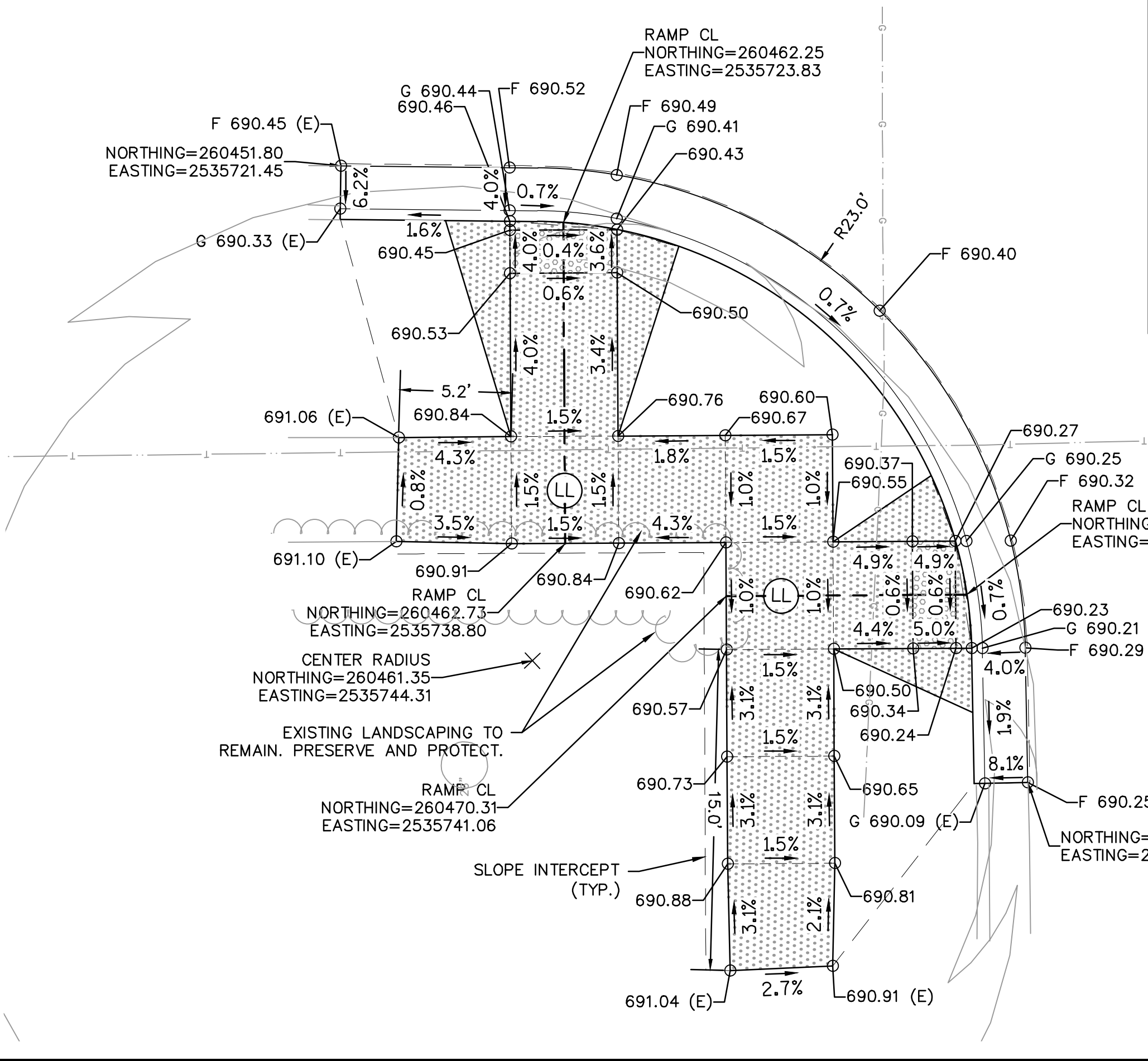
DESCRIPTION	
DATE	
<p>16745 W. Bluemound Road Brookfield, WI 53005-5938 (262) 781-1000 rasmith.com</p> <p>raSmith CREATIVITY BEYOND ENGINEERING</p> <p>Brookfield, WI Milwaukee, WI Appleton, WI Madison, WI Cedarburg, WI Naperville, IL Irvine, CA</p>	
VILLAGE OF STURTEVANT RACINE COUNTY, WISCONSIN	2023 PAVING PROGRAM SECTION 3 86TH STREET
DRAFT NOT FOR CONSTRUCTION	
<p>© COPYRIGHT 2023 R.A. Smith, Inc. DATE: 05/02/2023 SCALE: 1" = 30' JOB NO. 2232100 PROJECT MANAGER: JASON FEUCHT, P.E. DESIGNED BY: RJW CHECKED BY: JMF</p>	
SHEET NUMBER	38



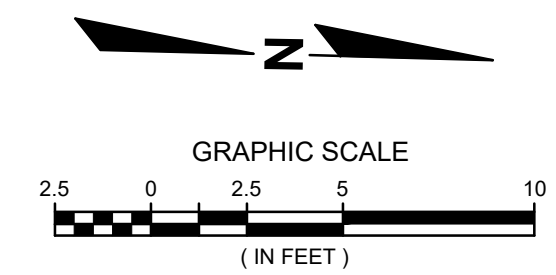
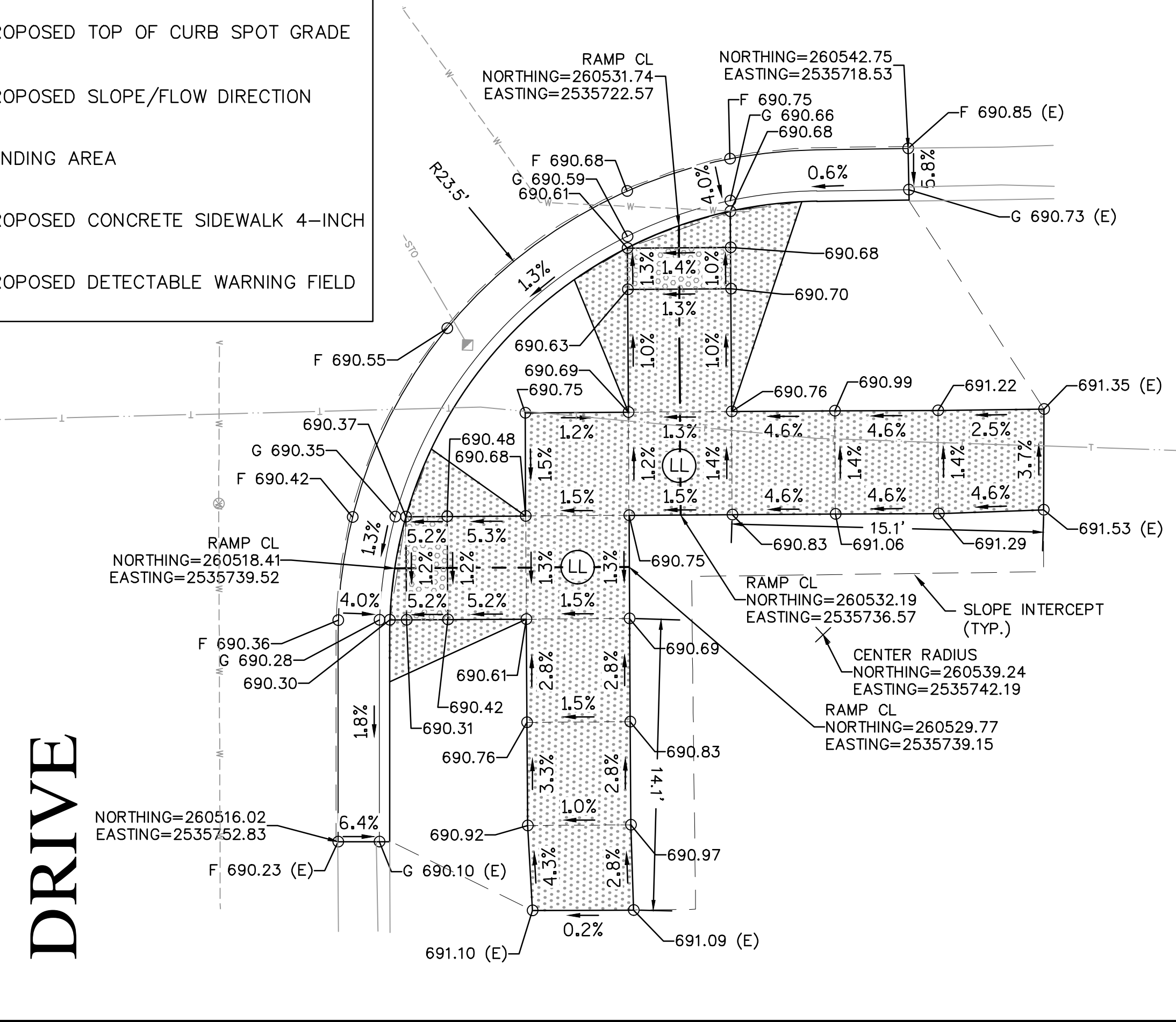
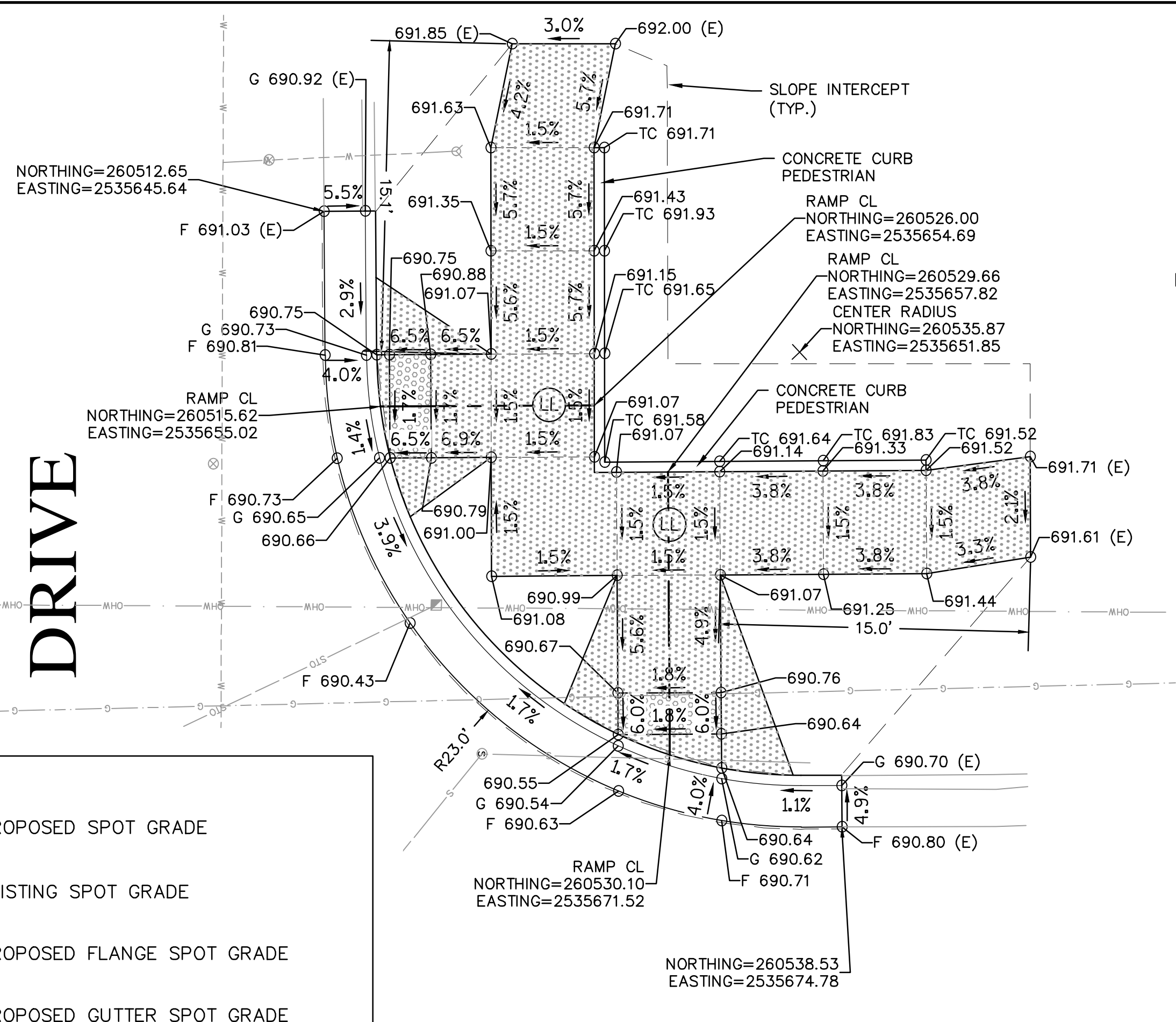
WESTBROCK DRIVE

LEGEND

XXX.XX	PROPOSED SPOT GRADE
XXX.XX (E)	EXISTING SPOT GRADE
F XXX.XX	PROPOSED FLANGE SPOT GRADE
G XXX.XX	PROPOSED GUTTER SPOT GRADE
TC XXX.XX	PROPOSED TOP OF CURB SPOT GRADE
1.5%	PROPOSED SLOPE/FLOW DIRECTION
(LL)	LANDING AREA
[Pattern]	PROPOSED CONCRETE SIDEWALK 4-INCH
[Pattern]	PROPOSED DETECTABLE WARNING FIELD

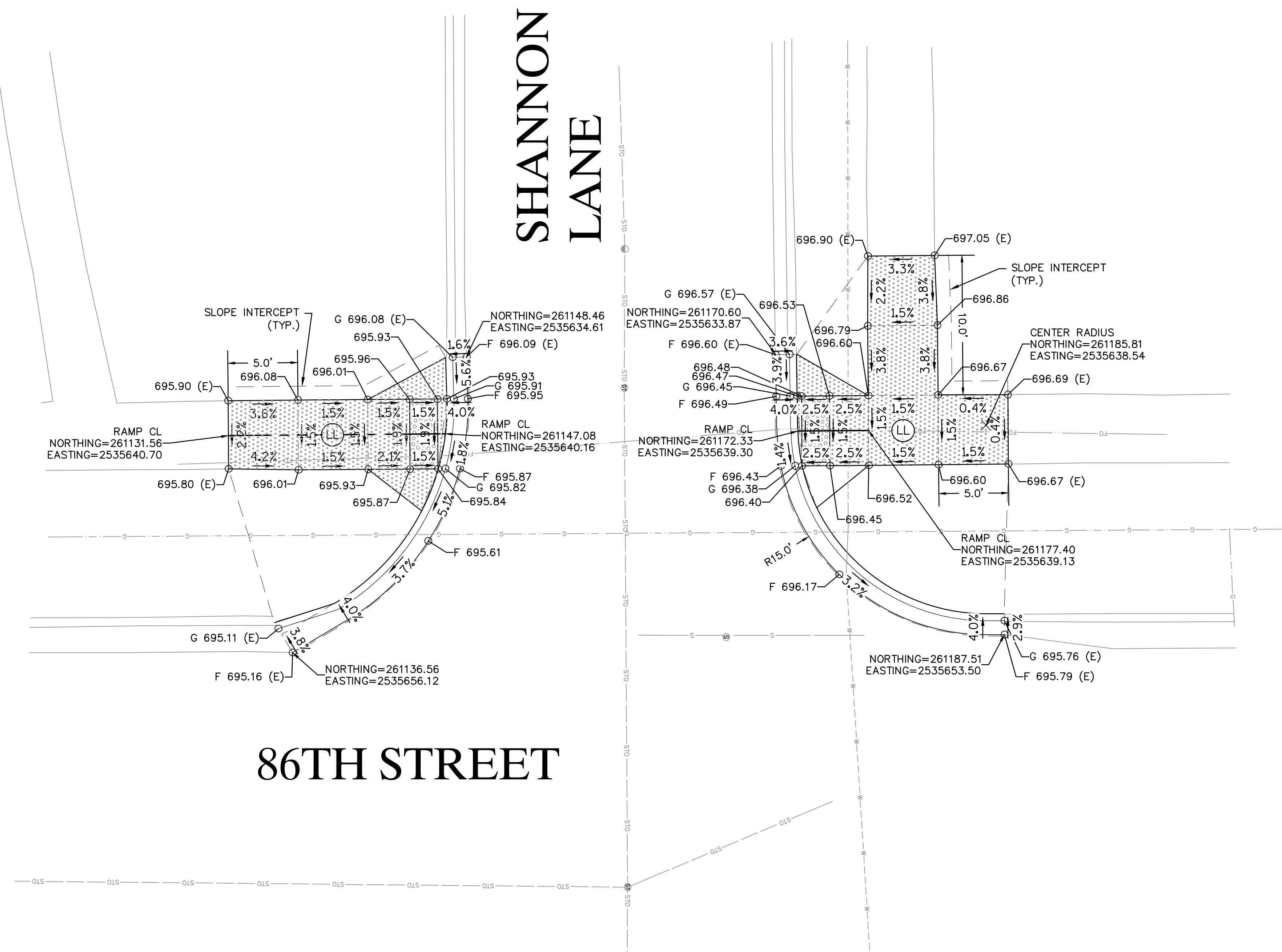
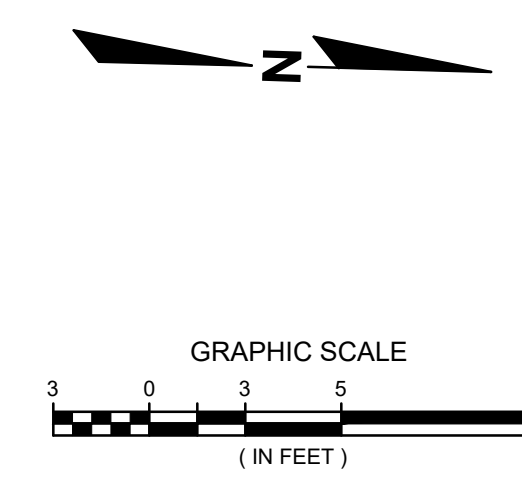


WESTBROCK DRIVE



- NOTES:**
1. ALL CURB RAMP OPENINGS SHOULD BE 5.5' ALONG B.O.C. TO ACCOMMODATE ROOM FOR DETECTABLE WARNING FIELDS.
 2. CONTRACTOR TO COORDINATE WITH PROPERTY OWNERS FOR REMOVAL OF LANDSCAPE ITEMS NEAR BACK OF WALK TO ALLOW FOR NEW SIDEWALK CONSTRUCTION. ALL EXISTING CURB AND SIDEWALK SHALL BE SAWCUT FULL DEPTH AND TO THE NEAREST JOINT.
 3. ALL NEW CONCRETE CURB & GUTTER SHALL BE 30" TYPE D CURB UNLESS OTHERWISE NOTED.
 4. ALL SPOT GRADES ARE FINISHED GRADES. CURB GRADES ARE FLANGE GRADES UNLESS OTHERWISE NOTED.
 5. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 6. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
 7. DASHED LINES SHOWN REPRESENT RAMP CENTERLINES, JOINTS, AND BREAK POINT LOCATIONS WITHIN THE RAMP.
 8. SEE DETAILS IN BACK OF PLAN FOR ADDITIONAL CURB RAMP DETAILS.

DESCRIPTION	
DATE	
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Village of Sturtevant Racine County, Wisconsin 2023 PAVING PROGRAM SECTION 3 DRAFT NOT FOR CONSTRUCTION 86TH STREET - CURB RAMP DETAILS	
© COPYRIGHT 2023 R.A. Smith, Inc. DATE: 05/02/2023 SCALE: 1" = 5' JOB NO. 2232100 PROJECT MANAGER: JASON FEUCHT, P.E. DESIGNED BY: RJW CHECKED BY: JMF SHEET NUMBER 39	



- NOTES:
1. ALL CURB RAMP OPENINGS SHOULD BE 5.5' ALONG B.O.C. TO ACCOMMODATE ROOM FOR DETECTABLE WARNING FIELDS.
 2. CONTRACTOR TO COORDINATE WITH PROPERTY OWNERS FOR REMOVAL OF LANDSCAPE ITEMS NEAR BACK OF WALK TO ALLOW FOR NEW SIDEWALK CONSTRUCTION.
 3. ALL EXISTING CURB AND SIDEWALK SHALL BE SAWCUT FULL DEPTH AND TO THE NEAREST JOINT.
 4. ALL NEW CONCRETE CURB & GUTTER SHALL BE 30" TYPE D CURB UNLESS OTHERWISE NOTED.
 5. ALL SPOT GRADES ARE FINISHED GRADES. CURB GRADES ARE FLANGE GRADES UNLESS OTHERWISE NOTED.
 6. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 7. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
 8. DASHED LINES SHOWN REPRESENT RAMP CENTERLINES, JOINTS, AND BREAK POINT LOCATIONS WITHIN THE RAMP.
 9. SEE DETAILS IN BACK OF PLAN FOR ADDITIONAL CURB RAMP DETAILS.

LEGEND	
XXX.XX	PROPOSED SPOT GRADE
XXX.XX (E)	EXISTING SPOT GRADE
F XXX.XX	PROPOSED FLANGE SPOT GRADE
G XXX.XX	PROPOSED GUTTER SPOT GRADE
TC XXX.XX	PROPOSED TOP OF CURB SPOT GRADE
1.5%	PROPOSED SLOPE/FLOW DIRECTION
(LL)	LANDING AREA
[Pattern]	PROPOSED CONCRETE SIDEWALK 4-INCH
[Pattern]	PROPOSED DETECTABLE WARNING FIELD

DATE	DESCRIPTION

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VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
SECTION 3
86TH STREET - CURB RAMP DETAILS

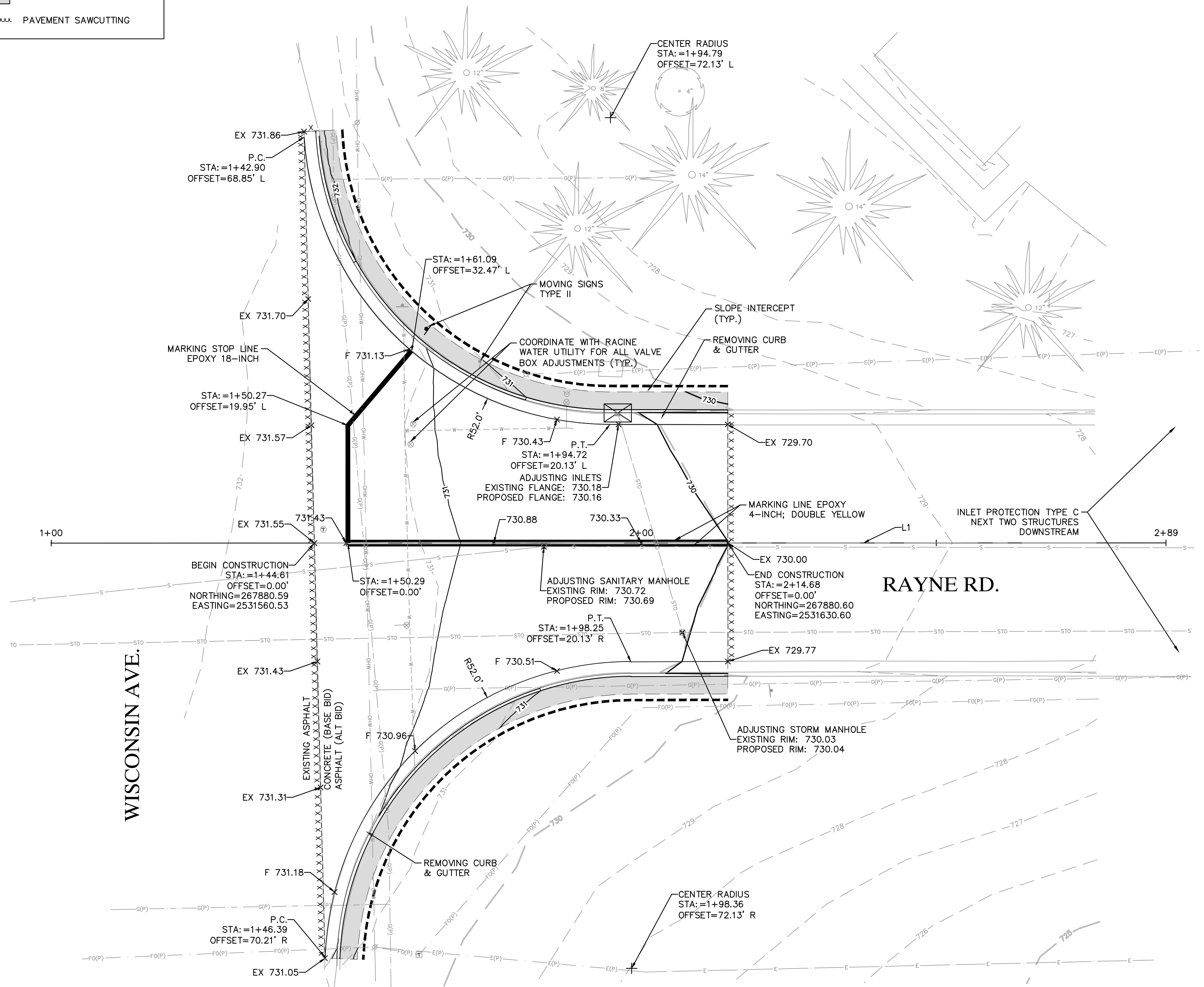
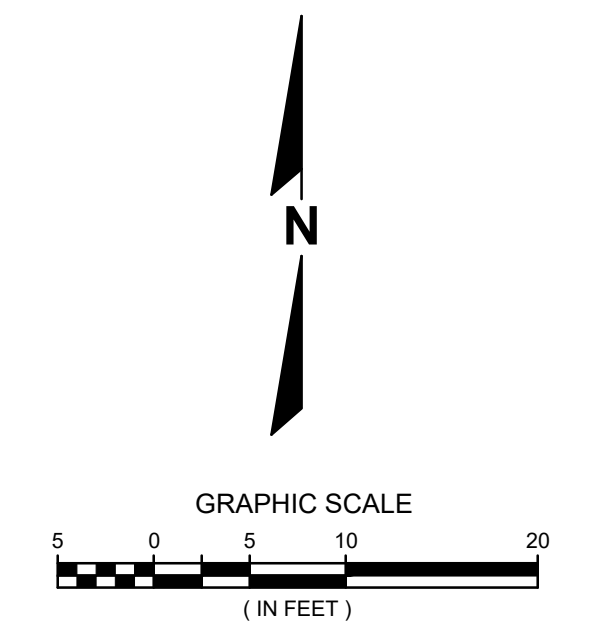
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JOB NO. 2232100
PROJECT MANAGER: JASON FEUCHT, P.E.
DESIGNED BY: RJW
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SHEET NUMBER
41

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LEGEND

	INLET PROTECTION (TYPE C) (SEE DETAIL)		EXISTING CONTOUR
	PROPOSED SPOT ELEVATION		PROPOSED CONTOUR
	PROPOSED FLANGE GRADE SPOT ELEVATION		LAWN RESTORATION
	SILT FENCE		PAVEMENT SAWCUTTING



CENTERLINE RAYNE																
Number	PI (STA)	PI (Y)	PI (X)	DELTA	DEGREE	TANGENT	LENGTH	EXTERNAL	RADIUS	PC (STA)	PT (STA)	DB	DA	END PI (STA)	END PI (Y)	END PI (X)
L1	1+00.00	267880.58	2531515.92										N89°59'27"E	2+88.95	267880.61	2531704.87

DESCRIPTION	
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VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN

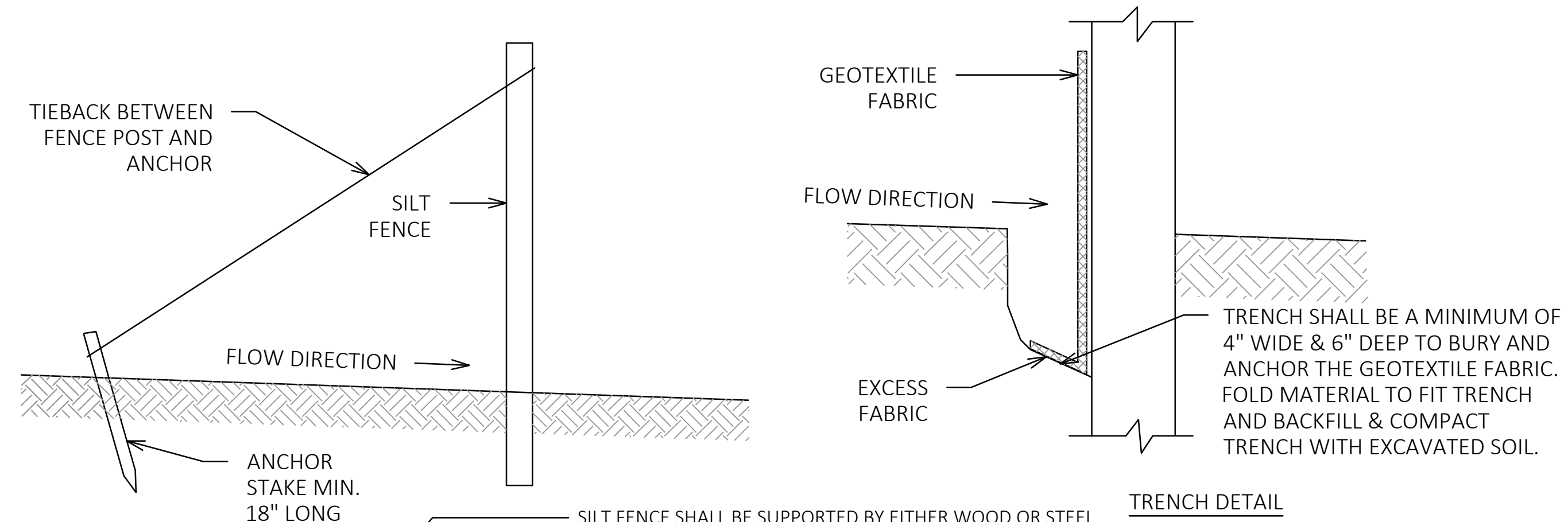
2023 PAVING PROGRAM
SECTION 4
RAYNE ROAD

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DATE: 05/02/2023
SCALE: 1" = 10'
JOB NO. 2232100
PROJECT MANAGER: JASON FEUCHT, P.E.
DESIGNED BY: RJW
CHECKED BY: JMF

SHEET NUMBER
42

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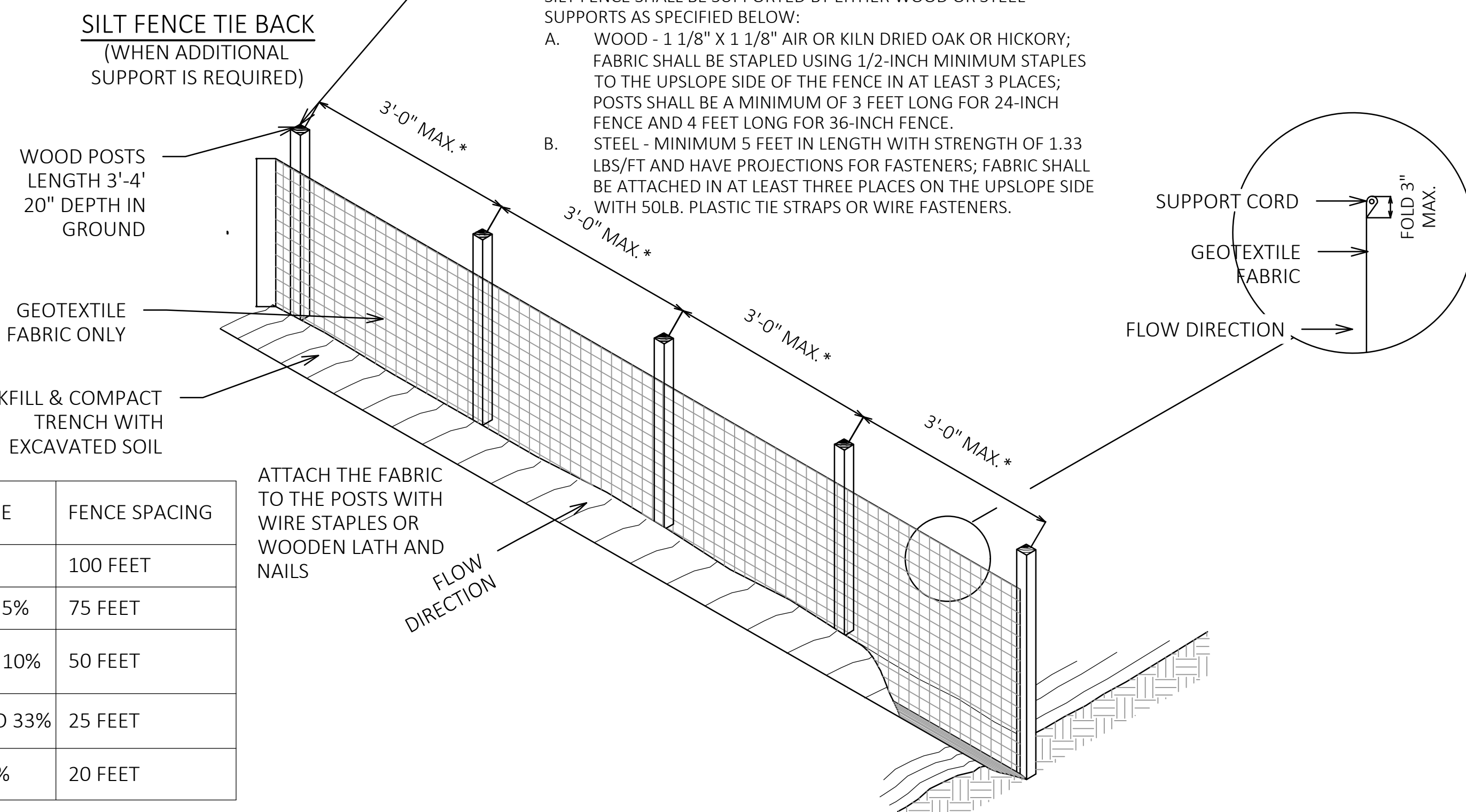
TRENCH DETAIL

TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.

SILT FENCE SHALL BE SUPPORTED BY EITHER WOOD OR STEEL SUPPORTS AS SPECIFIED BELOW:

A. WOOD - 1 1/8" X 1 1/8" AIR OR KILN DRIED OAK OR HICKORY; FABRIC SHALL BE STAPLED USING 1/2-INCH MINIMUM STAPLES TO THE UPSLOPE SIDE OF THE FENCE IN AT LEAST 3 PLACES; POSTS SHALL BE A MINIMUM OF 3 FEET LONG FOR 24-INCH FENCE AND 4 FEET LONG FOR 36-INCH FENCE.

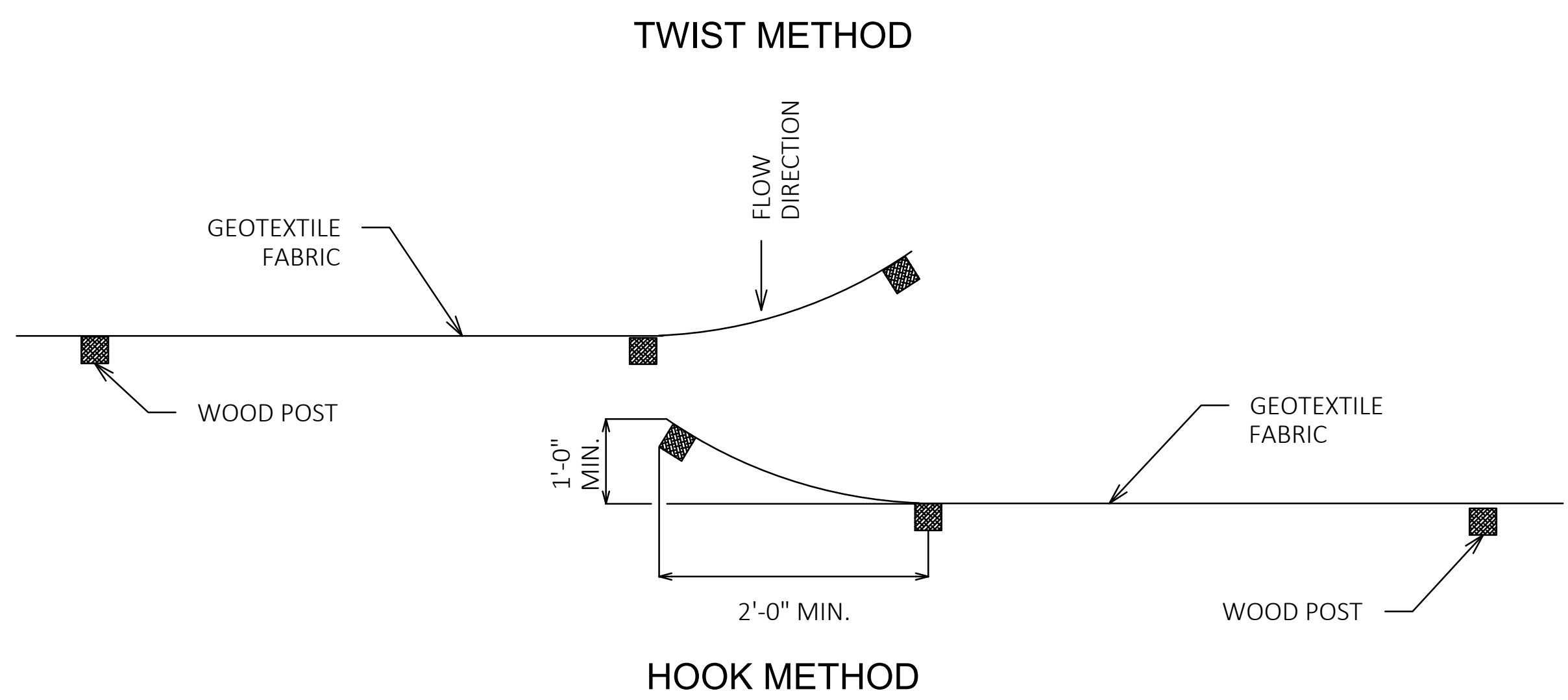
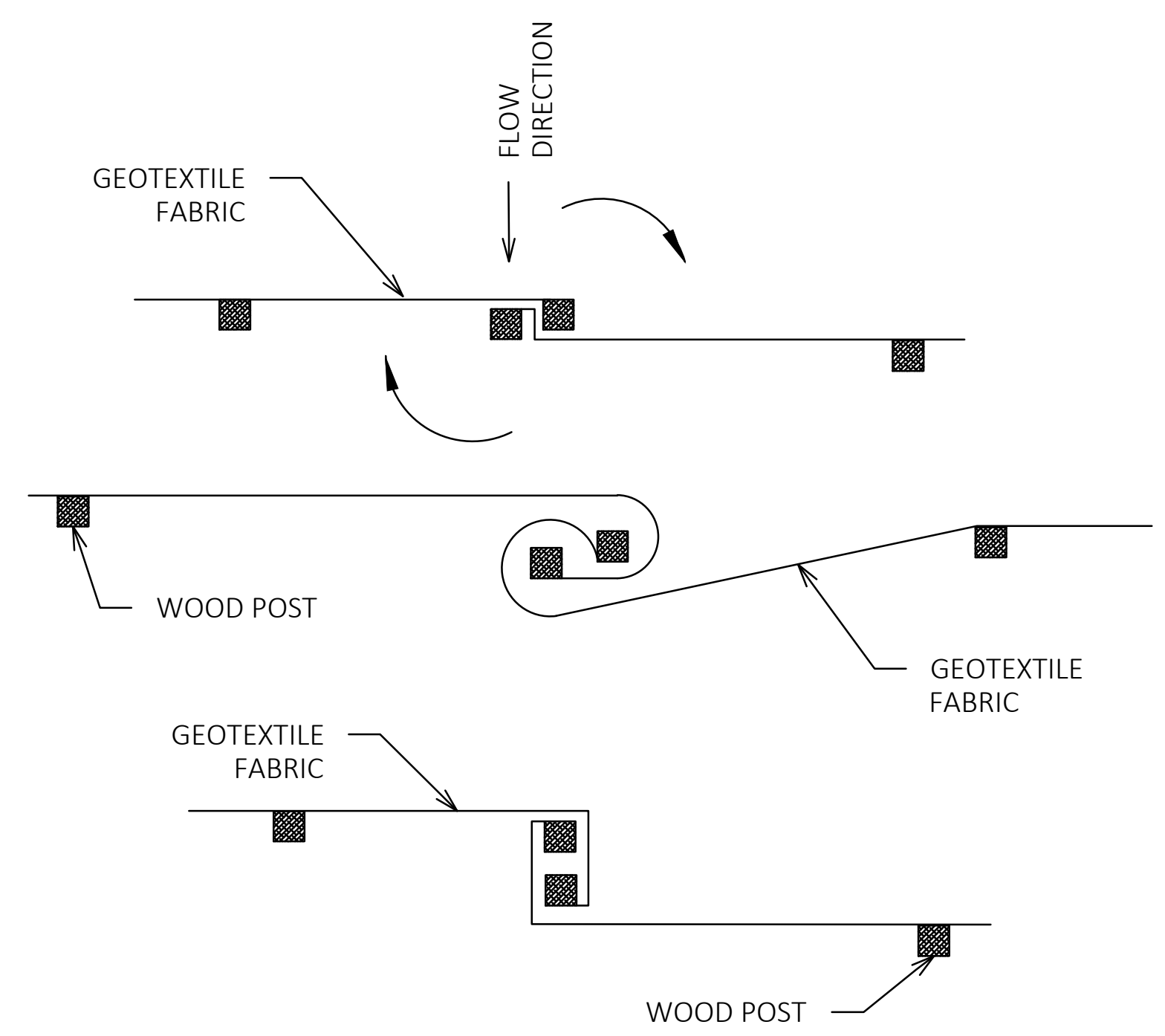
B. STEEL - MINIMUM 5 FEET IN LENGTH WITH STRENGTH OF 1.33 LBS/FT AND HAVE PROJECTIONS FOR FASTENERS; FABRIC SHALL BE ATTACHED IN AT LEAST THREE PLACES ON THE UPSLOPE SIDE WITH 50LB. PLASTIC TIE STRAPS OR WIRE FASTENERS.



SLOPE	FENCE SPACING
< 2%	100 FEET
2 TO 5%	75 FEET
5 TO 10%	50 FEET
10 TO 33%	25 FEET
> 33%	20 FEET

- NOTES:
- SILT FENCE INSTALLATION AND MATERIALS SHALL CONFORM TO WDNR CONSERVATION STANDARD 1056
 - SILT FENCE SHALL BE PLACED ON THE CONTOUR AND NOT PERPENDICULAR TO THE CONTOUR. THE ENDS SHALL BE EXTENDED UPSLOPE TO PREVENT WATER FROM FLOWING AROUND THE ENDS OF THE FENCE.
 - WHEN SILT FENCE IS INSTALLED ON A SLOPE, THE PARALLEL SPACING SHALL NOT EXCEED THE REQUIREMENTS IN THE TABLE ABOVE.
 - INSTALLED SILT FENCES SHALL BE MINIMUM 14 INCHES HIGH AND A MAXIMUM OF 28 INCHES HIGH MEASURED FROM THE INSTALLED GROUND ELEVATION.
 - A MINIMUM OF 20 INCHES OF THE POST SHALL EXTEND INTO THE GROUND AFTER INSTALLATION.
 - SILT FENCE SHALL BE ANCHORED BY SPREADING AT LEAST 8 INCHES OF THE FABRIC IN A 4-INCH TRENCH WIDE BY 6-INCH DEEP TRENCH, OR 6-INCH V-TRENCH ON THE UPSLOPE SIDE OF THE FENCE. TRENCH SHALL BE BACKFILLED AND COMPACTED. TRENCHES SHALL NOT BE EXCAVATED WIDER THAN NECESSARY FOR PROPER INSTALLATION.
 - CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS:
 - TWIST METHOD--OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES.
 - HOOK METHOD--HOOK THE END OF EACH SILT FENCE LENGTH.
 - DAMAGED OR DECOMPOSED FENCES, UNDERCUTTING, OR FLOW CHANNELS AROUND THE END OF BARRIERS SHALL BE REPAIRED OR CORRECTED.
 - SEDIMENT SHALL BE PROPERLY DISPOSED OF ONCE THE DEPOSITS REACH ONE HALF THE HEIGHT OF THE FENCE.
 - SILT FENCES SHALL BE REMOVED ONCE THE DISTURBED AREA IS PERMANENTLY STABILIZED AND IS NO LONGER SUSCEPTIBLE TO EROSION.
 - ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS.
 - 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED. 3'-0" MAX FOR NON WOVEN FABRIC

SILT FENCE DETAIL



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RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
DETAILS

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SCALE: N.T.S.

JOB NO. 2232100

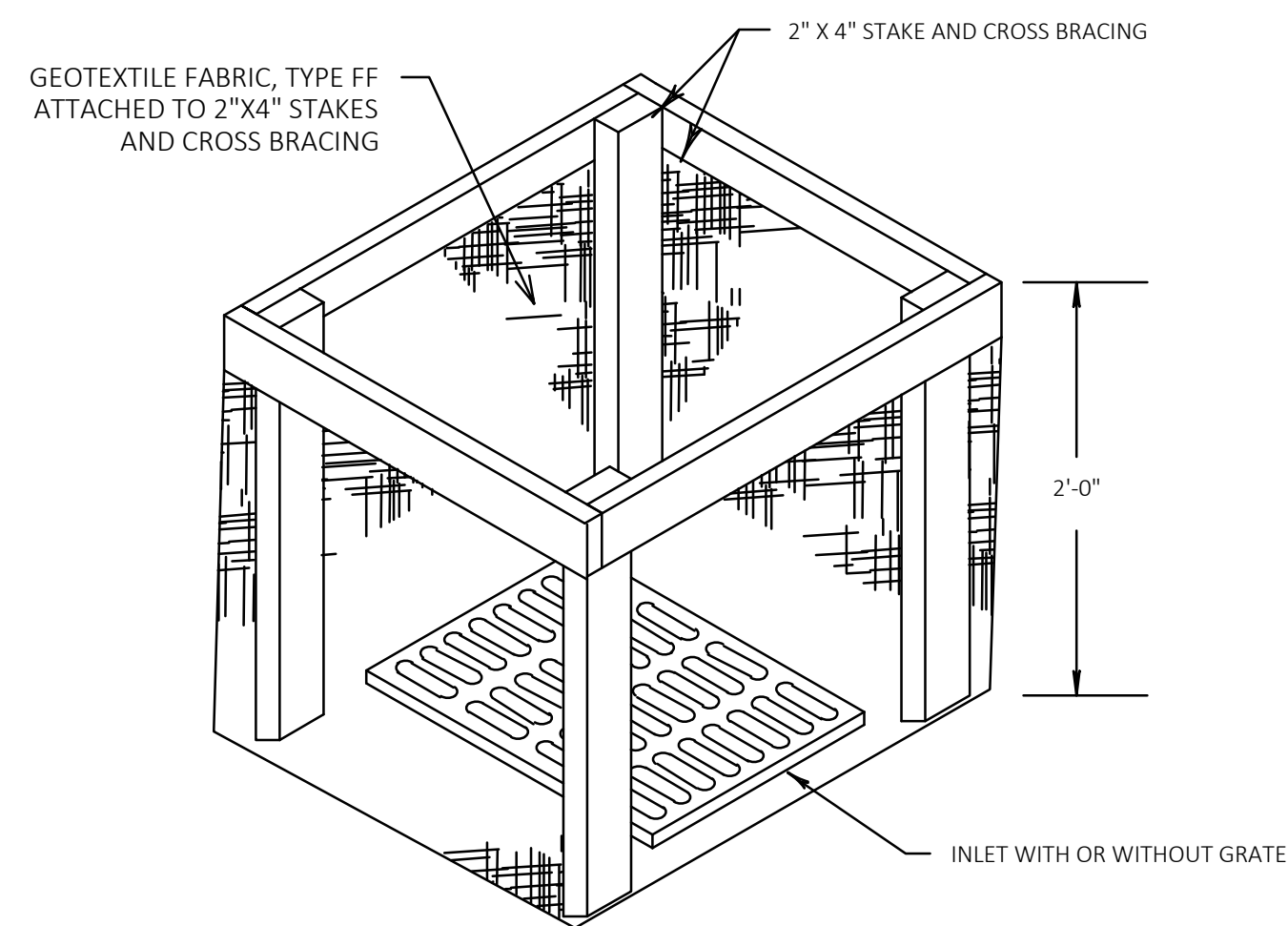
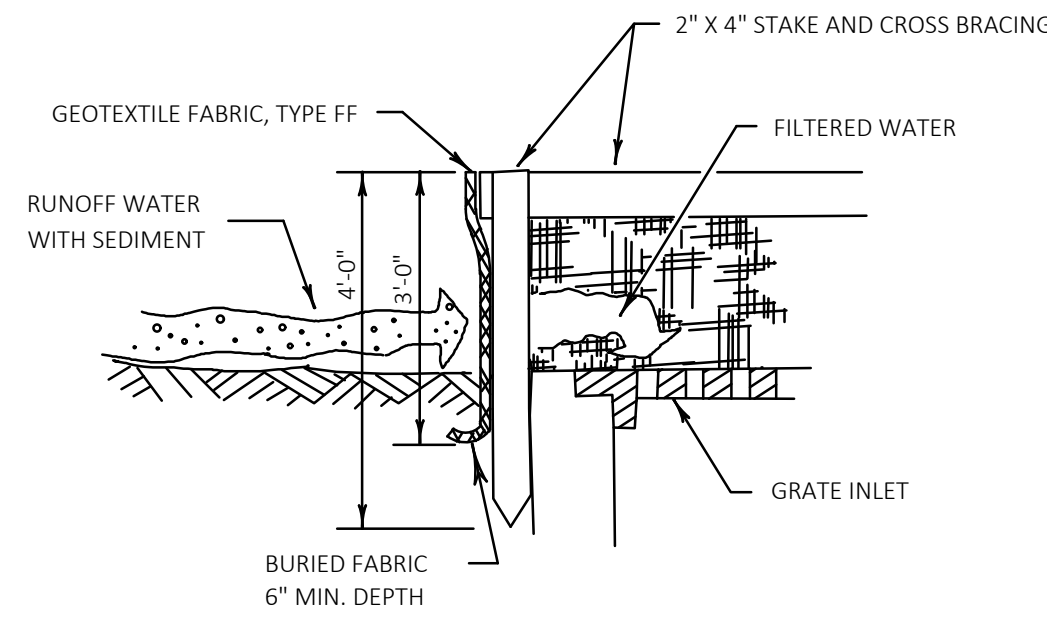
PROJECT MANAGER:
JASON FEUCHT, P.E.

DESIGNED BY: RJW

CHECKED BY: JMF

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43

NOTE: ATTACH GEOTEXTILE FABRIC, TYPE FF TO THE TOP OF STAKES AND CROSS BRACINGS.



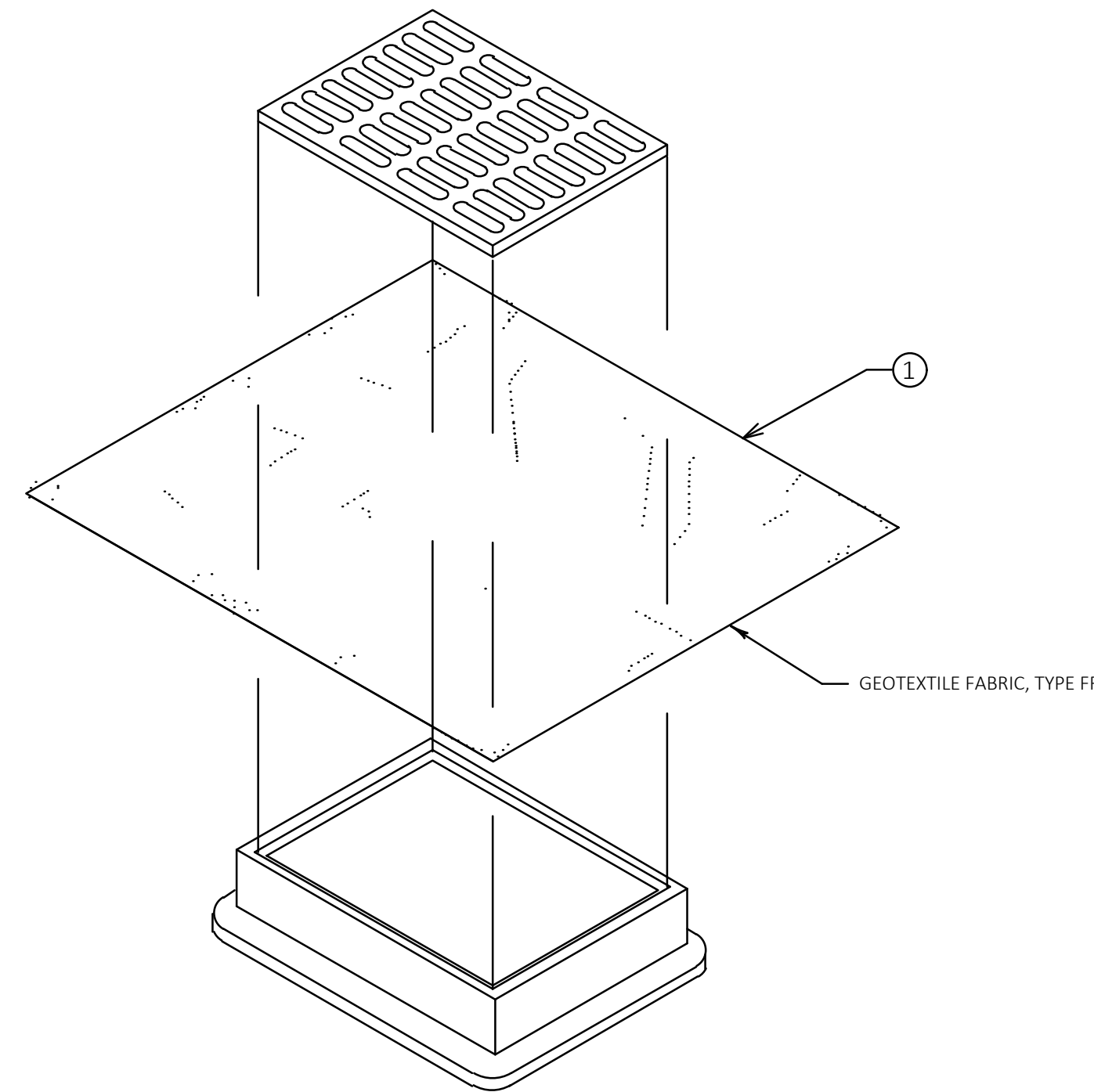
INLET PROTECTION, TYPE A

GENERAL NOTES:

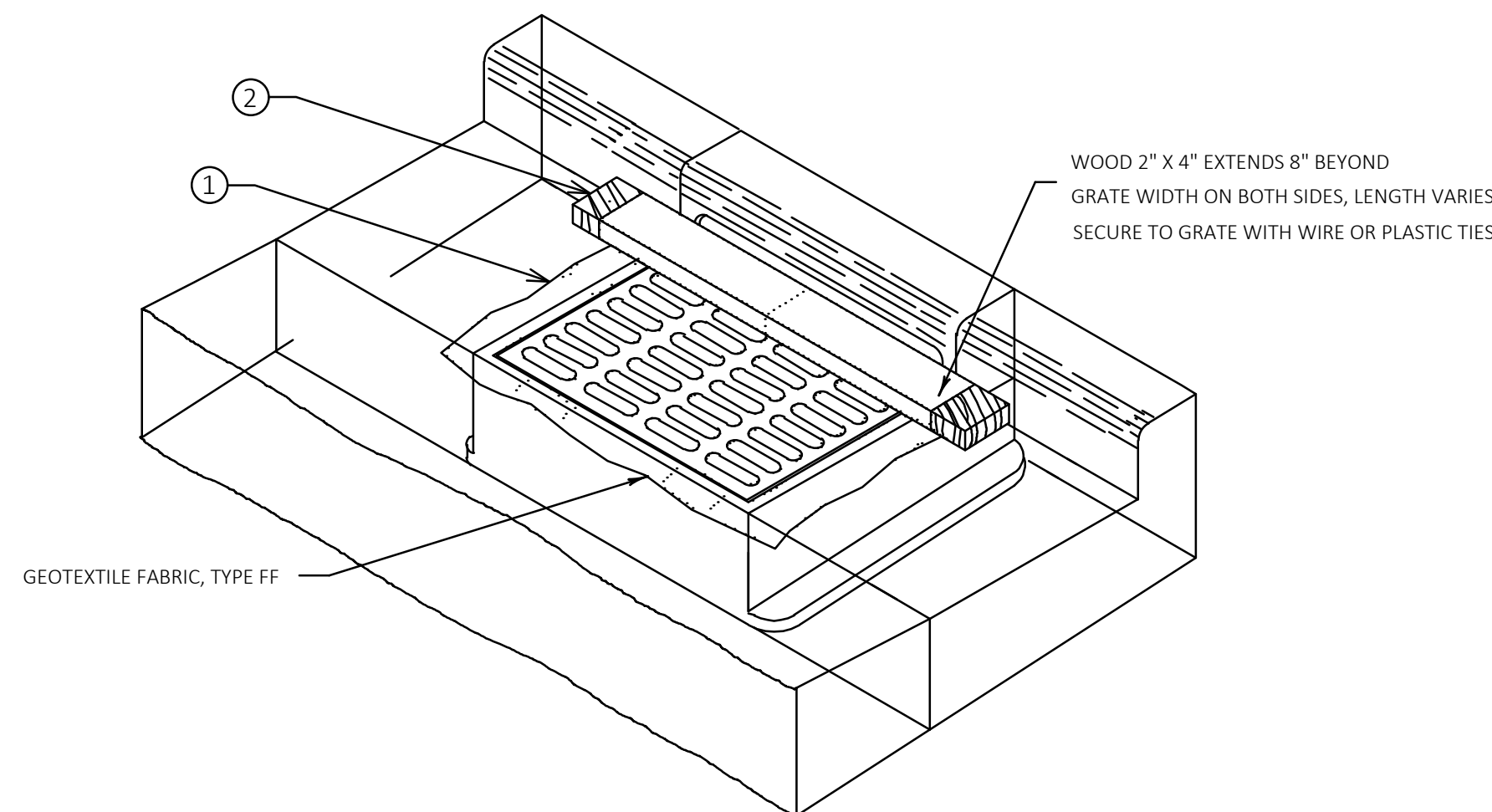
FABRIC SHALL BE REPLACED AT THE ENGINEERS DISCRETION.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**
(CAN BE INSTALLED IN ANY INLET WITH OUR WITHOUT A CURB BOX)

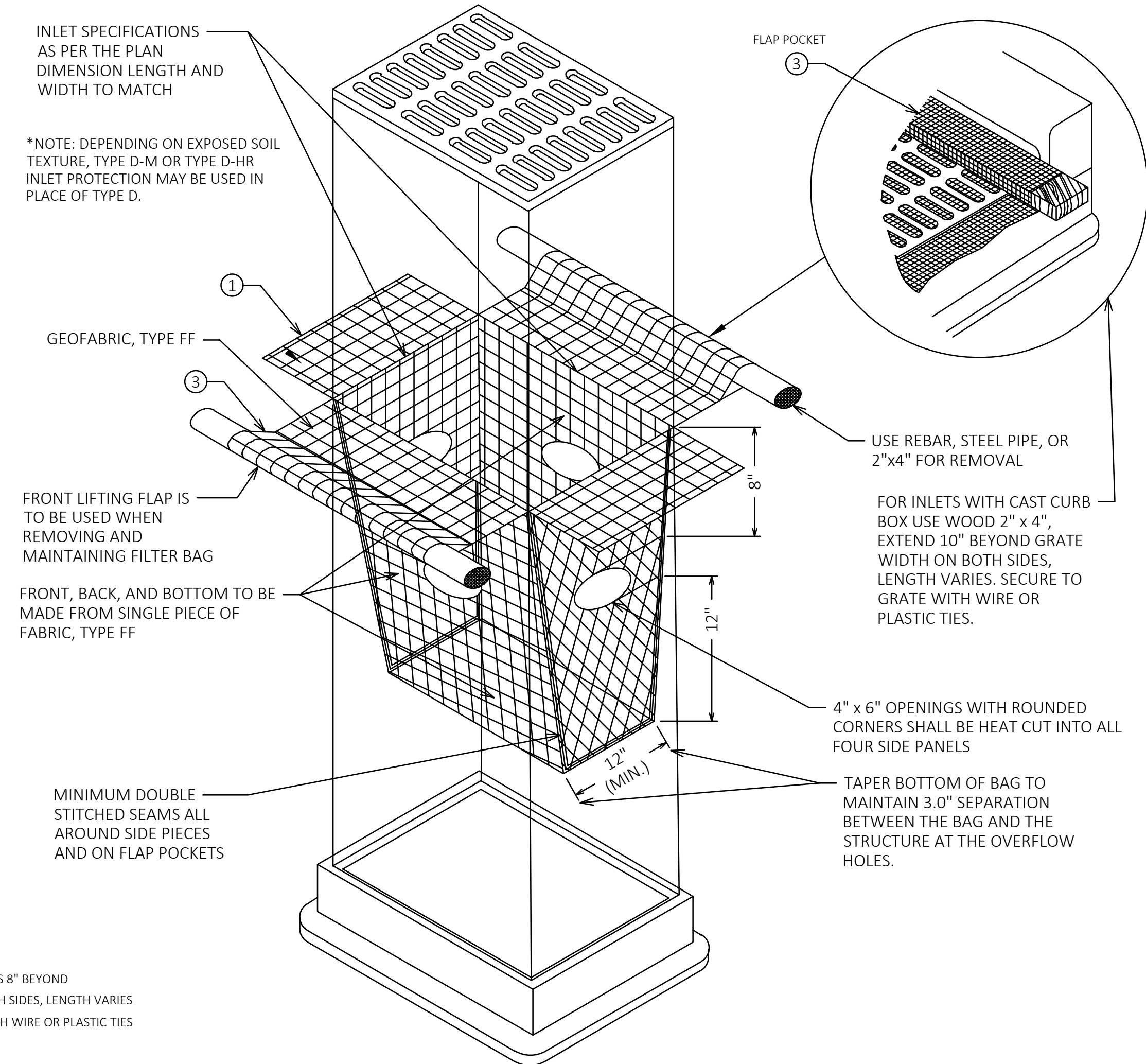


INLET PROTECTION, TYPE C (WITH CURB BOX)

INLET PROTECTION DETAIL

INLET SPECIFICATIONS AS PER THE PLAN DIMENSION LENGTH AND WIDTH TO MATCH

*NOTE: DEPENDING ON EXPOSED SOIL TEXTURE, TYPE D-M OR TYPE D-HR INLET PROTECTION MAY BE USED IN PLACE OF TYPE D.



INLET PROTECTION, TYPE D
(CAN BE INSTALLED IN ANY INLET WITH OR WITHOUT A CURB BOX AS PER NOTE 2)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS, OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE GRATE.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

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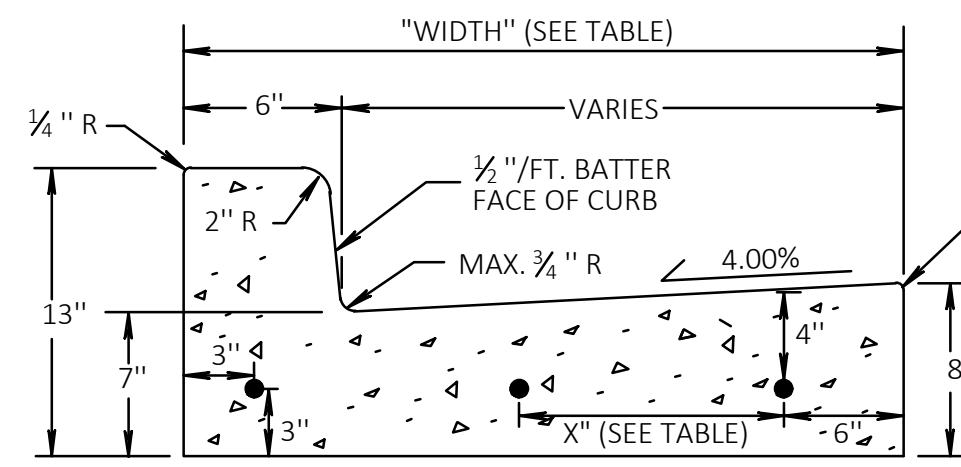
VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
DETAILS

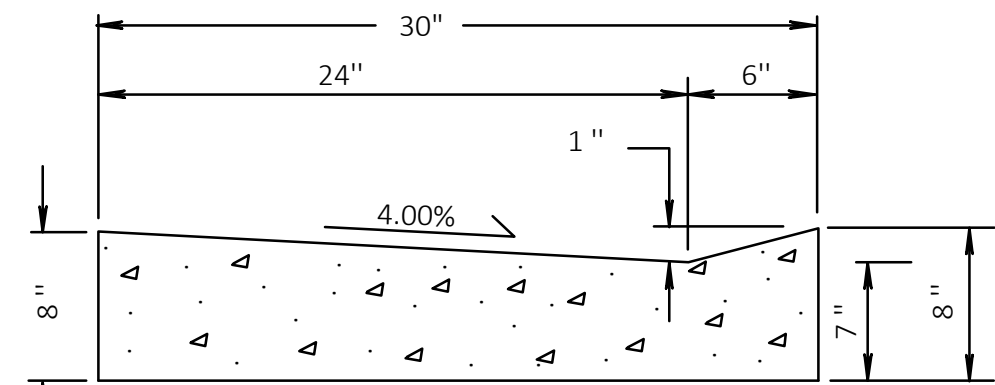
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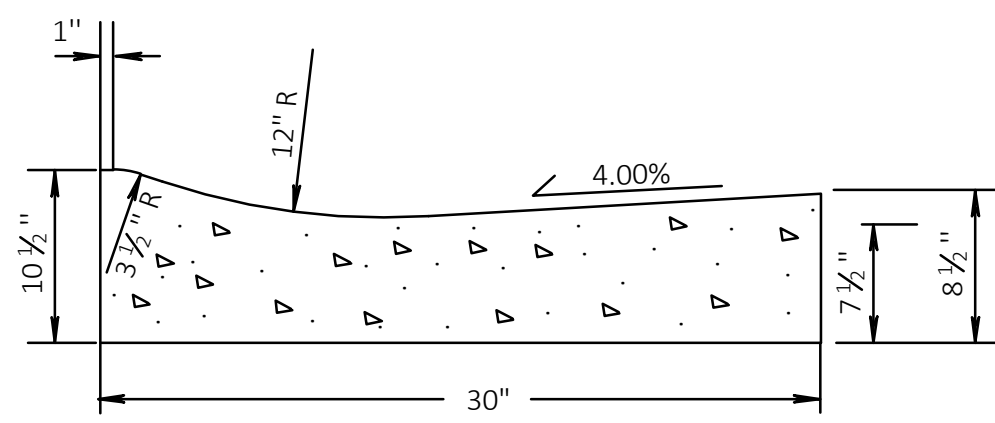
WIDTH	X (IN.)	# OF BARS
18"	9"	2
24"	15"	2
30"	10.5"	3



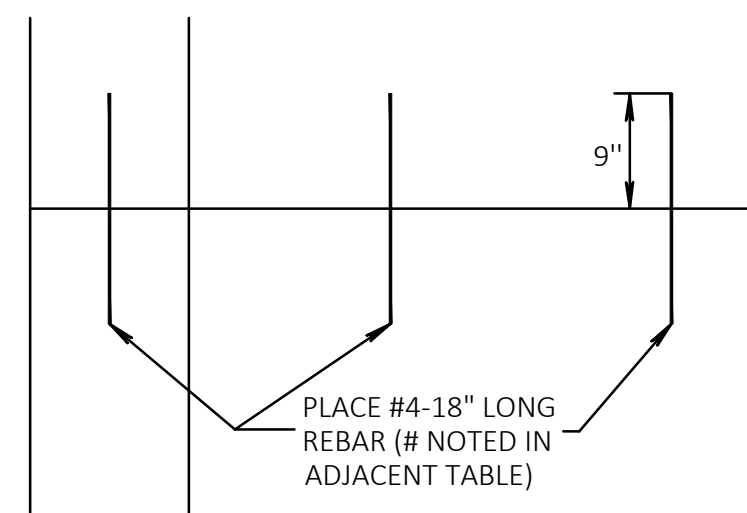
SECTION VIEW



30-INCH CURB & GUTTER AT DRIVEWAY OPENING

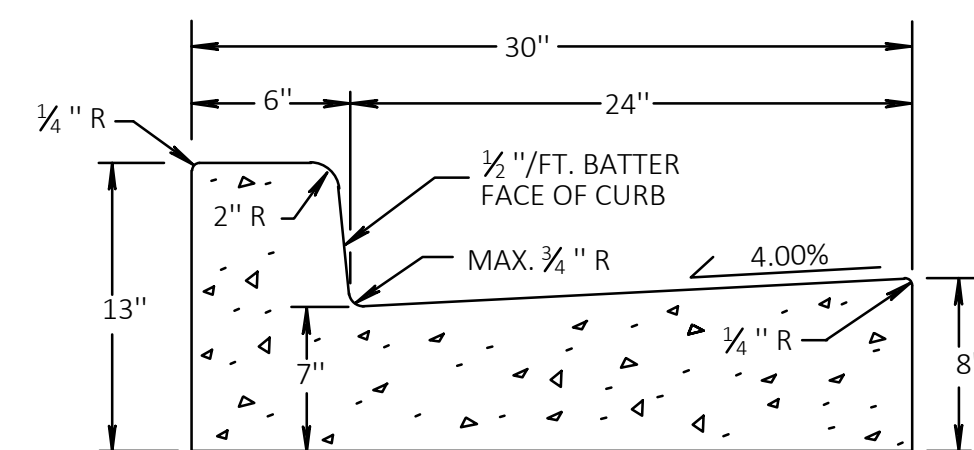


30-INCH MOUNTABLE CURB & GUTTER

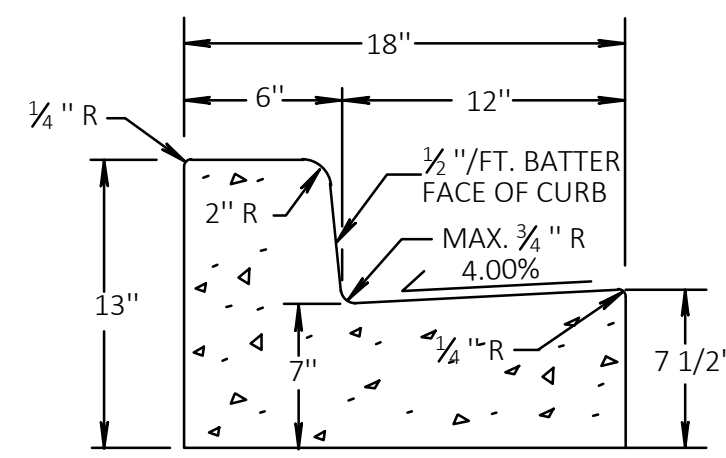


PLAN VIEW

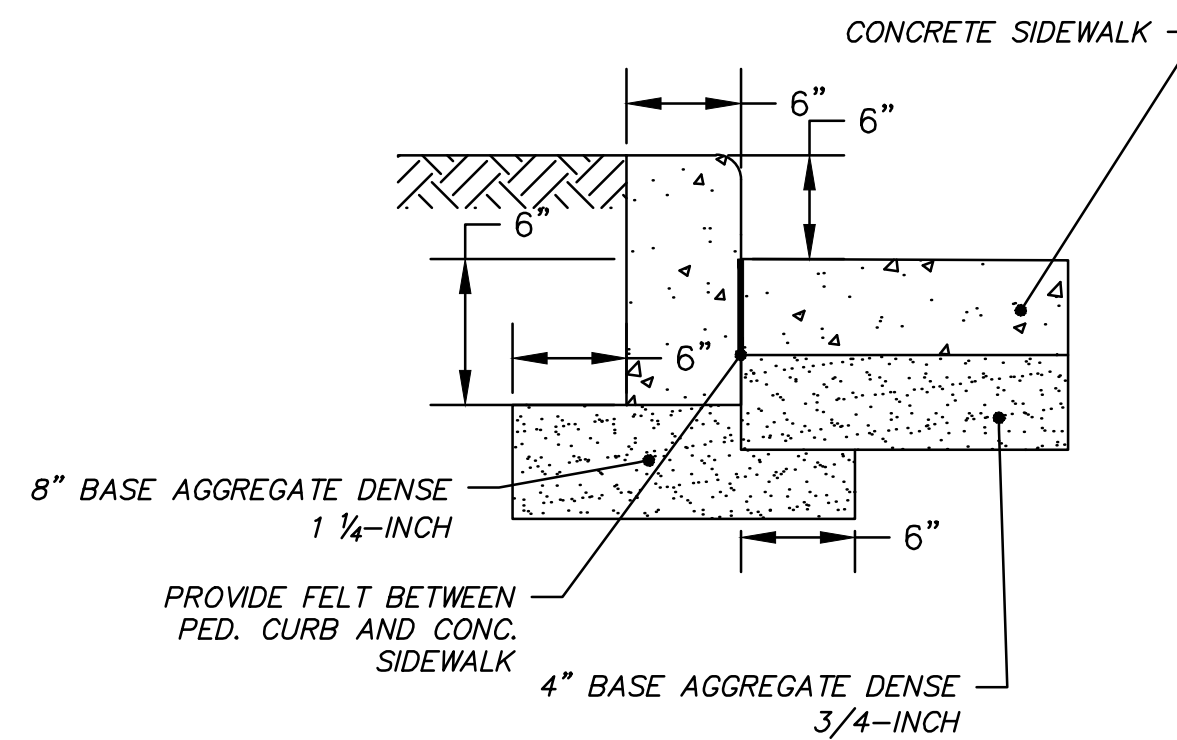
CURB & GUTTER TIES



30-INCH CURB & GUTTER

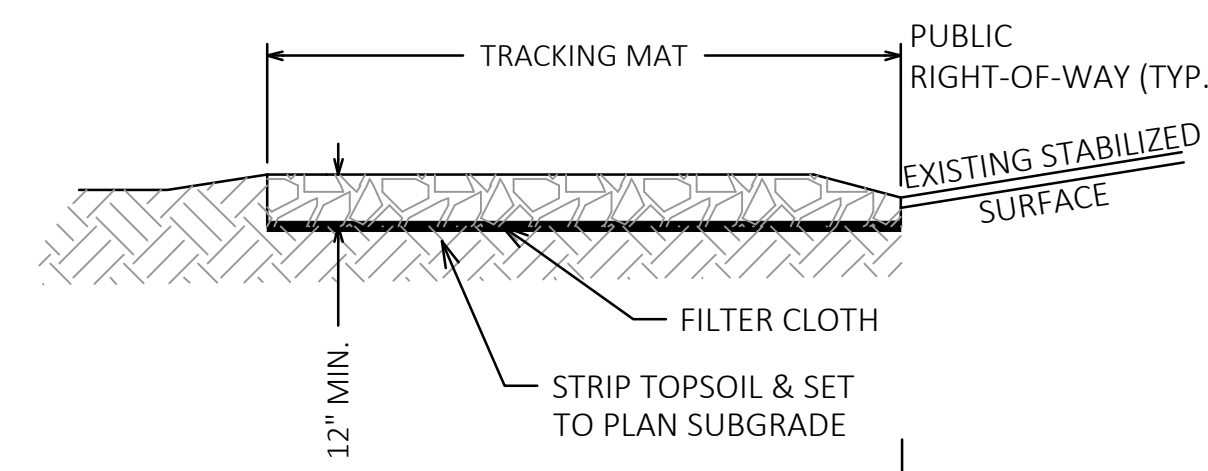


18-INCH CURB & GUTTER TYPE D



CONCRETE CURB PEDESTRIAN

CONCRETE CURB & GUTTER DETAILS



GRADATION FOR STONE TRACKING PADS

SIEVE SIZE	% BY WEIGHT PASSING
3"	100
2 1/2"	90-100
1 1/2"	25-60
3/4"	0-20
3/8"	0-5

CONSIDERATIONS:

1. VEHICLES TRAVELING ACROSS THE TRACKING PAD SHOULD MAINTAIN A SLOW CONSTANT SPEED.
2. THE BEST APPROACH TO PREVENTING OFF-SITE TRACKING IS TO RESTRICT VEHICLES TO STABILIZED AREAS.
3. ANY SEDIMENT TRACKED ONTO A PUBLIC OR PRIVATE ROAD SHOULD BE REMOVED BY STREET CLEANING, NOT FLUSHING, BEFORE THE END OF EACH WORKING DAY.

NOTES:

A. TRACKING PAD:

1. TRACKING PAD TO CONFORM TO WDNR CONSERVATION PRACTICE STANDARD 1057.
2. THE TRACKING PAD SHALL BE INSTALLED PRIOR TO ANY TRAFFIC LEAVING THE SITE.
3. THE AGGREGATE FOR THE TRACKING PAD SHALL BE HARD, DURABLE, ANGULAR STONE OR RECYCLED CONCRETE MEETING THE GRADATION OF TABLE 1 OF THE WISCONSIN DNR TECHNICAL STANDARD 1057.
4. THE AGGREGATE SHALL BE PLACED IN A LAYER AT LEAST 12 INCHES THICK. ON SITES WITH A HIGH WATER TABLE, OR WHERE SATURATED CONDITIONS ARE EXPECTED DURING THE LIFE OF THE CONTROL PRACTICE, STONE TRACKING PADS SHALL BE UNDERLAIN WITH A WISDOT TYPE HR GEOTEXTILE FABRIC TO PREVENT MIGRATION OF UNDERLYING SOIL INTO THE STONE.
5. THE TRACKING PAD SHALL BE THE FULL WIDTH OF THE EGRESS POINT, MINIMUM 12' WIDE. THE TRACKING PAD SHALL BE A MINIMUM OF 50 FEET LONG.
6. SURFACE WATER MUST BE PREVENTED FROM PASSING THROUGH THE TRACKING PAD. FLOWS SHALL BE DIVERTED AWAY FROM TRACKING PADS OR CONVEYED UNDER AND AROUND THEM BY USING A VARIETY OF PRACTICES, SUCH AS CULVERTS, DITCHES, SWALES, OR OTHER SIMILAR PRACTICES.

B. MAINTENANCE

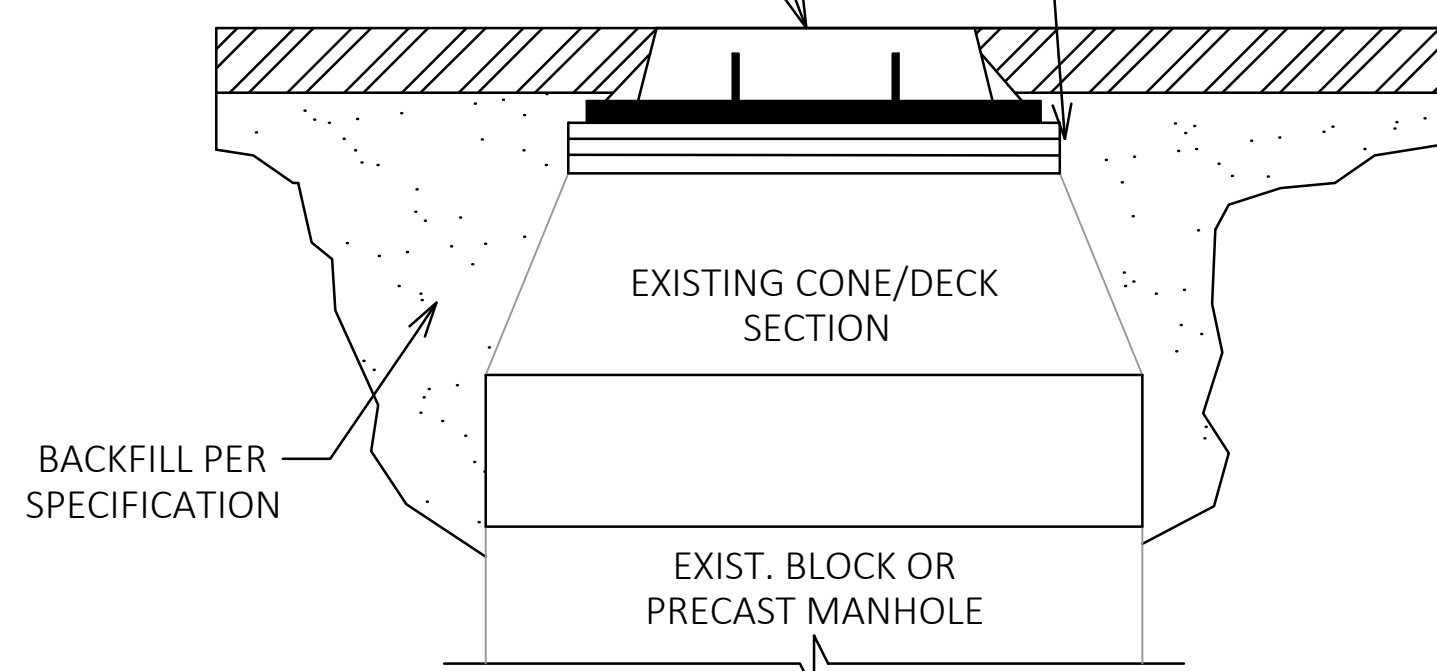
1. ROCKS LODGED BETWEEN THE TIRES IF DUAL WHEEL VEHICLES SHALL BE REMOVED PRIOR TO LEAVING THE CONSTRUCTION SITE.
2. THE TRACKING PAD PERFORMANCE SHALL BE MAINTAINED BY SCRAPING OR TOP-DRESSING WITH ADDITIONAL AGGREGATE.
3. A MINIMUM 12-INCH THICK PAD SHALL BE MAINTAINED.

TRACKING PAD DETAIL

*MANHOLE IN TERRACE - CASTING SHALL BE SET EQUAL TO FINISHED GRADE

MANHOLE IN PAVEMENT - CASTING SHALL BE SET 1/4" (.02') BELOW FINISHED PAVEMENT GRADE SPECIFIED IN PLAN

CONTRACTOR OPTION TO USE EITHER CONCRETE RINGS WITH CEMENT MORTAR OR WRAPPED HDPE RINGS WITH BUTYL RUBBER MORTAR. EXTERNAL SEAL REQUIRED FOR SANITARY MANHOLES.



ADJUSTING MANHOLES DETAIL

CONTRACTOR OPTION TO USE EITHER CONCRETE RINGS WITH CEMENT MORTAR OR WRAPPED HDPE RINGS WITH BUTYL RUBBER MORTAR.

REMOVE EXISTING CASTING AND RESET/REPLACE TO PROPER GRADE AND SLOPE

REMOVE AND REPLACE CURB & GUTTER AS DIRECTED

EXIST. CONCRETE CURB & GUTTER

6' TYP. OR AS DIRECTED

BACKFILL PER SPECIFICATION

EXISTING CONE/DECK SECTION

EXIST. BLOCK OR PRECAST CATCH BASIN

*IF THE DIFFERENCE BETWEEN EXISTING RIM ELEVATION AND PROPOSED RIM ELEVATION EXCEEDS 19" (1.58'), A NEW CONCRETE RISER SECTION SHALL BE INSTALLED BELOW THE EXISTING CONE/DECK SECTION OF STRUCTURE.

ADJUSTING INLETS DETAIL

DESCRIPTION

DATE

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VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
DETAILS

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DATE: 05/02/2023

SCALE: N.T.S.

JOB NO. 2232100

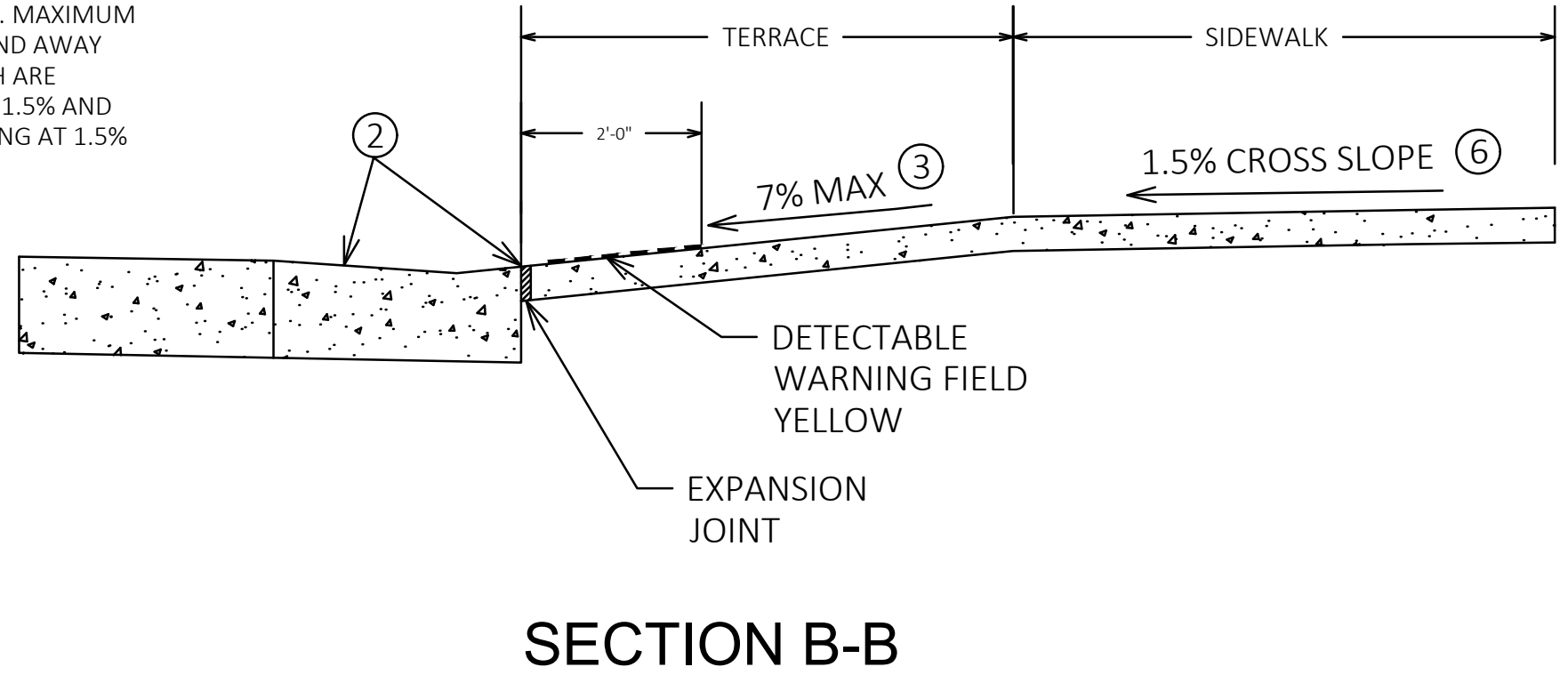
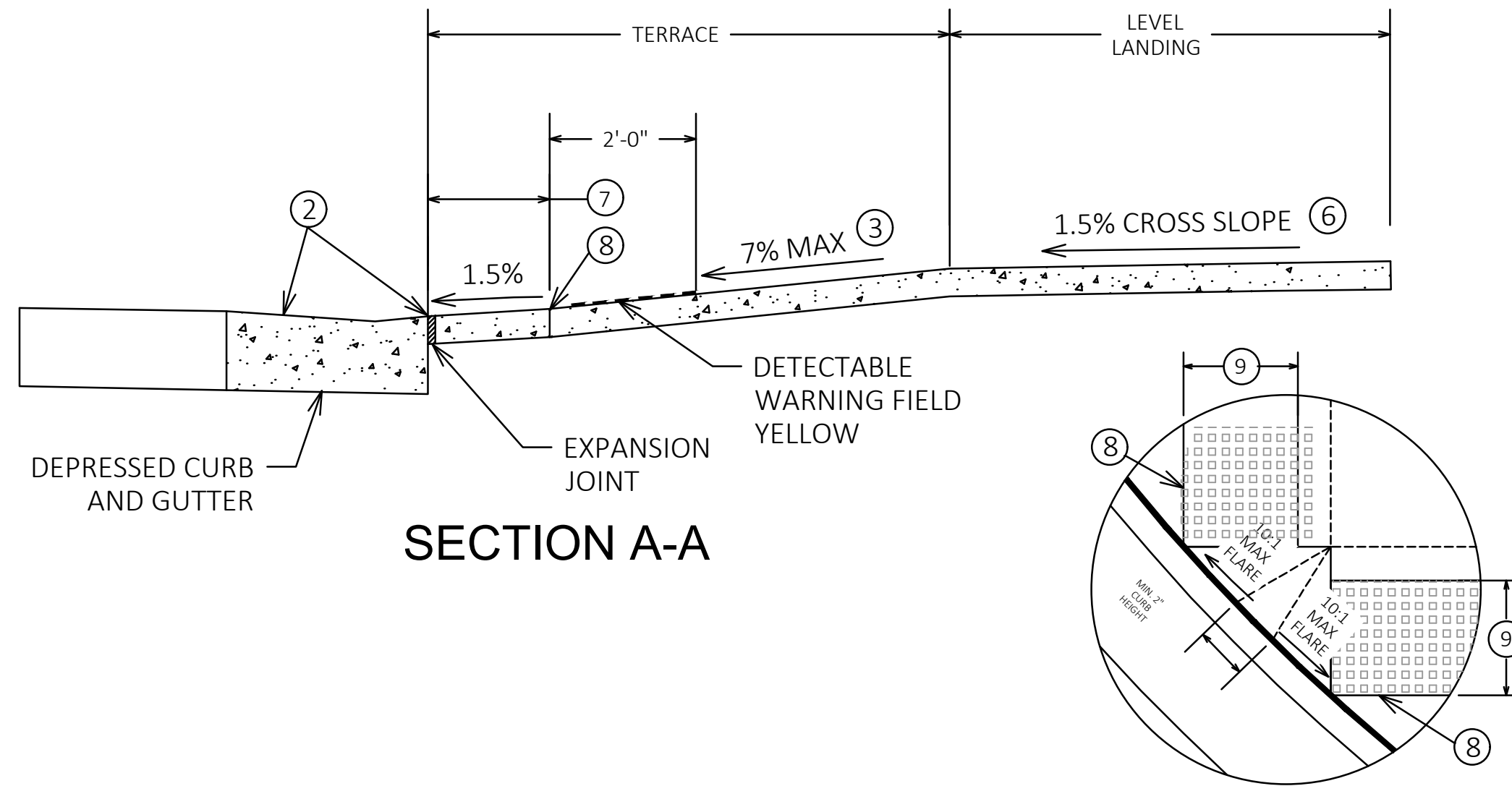
PROJECT MANAGER:
JASON FEUCHT, P.E.

DESIGNED BY: RJW

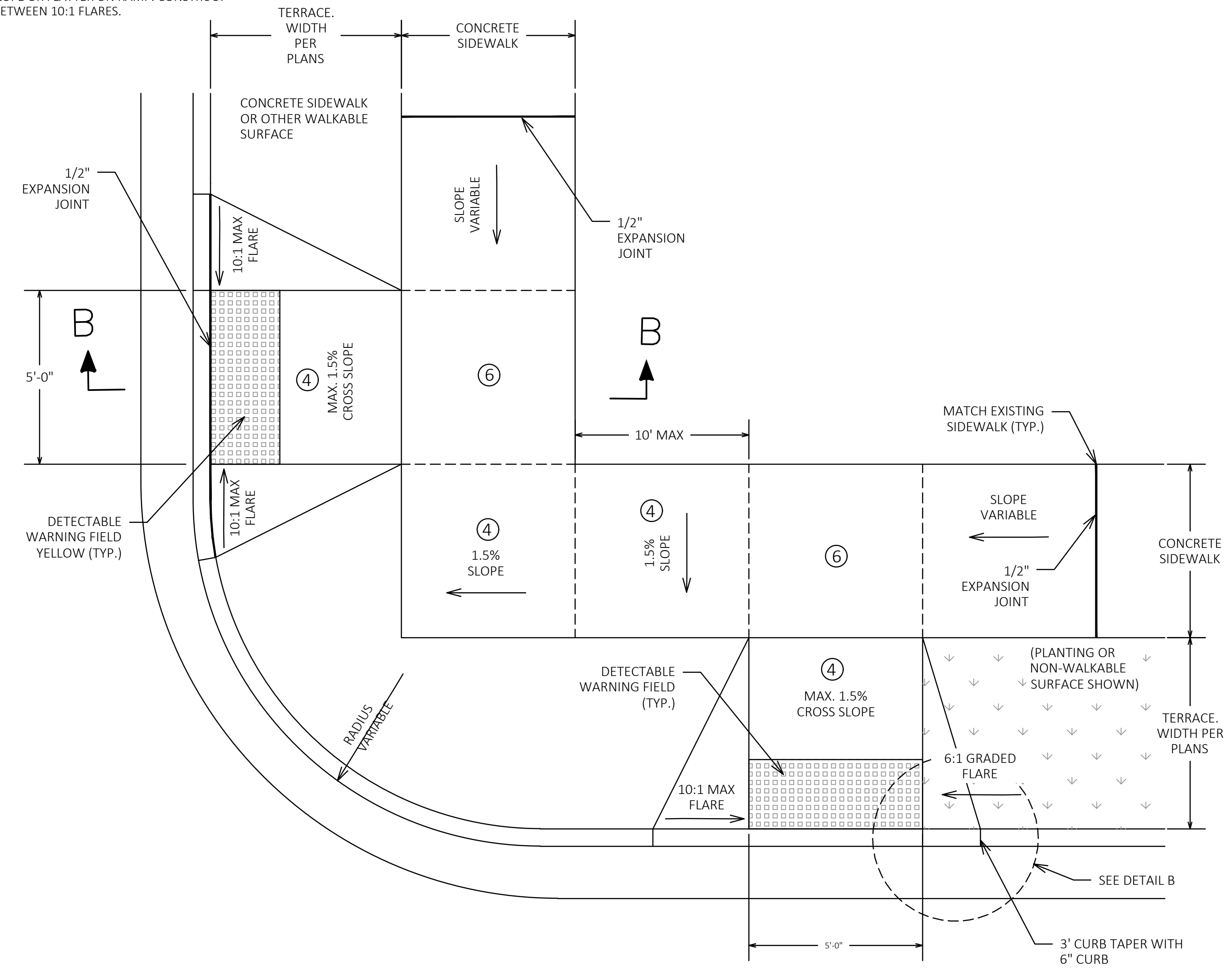
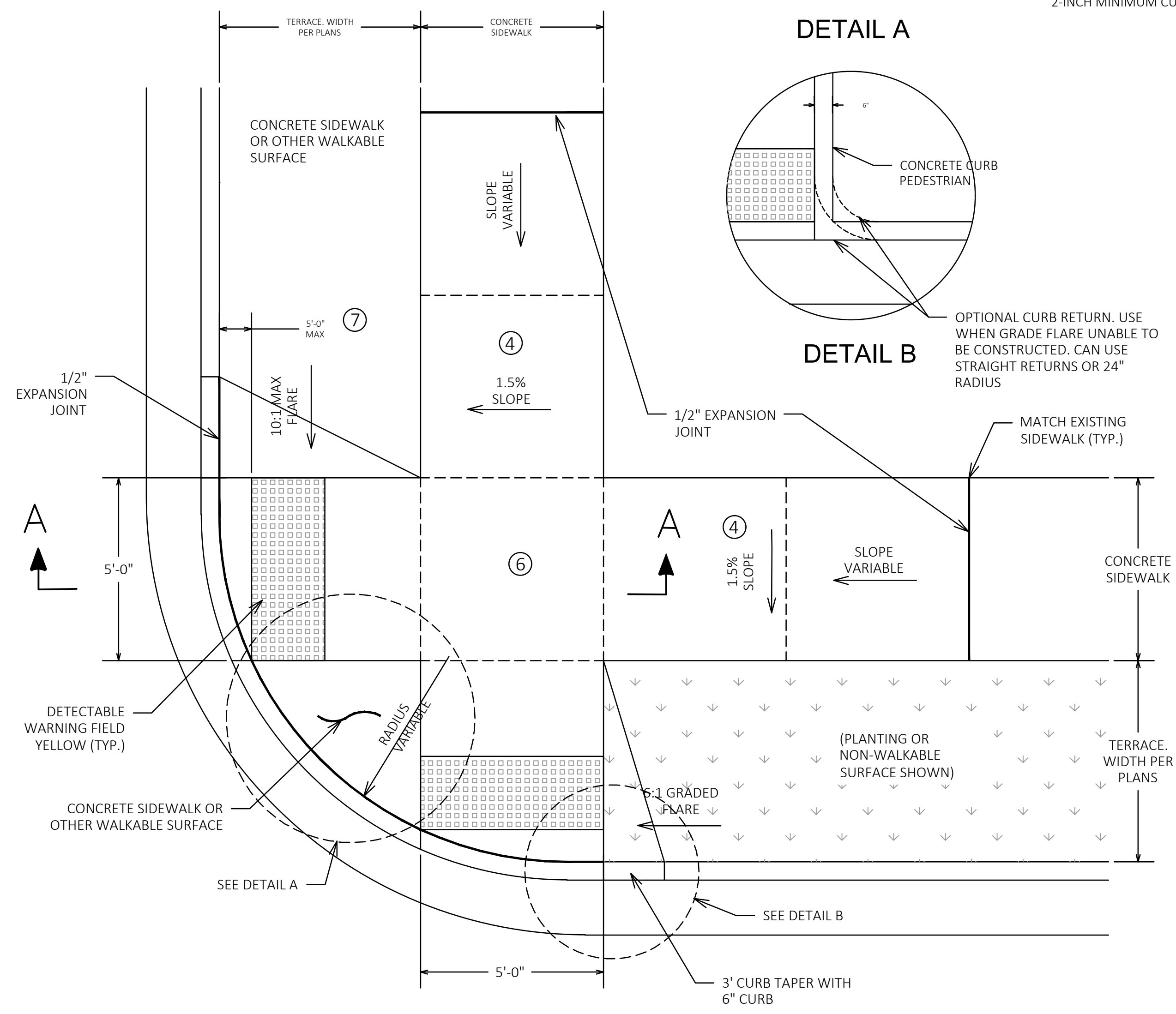
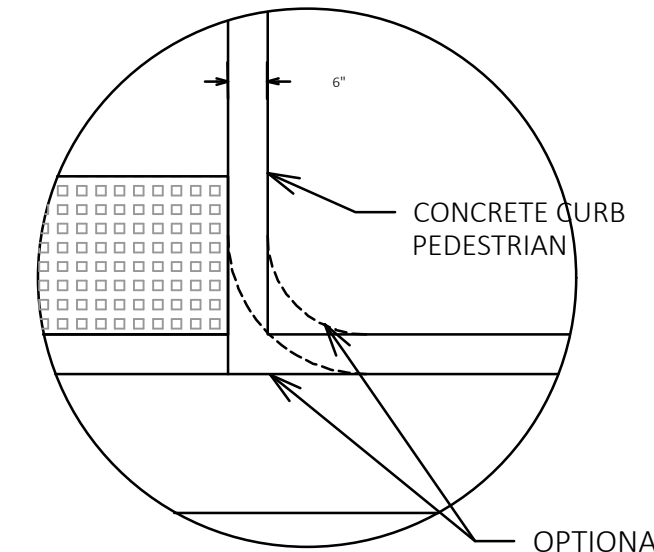
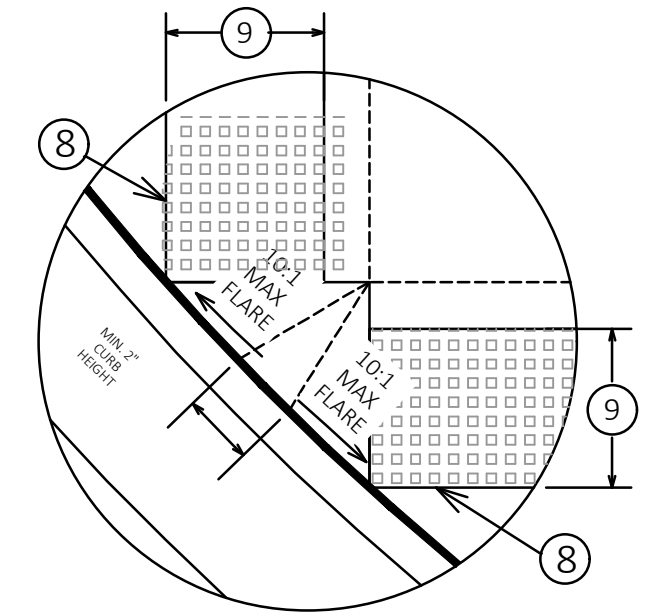
CHECKED BY: JMF

SHEET NUMBER

45



- GENERAL NOTES:**
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES, OR OTHER OBSTRUCTIONS IN FRONT OF RAMP.
 - DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE MADE FROM THE SAME MANUFACTURER.
 - GRADE CHANGE BETWEEN FLANGE SLOPE AND THE CURB RAMP SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLANGE SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND THE CURB AND AWAY FROM THE CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7% WHEN ADJACENT TO 1.5% LANDING. CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
 - AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLANGE SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
 - ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
 - PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
 - WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE PLATES.
 - PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
 - WHEN DISTANCE IS LESS THAN 6'-0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE THE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



CURB RAMP TYPE 2 - PLAN

CURB RAMP TYPE 3 - PLAN

CURB RAMP DETAILS

DESCRIPTION

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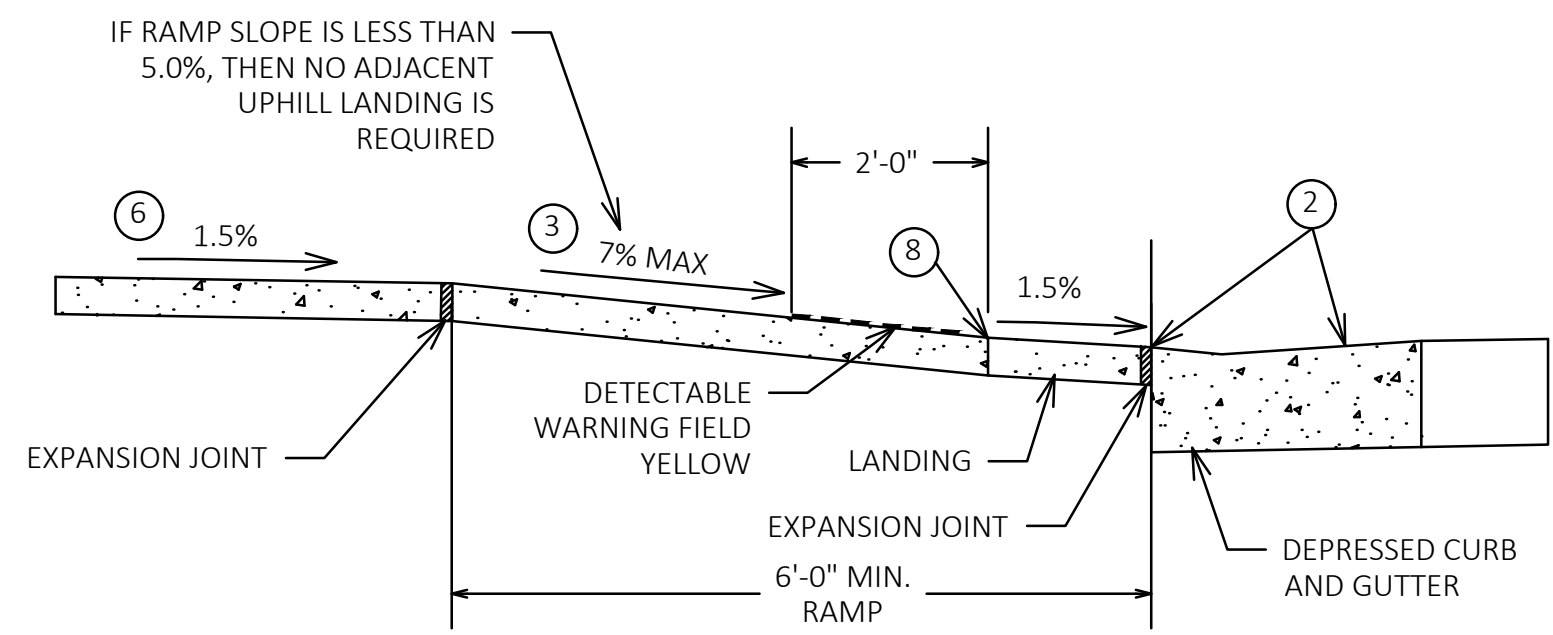
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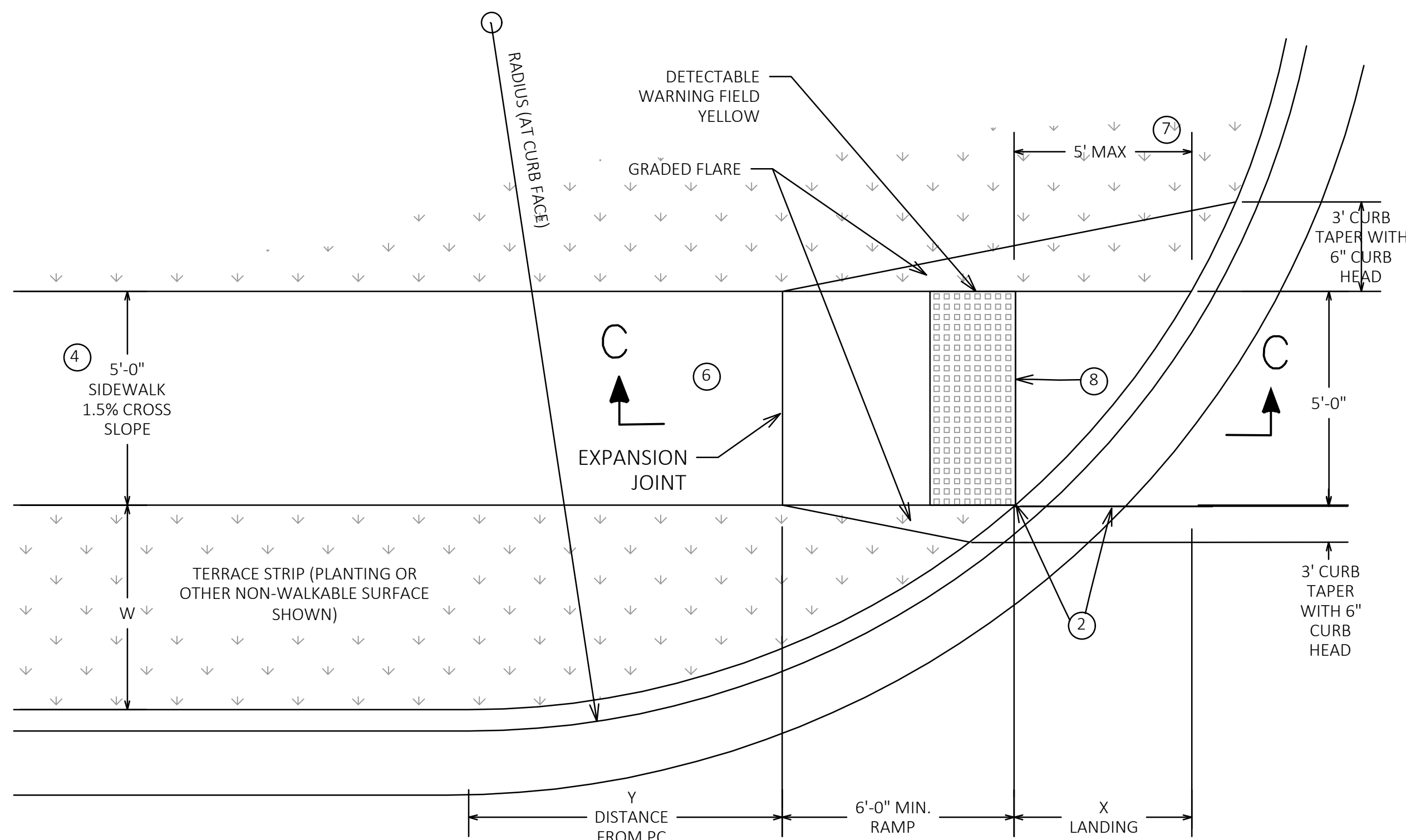
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DATE: 05/02/2023
SCALE: N.T.S.
JOB NO. 2232100
PROJECT MANAGER: JASON FEUCHT, P.E.
DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
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RADIUS (AT CURB FACE)	W = 3'-0"		W = 4'-0"		W = 5'-0"		W = 6'-0"		W = 7'-0"		W = 8'-0"		W = 9'-0"		W = 10'-0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2'-10 1/4"	0'-5"	2'-1"	1'-4 1/4"	1'-5"	2'-1"	0'-10"	2'-7 1/2"	0'-3 3/4"	3'-0 1/4"						
15 FEET	4'-6 3/4"	2'-1 3/4"	3'-9"	3'-5 3/4"	3'-1 1/4"	4'-6"	2'-6 3/4"	5'-4 1/2"	2'-1"	6'-1"	1'-8"	6'-8 1/2"	1'-3 1/4"	7'-2 1/2"	0'-10 3/4"	7'-7 1/4"
20 FEET	5'-9 3/4"	3'-6 1/2"	4'-11 1/2"	5'-1 3/4"	4'-3 1/4"	6'-5 1/2"	3'-8 3/4"	7'-7"	3'-3"	3'-6 1/2"	2'-10"	9'-4 1/2"	2'-5 1/2"	10'-1 1/4"	2'-1 1/4"	10'-9"
30 FEET			6'-9 1/2"	7'-11 1/4"	6'-0 1/4"	9'-8"	5'-5"	11'-1 3/4"	4'-10 3/4"	12'-5 3/4"	4'-5 1/2"	13'-7 3/4"	4'-0 3/4"	14'-8 1/2"	3'-8 1/2"	15'-8 1/4"
40 FEET									6'-1 3/4"	15'-8 1/2"	5'-8"	17'-2"	5'-3"	18'-5 3/4"	4'-10 3/4"	19'-8 1/4"
50 FEET														5'-10 1/4"	23'-2"	

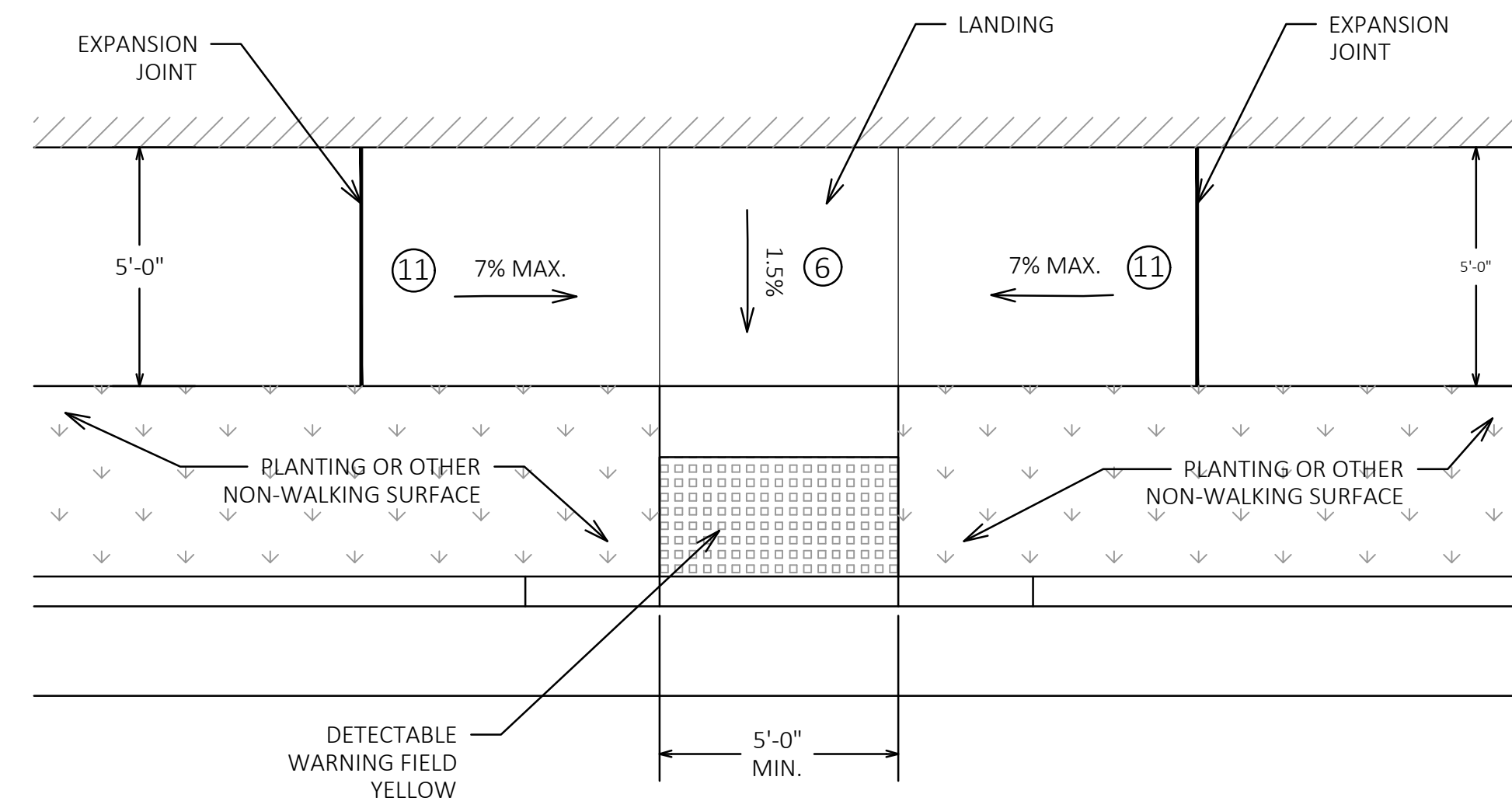
VALUES IN TABLE ARE APPLICABLE TO TYPE 4B AND 4B1 CURB RAMPS
INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



SECTION C-C



CURB RAMP TYPE 4B1 - PLAN



CURB RAMP TYPE 7B - PLAN

GENERAL NOTES:

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES, OR OTHER OBSTRUCTIONS IN FRONT OF RAMP.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE MADE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS. TYPE 4A CURB RAMP IS NOT SHOWN.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2 GRADE CHANGE BETWEEN FLANGE SLOPE AND THE CURB RAMP SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLANGE SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND THE CURB AND AWAY FROM THE CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7% WHEN ADJACENT TO 1.5% LANDING. CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLANGE SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE IN ANY DIRECTION) OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- 7 WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELDS.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 10 INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- 11 SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- 13 DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED).

DESCRIPTION	DATE

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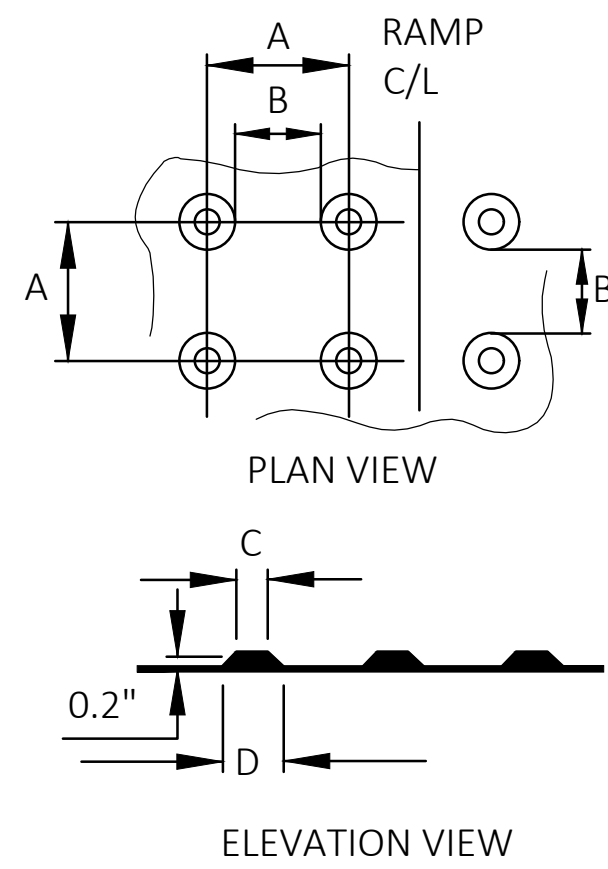
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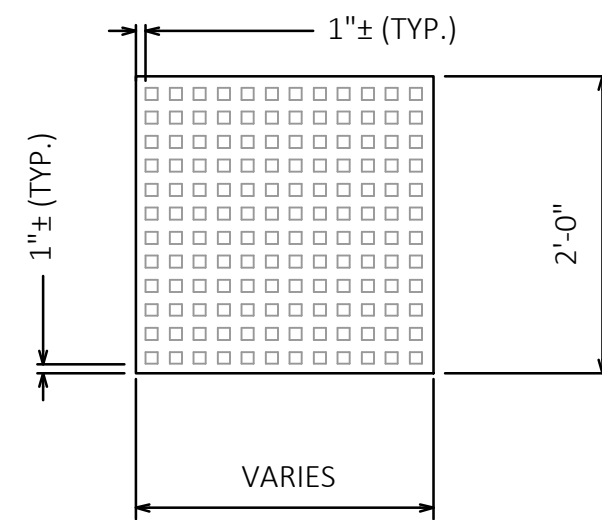
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	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.



TRUNCATED DOMES



PLAN VIEW - DETECTABLE WARNING FIELDS YELLOW (TYPICAL)

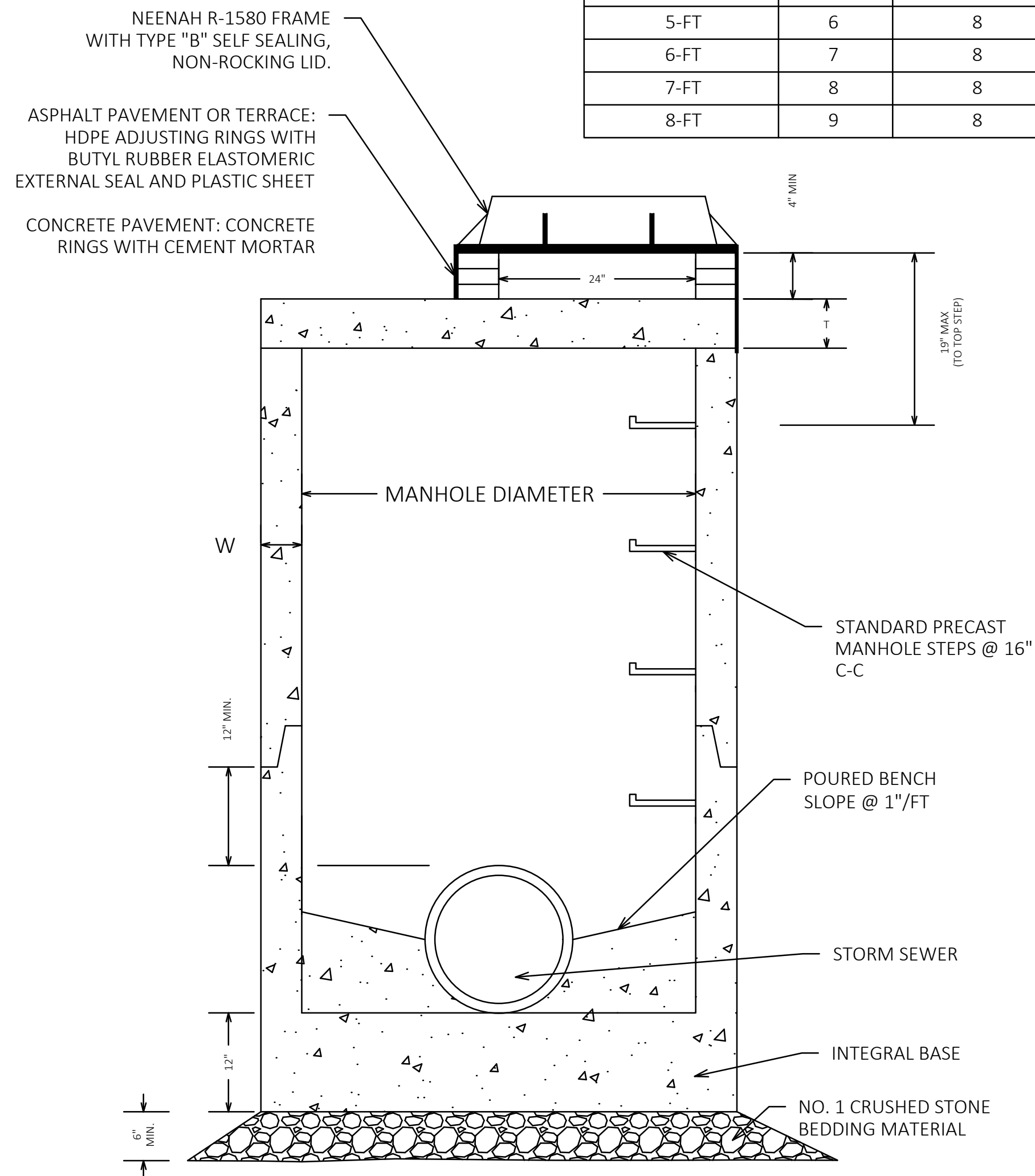
GENERAL NOTES:

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE MADE FROM THE SAME MANUFACTURER.

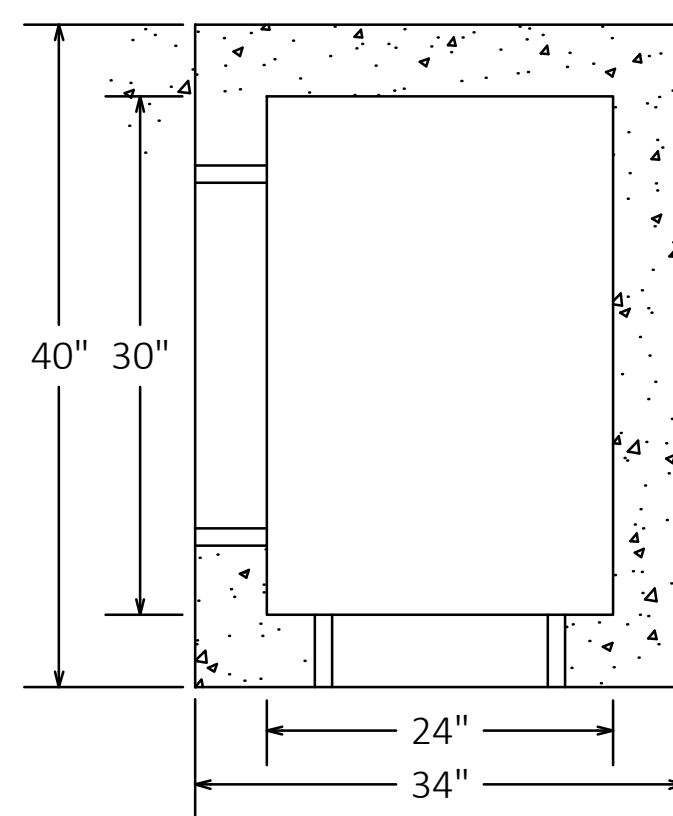
PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION.

DETECTABLE WARNING PLATES DETAIL

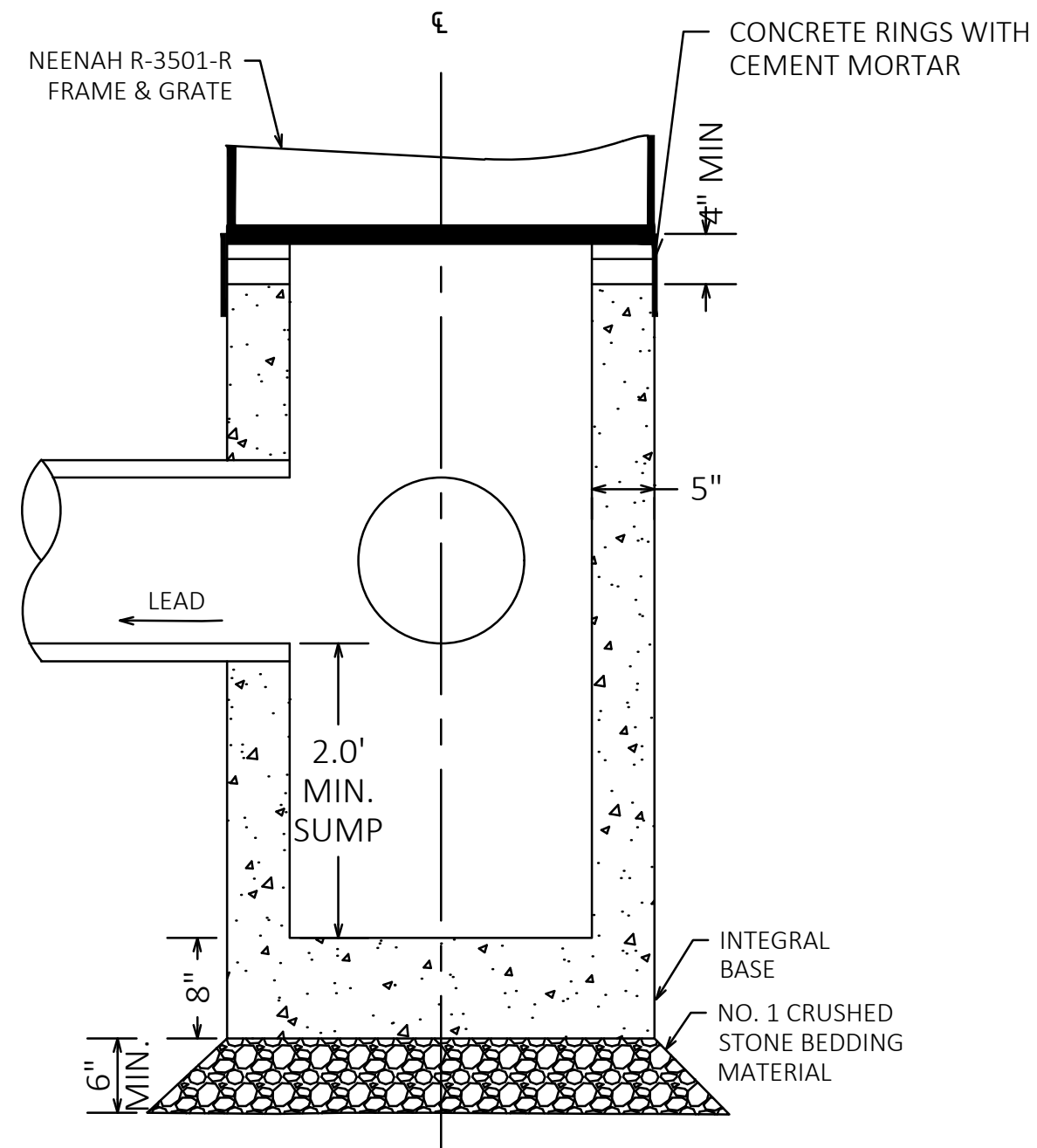
MANHOLE DIAMETER	W (INCHES)	T, MIN. (INCHES)
4-FT	5	6
5-FT	6	8
6-FT	7	8
7-FT	8	8
8-FT	9	8



STORM MANHOLE DETAIL

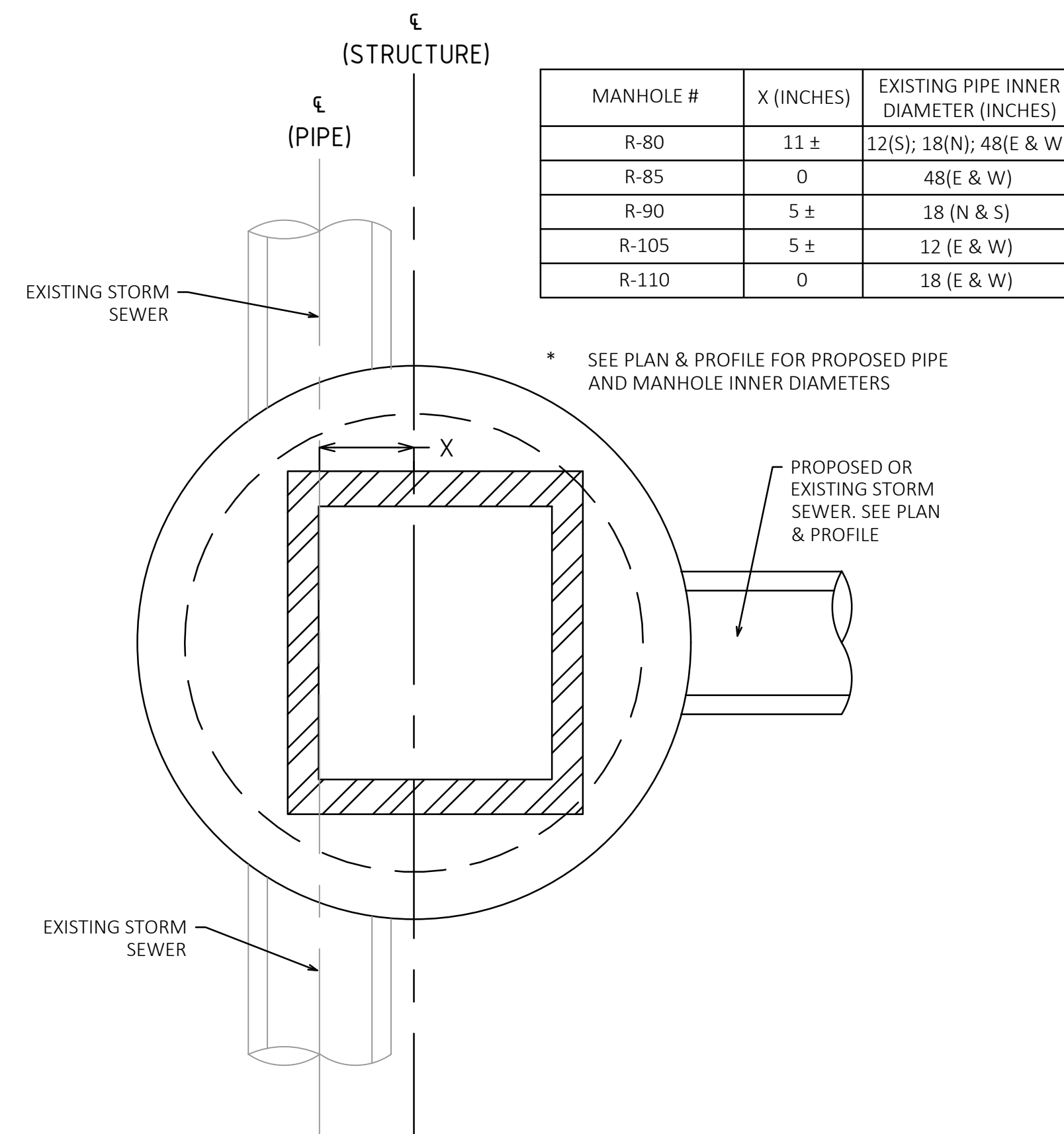


PLAN VIEW

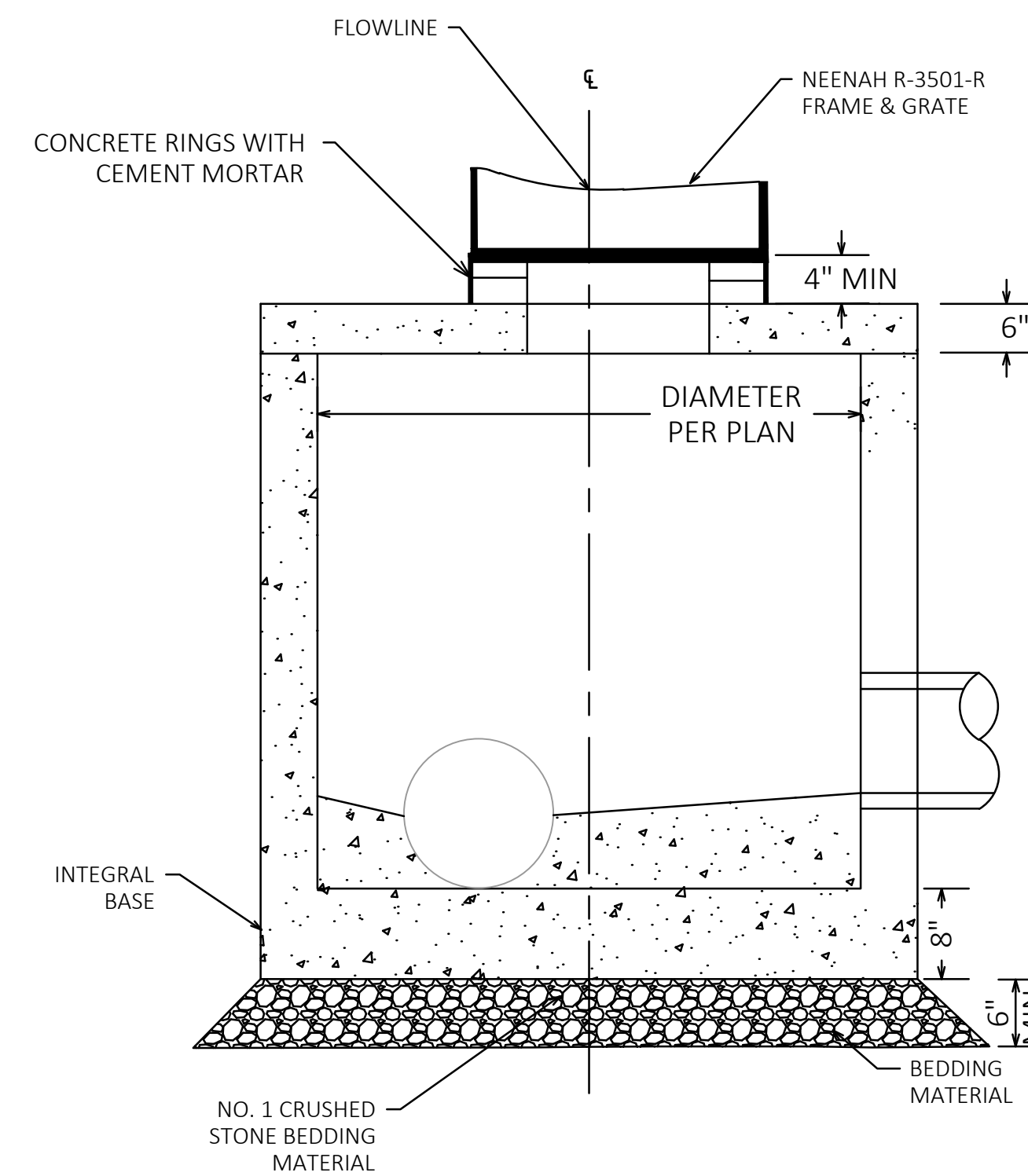


SECTION VIEW

CB 2'X2.5' DETAIL



PLAN VIEW



STORM MANHOLE DETAIL EXISTING STORM SEWER CONNECTION

MANHOLE #	X (INCHES)	EXISTING PIPE INNER DIAMETER (INCHES)
R-80	11 ±	12(S); 18(N); 48(E & W)
R-85	0	48(E & W)
R-90	5 ±	18 (N & S)
R-105	5 ±	12 (E & W)
R-110	0	18 (E & W)

* SEE PLAN & PROFILE FOR PROPOSED PIPE AND MANHOLE INNER DIAMETERS

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JOB NO. 2232100
PROJECT MANAGER: JASON FEUCHT, P.E.
DESIGNED BY: RJW
CHECKED BY: JMF
SHEET NUMBER
48

SDD 13C1 Concrete Pavement Longitudinal Joints and Ties

CONSTRUCTION JOINT

SAWED JOINT

GENERAL NOTES

- CREATE A LONGITUDINAL JOINT FOR PAVEMENT WIDTHS GREATER THAN 15 FEET.
- CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.
- ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.
- PAVEMENT THAT WAS IN PLACE PRIOR TO THE CONTRACT.

TIE BAR TABLE

PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 1/2"	NO. 4	30"	36"
≥ 10 1/2"	NO. 5	36"	36"
≥ 10 1/2"	NO. 4*	30"	24**

CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED: [Signature] DATE: [Date] PAVEMENT SUPERVISOR

CONTRACTION JOINT DOWEL ASSEMBLY

DOWELED CONTRACTION JOINT

DRILLED DOWEL BAR CONSTRUCTION JOINT

TRANSVERSE CONSTRUCTION JOINT

GENERAL NOTES

- CONSTRUCT TRANSVERSE CONTRACTION JOINTS NORMAL TO THE CENTERLINE. SHOW THE LOCATION OF CONTRACTION JOINTS THROUGH INTERSECTIONS ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.
- FOR PAVEMENT SLABS OF VARYING WIDTHS, LOCATE THE OUTER MOST DOWEL BAR SO THAT THE CENTER OF THE BAR IS A MINIMUM OF 6 INCHES FROM A MAXIMUM OF 18 INCHES FROM THE FREE EDGE OF PAVEMENT.
- LOCATE CONTRACTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO THE CONTRACTION JOINTS.
- OBTAIN THE ENGINEER'S APPROVAL FOR THE USE OF ALTERNATIVE DESIGNS OF THE DOWEL ASSEMBLY. USE MECHANICAL DOWEL BAR INSERTERS OR DOWEL ASSEMBLIES WHEN CONSTRUCTION CONTRACTION JOINTS.
- SECURE BASKETS WITH ANCHORS TO HOLD DOWEL BARS IN THE CORRECT POSITION AND ALIGNMENT. TYPE, LOCATION NUMBER AND LENGTH OF ANCHORS ARE DEPENDENT UPON FIELD CONDITIONS.
- FORM OR SAW CONSTRUCTION JOINTS. PROVIDE A 1/2" RADIUS AT FORMED JOINTS.
- PROVIDE A SMOOTH VERTICAL FACE FOR THE ENTIRE DEPTH OF THE PAVEMENT WHEN FORMING CONSTRUCTION JOINTS.
- INSTALL DOWEL BARS AT CONSTRUCTION JOINTS BY FORMING OR DRILLING. INSTALL FORMED DOWEL BARS 12 INCHES C-C AND 12 INCHES FROM PAVEMENT EDGE. REMOVE EXCESS CONCRETE FROM THE FREE END OF THE DOWEL BAR IF DOWEL BARS ARE FORMED THROUGH A HEADER BOARD. INSTALL DRILLED DOWEL BARS ACCORDING TO THE "DRILLED DOWEL BAR CONSTRUCTION JOINT" DETAIL.
- APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.
- ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY. MAXIMUM DRILLED HOLE SIZE IS 1/8" GREATER THAN DOWEL BAR DIAMETER, 9 INCHES IN LENGTH.

PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
6" & 8"	NONE	12"
7" & 7 1/2"	1"	14"
8" & ABOVE	1 1/2"	15"

URBAN DOWELED CONCRETE PAVEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED: [Signature] DATE: [Date] PAVEMENT SUPERVISOR

SDD 13C18-a Concrete Pavement Jointing

DETAIL "A"

DETAIL "B"

DETAIL "C"

DETAIL "D"

STANDARD INTERSECTION

APPROACH TO MEDIAN

LARGE RIGHT TURN

SKewed INTERSECTION

GENERAL NOTES

- THE PRIMARY ROADWAY CONTROLS THE TRANSVERSE JOINT PATTERN. ALIGN NEW JOINTS WITH EXISTING JOINTS OR CRACKS.
- CONSTRUCT TRANSVERSE JOINTS PERPENDICULAR TO THE ROADWAY.
- ADJUST TRANSVERSE JOINTS TO ALIGN WITH UTILITY FIXTURES (E.G. MANHOLES AND INLETS) IN THE PAVEMENT STRUCTURE WHEN POSSIBLE. WATER VALVES DO NOT REQUIRE JOINT ADJUSTMENT.
- AVOID SLABS LESS THAN 2 FEET WIDE OR GREATER THAN 15 FEET WIDE.
- SEE TABLE FOR TRANSVERSE JOINT SPACING. JOINT SPACING SPECIFIED IS MAXIMUM AND ACTUAL SPACING CAN BE ADJUSTED TO ACCOMMODATE INTERSECTIONS.
- AVOID ANGLES LESS THAN 90° BY DOGLEGGING JOINTS THROUGH CURVE RADIUS POINTS. USE 90° ANGLES WHEN POSSIBLE.
- CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.
- PROVIDE TRANSVERSE JOINTS AT ALL PAVEMENT WIDTH CHANGES.
- CONSTRUCT DOWELED EXPANSION JOINT ON THE SIDE ROAD OF AN INTERSECTION IF THE SIDE ROAD IS CONCRETE PAVEMENT AND GREATER THAN 100 FEET IN LENGTH. ALIGN EXPANSION JOINT WITH EDGE OF RADIUS.
- THE ENGINEER MAY APPROVE SLIGHT VARIATIONS FROM THESE JOINTING DETAILS.

PAVEMENT DEPTH AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	CONTRACTION JOINT SPACING
6", 8"	12"
7", 7 1/2"	14"
8" & ABOVE	15"

CONCRETE PAVEMENT JOINTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED: [Signature] DATE: [Date] PAVEMENT SUPERVISOR

SDD 13C18-c Concrete Pavement Joint Types

DOWELED TRANSVERSE

EXPANSION JOINTS

UNTIED - LONGITUDINAL

UNDOWELED TRANSVERSE

TIED LONGITUDINAL

DOWELED TRANSVERSE

TIED TRANSVERSE

TIED LONGITUDINAL TO EXISTING

CONTRACTION JOINTS

CONSTRUCTION JOINTS

CONCRETE PAVEMENT JOINT TYPES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED: [Signature] DATE: [Date] PAVEMENT SUPERVISOR

DESCRIPTION

DATE

16745 W. Bluemound Road
Brookfield, WI 53005-5938
(262) 781-1000
rasmith.com

raSmith
CREATIVITY BEYOND ENGINEERING

Brookfield, WI | Milwaukee, WI | Appleton, WI | Madison, WI
Cedarburg, WI | Naperville, IL | Irvine, CA

VILLAGE OF STURTEVANT
RACINE COUNTY, WISCONSIN

2023 PAVING PROGRAM
DETAILS

DRAFT
NOT FOR CONSTRUCTION

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R.A. Smith, Inc.

DATE: 05/02/2023

SCALE: N.T.S.

JOB NO. 2232100

PROJECT MANAGER:
JASON FEUCHT, P.E.

DESIGNED BY: RJW

CHECKED BY: JMF

SHEET NUMBER
50

